

City of
Woodbury
Gloucester County, NJ

Redevelopment Plan &
Form-Based Code



WOODBURY REDEVELOPMENT PLAN: DOWNTOWN BUSINESS DISTRICT

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for:
The City of Woodbury
Gloucester County
New Jersey

PLAN INTRODUCTION

The following Redevelopment Plan is intended to be a dynamic plan over the course of its implementation. The buildings in the Illustrative Build-Out plans and architectural renderings show one possible outcome of complete Plan implementation and are purposefully not intended to represent architectural style or material. What is ultimately built in Woodbury will be shaped by the implementation process, the developers and architects selected by the City, and economic forces. The circumstances and conditions that shaped this Plan may change over time, and if so, the Plan may be formally amended by the City to reflect those changes. This Plan is meant to guide development over the next 50 or more years and was created to illustrate where and how redevelopment should occur in light of the City's desire to revitalize the Broad Street Business District, respond to the potential development pressures from the proposed light-rail reintroduction, and manage growth within the boundaries of the City.

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Figure 1.1: Rendering of the Vision for Broad Street at Centre Street at night

Successful Main Streets are vibrant places that are activated throughout the day and evening, weekdays and weekends. This contributes to a sense of real and perceived safety.

1.0 EXECUTIVE SUMMARY

In our fast-paced, modern lifestyles where people have multiple options as to where they can live, work, and shop, convenience and accessibility are considered essential for a successful Main Street. Parking, pedestrian and vehicular circulation must be designed in a predictable, logical manner, providing multiple routes in a highly interconnected network. At the same time, density and a flexible mix of uses are vital for creating the critical mass to ensure that Broad Street is vibrant, walkable, safe, and sustainable. The Broad Street Business District Redevelopment Plan is a bold, yet attainable vision to achieve revitalization. It will require strong leadership, support, and commitment from the City, its elected officials, and the entire Woodbury community to realize this vision.

1.1 Introduction

Upon review and consideration of the “*Woodbury Redevelopment Plan: Downtown Business District - Needs Study for Area In Need of Redevelopment/Rehabilitation*” as prepared by GROUP MELVIN DESIGN and dated February 11, 2009, the City’s Planning/Zoning Board recommended to the City Council that the Downtown Business District as defined in this plan be designated an area in need of redevelopment under the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et. seq. (the “LRHL”). On May 11, 2009, the City Council memorialized a resolution designating the Downtown Business District an area in need of redevelopment (Resolution #09-113). As an outcome of these actions, City Council authorized preparation of a Redevelopment Plan and hired GROUP MELVIN DESIGN to assist. This document serves as the *Redevelopment Plan*.



Figure 1.2: Illustrative Build-Out Plan



Figure 1.3: New Parallel Connector Street

1.2 Summary of Findings

Introducing a parallel road network improves connectivity and brings clarity and predictability to circulation on and around Broad Street.

Key to successfully revitalizing a community's business district is recognizing the fundamental attributes of a thriving downtown. Community workshops, stakeholder interviews and research led to the determination that there are seven critical components of a successful Main Street:

1. **Safe:** Sidewalks, parking areas, and mid-block connections are well lighted and activated through ground floor building activities.
2. **Vibrant:** Street and sidewalk activity occurs throughout the week and weekend and extends into evening hours.
3. **Convenient and Accessible:** Travel routes and parking areas are direct, accessible and predictable for vehicles and pedestrians alike.
4. **Flexible:** Traditional urban form is preserved, while flexibility is granted to allow building lots and structures to be reconfigured to meet market conditions.
5. **Mix of Uses:** A balance of commercial and residential uses allows for both day and nighttime populations to support the Main Street.
6. **Density:** Provides a customer base for Main Street businesses to thrive and for public spaces to maintain safety and vibrancy.
7. **Public Spaces:** Public spaces in the form of sidewalks and public plazas are where Main Streets live. They should be porous, open, and accessible, and should be programmed through the day and evening.



Figure 1.4: Mid-Block Cut-Through

Given the criteria for a successful main street, what are the opportunities present in Woodbury to create a sustainable downtown? What are issues facing Woodbury residents and businesses that must be addressed to successfully redevelop the Broad Street corridor, Woodbury's main street?

Improving pedestrian access to and through buildings will improve the convenience of visiting Broad Street and adjacent streets with ground-floor retail.

Opportunities:

1. **Traditional urban appeal.** Regional and national trends toward smart growth, downsizing and energy conservation means renewed interest in small towns like Woodbury.
2. **Regional position.** Woodbury is uniquely positioned not only in terms of its small town appeal, but also as a community of regional significance.
3. **Traditional bone structure.** Woodbury has the higher density, mix of use, interconnected street network and multi-modal circulation system typical of a traditional main street community.
4. **Public spaces.** The City has an opportunity to enhance its existing park system, interconnecting individual parks, activating them with programmed events and venues, and creating new spaces.
5. **Community engagement.** Woodbury's constituents have shown a commitment to listen, learn and participate through community workshops.



Issues:

1. **Overcoming real and perceived safety concerns.** In addition to pedestrian safety surrounding the speed of traffic on Broad Street, the corridor lacks the round the clock presence of people to keep an eye on the street.
2. **Diversifying housing options.** Over half of the City's housing stock consists of single family homes, the house of choice for the traditional family unit. Yet, married couples with children comprise only 18.5% of the City's households.
3. **Improving the circulation system.** Local and regional traffic converge on Broad Street, creating traffic problems that could be reduced with more travel routes, interconnections, and improved access to parking.
4. **Addressing antiquated lot configurations.** Many properties in downtown Woodbury have an excessively long, narrow configuration. This is a fundamental obstacle to economic revitalization.
5. **Addressing outdated building floor plates.** Some of the older buildings in town are only 25 to 35 feet wide and up to 200 feet deep. This type of configuration does not lend itself to contemporary office or residential development needs.
6. **Lack of central gathering places.** Residents observe the need to improve existing parks, construct connections between parks, and create new parks or plazas, all within its downtown.
7. **The need to plan for transit hubs.** The City must consider redevelopment needs and opportunities, parking, and pedestrian and vehicular circulation opportunities created by a reactivated commuter rail line.



Figure 1.5: Proposed Centre Street Plaza

The Centre Street Plaza is the nexus of the parallel street network. It serves to not only create the full parallel connection from Cooper to Barber, but also provide access to Broad Street, while creating a significant public space for the community.

1.3 Redevelopment Goal and Objectives

What emerged from the above findings is a vision statement and accompanying goal and objectives serving as the basis for redevelopment. The goal and objectives are as follows:

Goal: Redevelopment shall facilitate sustained economic, social and environmental health and vitality in downtown Woodbury.

Objectives:

1. Use density, a mix of uses and public spaces as building blocks to transform Broad Street into an active, vibrant main street corridor.
2. Expand the inventory of housing options to meet the needs of a diverse population.
3. Capitalize on existing community assets to strengthen the City's position in the regional economy.
4. Maintain a highly interconnected, multi-modal street network that is legible, porous and safe.
5. Accommodate contemporary building needs while respecting traditional urban form.
6. Create a unique sense of place in downtown Woodbury.



Figure 1.6: Illustrative Build-Out Plan
Proposed for North Broad Street

Proposed redevelopment along North Broad is anchored by Underwood Memorial Hospital across the street, and adjacent to Stewart Lake.

1.4 Plan Recommendations

To fulfill the Redevelopment vision, a series of plans were created to address land use and form, circulation and parking, public spaces, gateways, and infrastructure. Each of these plans is highlighted in Section 4 of the Plan document. The culmination of these plans is the Illustrative Build-Out Plan. This Plan suggests how redevelopment may be carried out in order to accomplish the goal and objectives established during the planning process. The Build-Out Plan does not prescribe an architectural style or mandate building location; rather it is a suggestion of building massing and relationship to the public realm. This illustrative master plan highlights redevelopment recommendations collectively in one drawing.

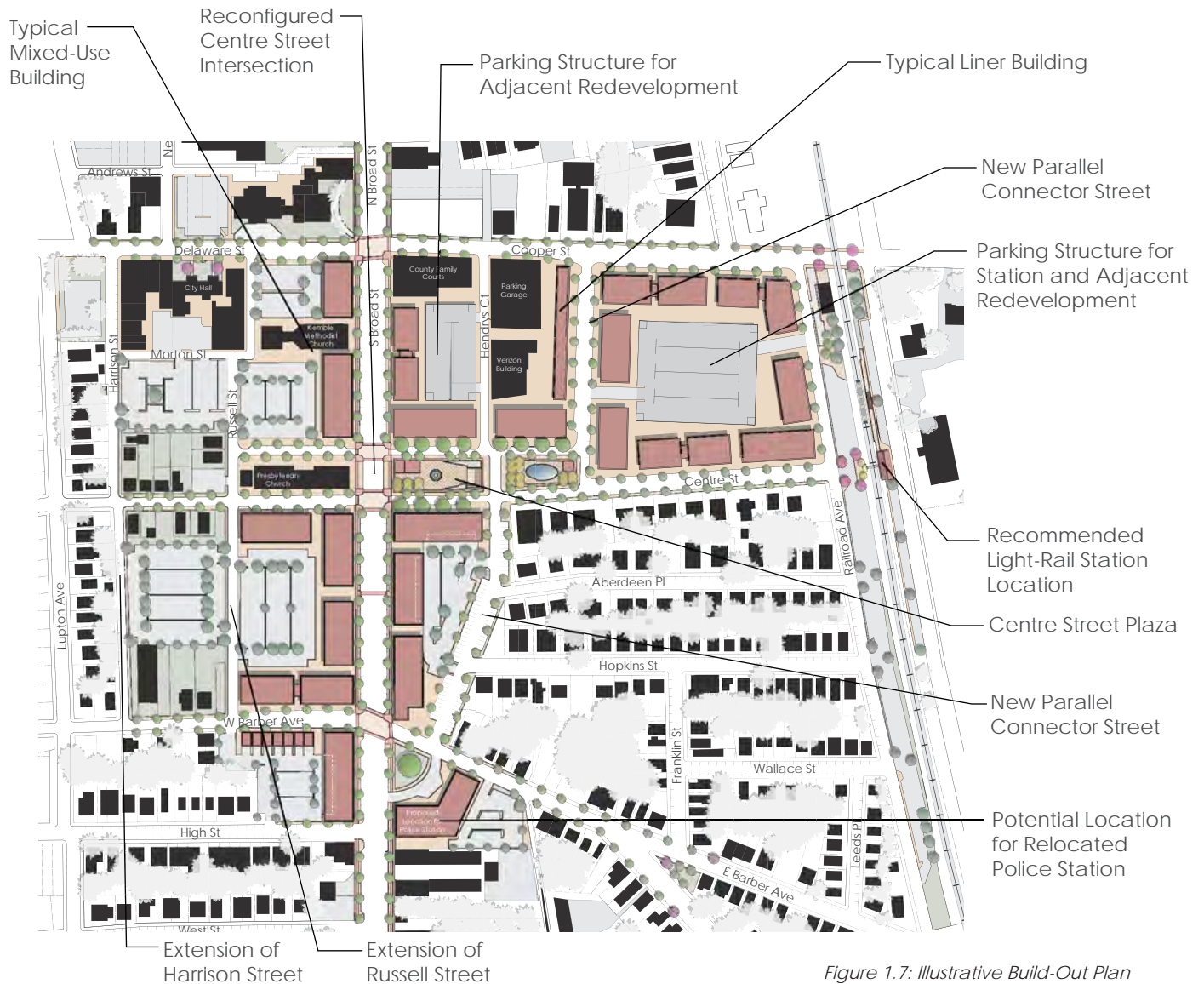


Figure 1.7: Illustrative Build-Out Plan Proposed for South Broad Street

The Illustrative Build-Out Plan contains 1.5 million square feet of potential new development throughout the Redevelopment Area. While many factors will influence the mix of uses, the Plan offers a suggested blend of dwelling units and commercial square footages that will achieve the vision. The goal of the ultimate build-out is to introduce approximately 800 housing units, 280,000 square feet of office, and 220,000 square feet of ground-floor retail in the specific redevelopment sites identified in the Plan. This strategy would accommodate approximately 1,200 new City residents to help to ensure that Broad Street is active and vibrant through the evenings and weekends, to support local businesses and to contribute to the employment base in the City.

Redevelopment along South Broad is situated to link the proposed rail station with the downtown and to revitalize the City's "Main Street."

1.5 Plan Implementation

The Redevelopment Plan sets forth a series of implementation approaches to achieve the City's redevelopment goals. These approaches respond to the statutory framework for redevelopment in New Jersey, while being tailored to the unique context of the City of Woodbury. The Redevelopment Plan also identifies the selected institutional framework and associated authorities available to the City to oversee redevelopment activities within the Redevelopment Area.

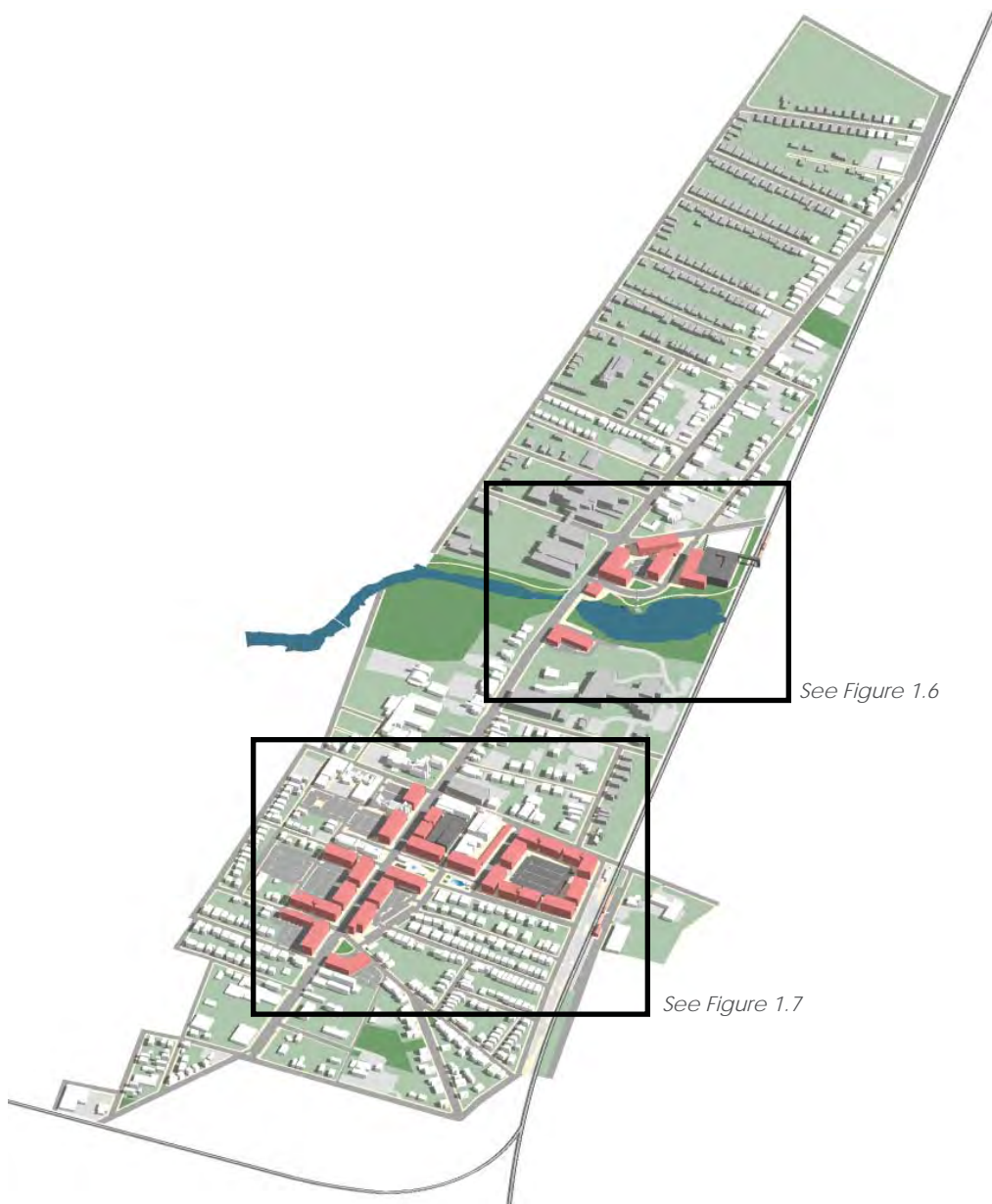


Figure 1.8: Proposed Redevelopment Build-Out



To guide implementation throughout the Redevelopment Area, in lieu of traditional redevelopment design standards, a Form-Based Code (FBC) was created. The *FBC* applies to the whole of the *Downtown Business District Redevelopment Area*. The regulations presented within the *FBC* address the location and density of buildings; their interaction with public right-of-ways; and site planning and architectural considerations that help to implement the Vision of the *Redevelopment Plan*. These regulations and standards are determined by Transect, Building Type, and Street Frontage. Once the regulations for the specific site have been determined, the General Design Standards should be reviewed to understand the rules that apply across the Redevelopment Area, regardless of Transect, Frontage, and Building Type. The *FBC* supersedes any of the other provisions of the City Zoning Code and the Subdivision and Land Development Ordinance.

Figure 1.9: Proposed Centre Street Plaza

A plaza along Centre Street allows for a public space along Broad that will be activated and kept safe by adjacent ground floor retail and upper floor population on a daily basis. Programming the plaza with elements such as a café, stage, fountain, and a pond/ice rink, will ensure that the space is also a destination and remains active throughout the year.

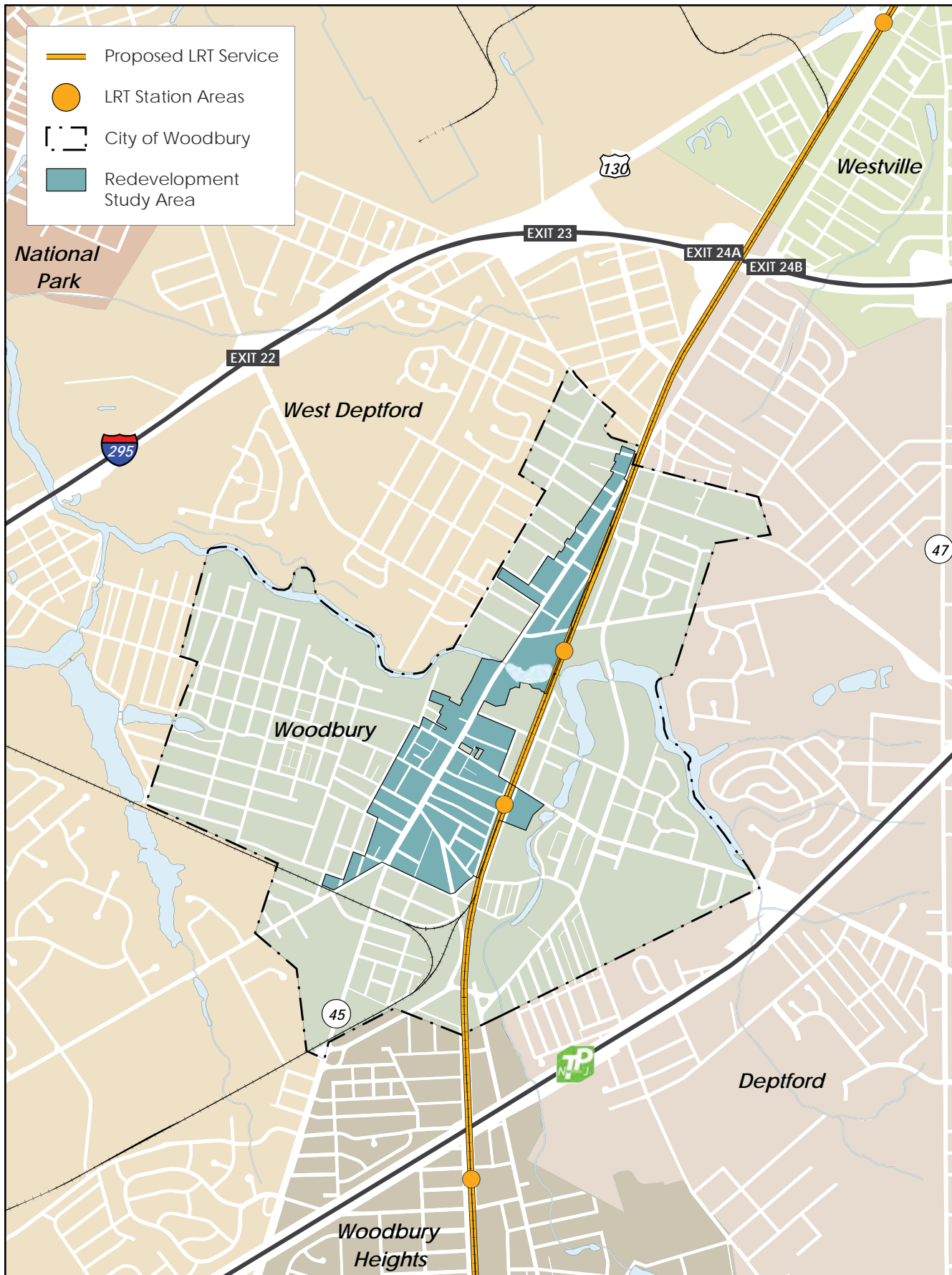


Figure 2.1: Regional Context Map

2.0 | PLAN BACKGROUND

2.1 Why Redevelopment?

Many long-time residents of Woodbury recall the movie houses and stores that once lined Broad Street, creating an active, vibrant commercial corridor. Broad Street's businesses not only attracted visitors but also served the needs of the surrounding community. Since then, competition with neighboring retail centers and residential communities that offer modern amenities in a safe and predictable configuration have contributed to a struggling "Main Street" in Woodbury. Even though the most recent global building boom focused on building new compact, walkable communities and town centers, much of this investment skipped over existing small cities and towns and developed in suburbia. Through this economic downturn, however, the trend towards downtown living and shopping continues, as a renewed national focus has been placed on downsizing, smart growth and reducing energy demands.

Developing new buildings or redeveloping older structures in an existing community presents challenges and risks different from those in new communities or on rural lands. Potential developers, business owners, and residents must be able to accommodate their own needs as well as modern zoning and building code requirements while respecting the form of the community. Redevelopment enables a community to retain greater control in implementing their Plan. It provides people within the community an enhanced ability to negotiate with developers and provides economic incentives for projects that fulfill the community vision.



2.2 The Attributes of a Successful Main Street

What are the fundamental attributes of a successful main street? What aspects of downtown Woodbury's built environment – its streets, parking and land use patterns – present opportunities to restore Broad Street's role as a healthy, thriving and enduring main street? The answers to these questions provide the underpinnings, the planning approach and the design solutions for the *Redevelopment Plan*.

To respond to these questions, the City directed GROUP MELVIN DESIGN, its consultant team, to conduct an extensive stakeholder outreach process. In addition to hosting three community-wide workshops with the general public, the team interviewed 28 public and private sector stakeholders and stakeholder groups with a direct interest in the redevelopment of downtown Woodbury. Stakeholders included City staff and elected officials, Gloucester County, Delaware River Port Authority, Underwood Memorial Hospital, Main Street Woodbury, Greater Woodbury Ministerium, Woodbury Rotary, Woodbury Breakfast Rotary, Kiwanis, Greater Woodbury Chamber of Commerce, West End Neighborhood Association, Woodbury Arts Council, Woodbury Sketch Club, Roadrunners, Center City Neighborhood Watch and Neighborhood Preservation Program, as well as a number of private business owners. Additionally, GROUP MELVIN DESIGN reached out to national developers of retail space and mixed use main streets.

From this process of community engagement and interviews emerged a series of recurring themes. These themes suggest that successful main streets exhibit four quality of life attributes which in turn, are supported by three related physical attributes.

Quality of Life Attributes of Successful Main Streets:

1. **Safety.** Successful main streets are acknowledged as having pedestrian activity occurring on a 24/7 basis. This contributes to a sense of real and perceived safety. In a safe environment, streets, pedestrian paths, public spaces and parking areas are well lighted. An appropriate balance of office uses provides a strong weekday pedestrian presence, particularly during commuting and lunch hours, while restaurants shops and entertainment venues draw pedestrians out during evening hours. A healthy main street also includes residential units. Residents in flats above ground floor retail are ever-present eyes on the street, not only



Figure 2.3: A mix of uses with a density of 25 to 30 units per acre creates 24/7 activity



Figure 2.4: Programmed outdoor events increase the vibrancy of a main street



Figure 2.5: A grid street network enhances convenience and accessibility

during the weekday, but also on weekends and at night. Taken together, these conditions generate a heightened sense of security and comfort among residents, visitors and merchants which in turn, reinforces activity and street life throughout the week.

2. **Vibrancy.** Successful main streets exhibit vitality when they are activated throughout the day and evening, weekdays and weekends. For this to occur, there must be multiple destinations and diverse activities made available to residents and visitors alike.
3. **Convenience and Accessibility.** In our fast paced, modern lifestyles where people have multiple options as to where they can live, work, and shop, convenience and accessibility are considered essential for a successful main street. Travel routes must be designed so that various activity centers and destinations are connected in a direct manner. Vehicular circulation should be designed in a predictable, logical manner, providing multiple routes in a highly interconnected network. Parking areas must be equally predictable, easy to locate and readily accessed from the street. An adequate inventory of parking must be available and located within a reasonable distance of establishments' front doors. Sidewalks must be continuous, sufficiently wide, attractively landscaped and lighted. Pedestrian paths should link parking areas to buildings and to the main street for direct, safe and convenient travel.
4. **Flexibility.** Successful main streets are not static. While the integrity of walkable streets with predictable urban store fronts must be maintained in a successful downtown it is also true that, land uses, building footprints and building lots must be able to change and be reconfigured, in response to evolving economic conditions.

Physical Attributes of Successful Main Streets:

1. **Mix of Uses.** A balanced mix of uses, both residential and non-residential, is essential to create a sustainable main street. A mix of residential and nonresidential uses in a concentrated area brings destinations closer together, contributing to convenience and accessibility. It facilitates more foot traffic during the day and evening, which in turn, contributes to a safe and vibrant main street. A diverse range of uses also contributes to sustainability allowing main streets to adapt to changing economic conditions. It is recommended that approximately 30 percent retail, 30 percent office and 30 percent residential be provided.
2. **Density.** A successful main street maintains not only a mix of uses but a sufficient density of each of these uses. An analysis of Woodbury's demographics and position in the regional economy suggests a need for higher densities of residential and office uses to support the retail component of its downtown. A net residential density of 20 to 35 dwelling units per acre is not uncommon in small towns like Woodbury. Sufficient residential density provides other related benefits as well. Reduced private outdoor space in denser conditions encourages people to "take to the streets" for outdoor activity, enhancing social interaction and main street vibrancy. Homes above ground floor main street businesses offer built-in security, with more people present on a 24/7 basis to keep an eye on the street.
3. **Public Spaces.** Streets, plazas and parks collectively comprise public spaces. These spaces must be strategically designed to ensure that they draw people in to socialize, relax, or engage in activities. They must be designed with safety, convenience and accessibility in mind. To that end, they should be well lighted and highly porous, with plenty of open, accessible connections to the street. They should be appropriately scaled and sized to support the resident population. They should be designed to accommodate varied activities that are proactively programmed and managed on an ongoing basis.



Figure 2.6: Flexibility in the urban fabric allows for infill development and adaptive reuse



Figure 2.7: Public spaces have the ability to serve as outdoor gathering spaces

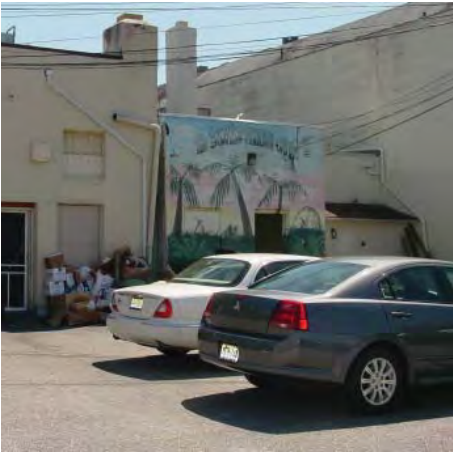


Figure 2.8: An unsightly, make-shift parking area located behind a South Broad Street business

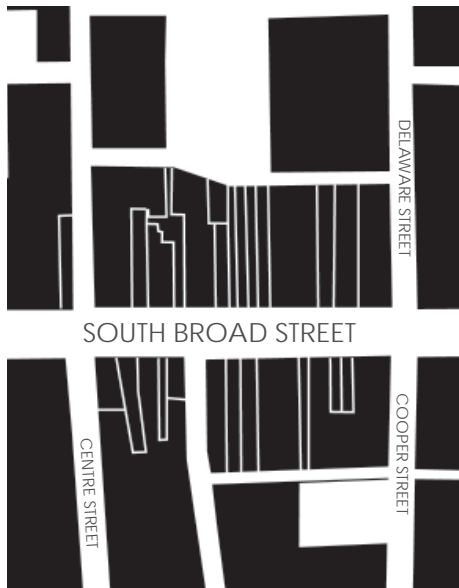


Figure 2.9: The current block and lot configurations along South Broad Street hinder growth of the commercial district



Figure 2.10: Vacant storefronts along South Broad Street

Finally, they should be well-maintained and offer a comfortable, clean environment, with landscaping, pedestrian furniture and features that offer diverse visual interest.

The seven prerequisites to a successful main street suggest the nature of physical improvements needed to create sustained revitalization in downtown Woodbury. Based on stakeholder input, field reconnaissance and comprehensive data review and physical inventory, the following three conditions were identified as integral to this outcome:

1. **Parking and Circulation.** Additional streets are needed to break up excessively large blocks, provide more travel routes, and divert local trips off of Broad Street and onto parallel local streets that feed clearly identified parking areas. Parking areas are too often perceived as vacant lots that are poorly maintained and isolated. To overcome this, they need to be well marked, well lighted and landscaped. They need to be accessible from a logical system of streets to feel safe and convenient. The buildings that they serve should be attractively designed, with inviting rear entrances. Wayfinding signage is also an important consideration, directing traffic to designated parking areas. Furthermore, pedestrians must have direct access from parking areas to Broad Street and local roads through open, lighted passageways. Pedestrian circulation demands safer intersection crossings and well-maintained and landscaped sidewalks. Separated pedestrian paths which are currently lacking could provide safe, direct alternate routes for those on foot, linking key activity centers. All of this will contribute to a predictable and comprehensive network of roads, parking and pedestrian facilities, thus leading to a heightened sense of safety. Finally, it is essential that a comprehensive circulation system be designed to address the impacts and opportunities presented by two planned commuter rail stations on the soon-to-be-restored commuter rail line. Vehicular and pedestrian circulation, parking, and land uses must all be accommodated in an integrated manner.

2. **Block and Lot Configuration.** A review of Woodbury's tax parcel maps reveals a pattern of antiquated tax parcel configurations. Excessively long, narrow lots limit the ability of existing or future buildings and their associated parking and loading areas to be designed to meet contemporary commercial and mixed use development needs. This is a fundamental obstacle to economic revitalization.

3. **Adaptive Reuse of Buildings.** While historic preservation is a laudable goal, it must be balanced against the need for sustainable economic revitalization. Sometimes, the design of older buildings does not lend itself to contemporary solutions, particularly for office and residential development. It is not unusual to find buildings in downtown Woodbury that are 25 to 35 feet wide and as much as 200 feet in depth, consistent with existing lot configurations. The floor plates pose a substantial limitation on contemporary residential and office uses. Many of these buildings currently lay vacant along Broad Street. Redevelopment can be accomplished in a manner that preserves the traditional urban form of downtown Woodbury; however, this demands a careful balance between preservation of older buildings and the flexibility to adapt these buildings and the lots on which they reside to new configurations, accommodating changes that address today's commercial and residential needs.

2.3 The Redevelopment Act

The Local Redevelopment and Housing Law or LRHL (N.J.S.A. 40A-12A.1 et seq.) authorizes municipalities to initiate a process that transforms underutilized or poorly designed properties into healthier, more vibrant, or economically productive land uses. The process has been used successfully throughout New Jersey to creatively improve properties meeting specific redevelopment criteria. Projects approved for redevelopment are often eligible for certain types of technical and financial assistance from the State.

Under the LRHL, the redevelopment process involves several key steps, as follows:

- A. At the local governing body's direction, the planning board conducts a preliminary investigation to establish whether a specific area is in need of redevelopment.
- B. The preliminary investigation is conducted, followed by a public hearing on the proposed area in need of redevelopment.
- C. The governing body considers the planning board's recommendation and determines whether to designate the area or a portion thereof in need of redevelopment.
- D. A Redevelopment Plan is prepared upon authorization by the governing body, and then adopted by the governing body by ordinance.
- E. A public entity is designated as the redevelopment entity to oversee Redevelopment Plan implementation.
- F. The redevelopment entity chooses the redeveloper(s) who will implement the redevelopment project.

For the City of Woodbury, this process was approved on February 9, 2009, when City Council adopted Resolution #09-61, authorizing a study to determine the need for redevelopment along the Broad Street Corridor.

2.4 Preliminary Investigation

The Preliminary Investigation consisted of four principle tasks. First a preliminary Study Area boundary was established. Following this, a field survey and background research were conducted of all parcels in the study area to examine land uses, property vacancies, property conditions, building heights and property ownership. Individual parcels were then evaluated against the eight statutory criteria to determine whether they qualify as part of an area in need of redevelopment. The Preliminary Investigation concluded that the study area was in need of redevelopment, meeting



Figure 2.11: Redevelopment Area (Grey)

at least five of the eight statutory criteria. However, the study area boundaries were adjusted slightly, to eliminate a handful of parcels that did not meet the criteria.

The Preliminary Investigation resulted in a published document dated, February 11, 2009, which concluded with the following statements:

"Creating a Redevelopment Plan will help to reinforce the strong residential neighborhoods and improve their value through the creation of a strong, walkable, lively and viable commercial district."

"It is the recommendation of this investigation that this reduced Study Area be designated by the City Council of Woodbury as an area in need of redevelopment."

Three Steering Committee meetings open to the public were conducted and two additional meetings of the Woodbury Planning/Zoning Board were held on April 6, 2009 and May 4, 2009 with regard to the proposed area in need of designation. On May 11, 2009, the City Council memorialized a resolution designating the recommended area as an area in need of redevelopment and authorized preparation of a Redevelopment Plan. GROUP MELVIN DESIGN was the consultant hired to conduct the Preliminary Investigation and, subsequently, to prepare the Redevelopment Plan.

This document, entitled *"Woodbury Redevelopment Plan: Downtown Business District - Needs Study for Area in Need of Redevelopment/Rehabilitation,"* is available for public review at the City of Woodbury municipal building.

2.5 Description of Redevelopment Area

2.5.1 Regional Context

Approximately 12 miles from Philadelphia by car, the City of Woodbury is located in Gloucester County along NJ State Route 45 (*Figure 2.1*). It is bordered by West Deptford to the west, Deptford to the east, and Woodbury Heights to the south. The City of Woodbury is the county seat for Gloucester County and has held that role for more than 200 years.

According to the U.S. Census, Woodbury has an estimated residential population of 10,450 (2008). The *Comprehensive Master Plan* for the City of Woodbury (December, 2006) reports that over 10,000 professionals travel into Woodbury every day for work. As the county seat, much of the land in the heart of downtown Woodbury is dedicated to County offices and courts. Classified as a “Metropolitan Planning Area (PA1)” by the New Jersey Office of Smart Growth, Woodbury is exactly the type of place that is addressed by the first goal of the State Plan to revitalize the State’s Cities and Towns. The strategy for this goal is to “protect, preserve and develop the valuable human and economic assets in cities, towns and other urban areas” and “plan to improve their livability and sustainability.”

The 1.5 mile corridor of Broad Street through Woodbury stretches from one end of the City to the other creating a commercial spine for the City. Broad Street has historically been the core of commercial activity in Woodbury. Traditionally, the business district was bounded by Stewart Lake to the north, stretching to the fork in the road at Kings Highway and Mantua Pike, at the southern end. People and goods were moved along the rail line that runs east of and parallel to Broad Street within the City’s business district, making the connections between that area of Broad Street and the tracks critical. This relationship is evidenced by the narrow, long blocks that run perpendicular to Broad Street.



Figure 2.12: West side of South Broad Street - Delaware to West Centre Streets



Figure 2.13: East side of South Broad Street - East Centre Street to Aberdeen Place

Broad Street is not only the primary commercial corridor for Woodbury, but also a State Highway known as Route 45. It is a regional arterial that provides critical truck, car, and transit connections between South Jersey communities. Over time, as mobility increased, competition with other retail and business centers grew. No longer bound by the need for proximity to the rail station or the traditional business core, commercial activity stretched further north along Broad Street, and south beyond the City boundary, where onsite parking was more easily accommodated.

In recent years, the growth of the two major employers, Underwood-Memorial Hospital and Gloucester County, has meant an increase in jobs and auto and pedestrian traffic to and through Woodbury. Thus, the efficient flow of traffic around and through Woodbury is not only important to the local residents and visitors, it is also essential for emergency vehicles and the movement of goods to local and regional businesses.

As we look ahead, restoration of the commuter rail line by the Delaware River Port Authority (DRPA) will connect Glassboro, Gloucester County to the City of Camden in Camden County. Plans to restore commuter service include construction of two rail stations in Woodbury. These significant infrastructure improvements will impact the City of Woodbury in several ways. They will trigger the need to develop enhanced circulation and parking facilities to accommodate those commuting to Woodbury to use the new rail service. They will dictate the need for enhanced



Figure 2.14: North side of High Street - Lupton to South Broad Streets



Figure 2.15: West side of North Broad Street - Crescent Avenue to Dickerson Street

pedestrian infrastructure so that upon arrival in Woodbury, commuters can walk to their destinations. And, they will create the demand for new residential housing and professional office options for those choosing to live and/or work in locations convenient to regional rail.

2.5.2 Area in Need of Redevelopment

The Area in Need of Redevelopment is established along the City boundary to the north (just south of Hessian Street), Emerson and Carpenter Streets to the south, the rail line to the east (with the exception of an extension to include the St. Patrick's School along Cooper), and roughly Lupton Ave, Tatum, and Cherry Streets to the west (*Figure 2.2*). Within the Area in Need of Redevelopment are prominent vacant properties, obsolete block and property configurations and significantly underutilized properties including public owned lands. At the same time, the Area exhibits a compact development pattern, a mix of residential, commercial and institutional uses, an interconnected street system, and existing and potential public spaces. It hosts major employers and small businesses, discrete neighborhoods and a walkable school district. Bus and rail infrastructure are within or adjacent to the Area. Implementing a strategic approach to redevelopment means capitalizing on existing assets, transforming liabilities into opportunities, and bringing about sustained economic revitalization for the benefit of residents and businesses.



Figure 2.16: North side of Progress Avenue - Tatum to North Broad Streets



Figure 2.17: North side of East Barber Avenue - South Broad to Franklin Streets

2.6 Planning Process and Public Participation

As previously noted, on May 11, 2009 City Council authorized preparation of a Redevelopment Plan, following adoption of the Preliminary Investigation by the governing body. The City contracted with GROUP MELVIN DESIGN to prepare the Redevelopment Plan. The planning process consisted of four key tasks:

- A. Community and stakeholder outreach
- B. Background research and analysis
- C. Plan development
- D. Plan review and adoption

2.6.1 Community and Stakeholder Outreach

Public participation was central to the planning process, and more specifically, to defining the vision for the *Redevelopment Plan*. To this end, three community events were held and stakeholder interviews were conducted.

During the first community workshop held on August 25, 2009, more than 50 community members articulated their vision of how a revitalized Broad Street corridor would look and function in the future. Examining base maps of the Redevelopment Area, workshop participants identified places and activities that were important community assets, sites that were problem areas, and circulation issues and opportunities.



Figure 2.18: Photo from August 25, 2009 Community Workshop

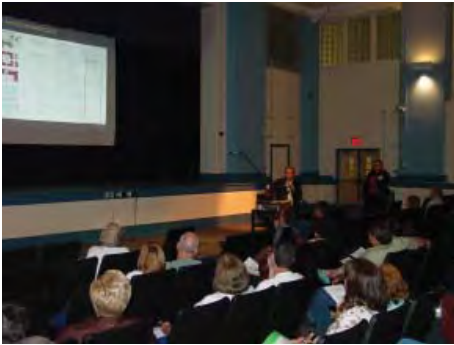


Figure 2.19: Photos from September 2, 2009 Community Workshop

The second workshop, conducted on September 2, 2009, was attended by approximately 40 people. This event focused on design considerations important to defining and maintaining community character in Woodbury. Examples of various building types, building height and scale, public spaces, streetscapes and sidewalks, parking, and gateways, were presented on slides. Participants selected examples that they felt were most appropriate for downtown Woodbury's future growth and revitalization.

Based on community input from the first two public workshops, the City consultant team prepared a draft conceptual plan. The plan consisted of maps and photographs showing locations for future potential physical improvements and what they might look like. At a third community workshop held on October 29, 2009, 25 participants offered verbal and written comments to the consultants during an informal open house format, following the presentation. This was followed by a summary presentation of the redevelopment planning process to date and corresponding findings. The presentation was delivered during the November 19, 2009 City Council meeting.

In addition to these workshops, the consultant team conducted a series of individual and group interviews. Interviews were conducted with key public and private sector stakeholders who will influence or be affected by the redevelopment of downtown Woodbury. Interviews gleaned a picture of what constitutes the key obstacles to and opportunities for redevelopment and the criteria for sustained economic revitalization.

2.6.2 Background Research and Analysis

Extensive research and analysis was an essential part of the planning process, given the complex nature of redeveloping a largely built up area. Much of the groundwork was laid during the preparation of the Preliminary Investigation, the document providing justification for the *Redevelopment Plan*. To complete the Preliminary Investigation, City master plan documents and ordinances were reviewed. GROUP MELVIN DESIGN conducted field surveys to assess and map existing land uses, vacant parcels, building conditions, property vacancy and property conditions within the downtown area. Tax data was used to discern parcel configurations, lot acreages and ownership patterns. This information, in turn, provided the basis or beginning analysis for the *Redevelopment Plan*.



Figure 2.20: Photos from October 29, 2009 Community Workshop

Subsequent research and inventory work was conducted on cultural and institutional assets and on infrastructure to determine what physical features would be preserved under the *Redevelopment Plan*. Historical research also included a review of Sanborn maps, to understand the evolution of the City's building lots, buildings, and streets. GROUP MELVIN DESIGN contracted with Joseph F. McKernan Jr. Architects & Associates to assess the viability of existing building floor plans. National developers of retail and mixed use were interviewed to understand regional and national trends in retail and mixed use development. Kernan Consulting Engineers analyzed preliminary concept plans for circulation improvements and reviewed utility capacity and demands. Outreach to the Delaware River Port Authority provided important information concerning the future location and infrastructure needs associated with the planned rail stations in Woodbury. Finally, the consultant team reviewed County, regional, and State plans and master plans and regulations of adjacent municipalities to analyze the relationship of the *Redevelopment Plan* to said documents.

2.6.3 Plan Development

Background research and public outreach shaped the plan vision, goals and implementation strategies. These, in turn, formed the framework, for the *Regulating Plan*, including the *Land Use/Transect Plan*, *Street Regulating Plan*, *Public Spaces Plan*, *Gateway Plan* and *Infrastructure Plan* and for the approach to plan implementation.

2.6.4 Plan Review and Adoption

Under the Local Redevelopment and Housing Law (LRHL), when the governing body prepares a Redevelopment Plan, it must refer the plan to the planning board for review and recommendations. The Planning Board has up to 45 days to complete the review and transmit its recommendations to the governing body. Accordingly, the draft Redevelopment Plan was transmitted to the Planning Board and presented at a Planning Board public meeting for review and consideration on March 15, 2010. The Planning Board adopted a resolution referring the Plan to City Council for adoption, with no revisions recommended. On May 10, 2010, City Council introduced the revised Plan and conducted a first reading. On May 24, 2010, it held a public hearing and adopted the plan by ordinance.

2.7 Plan Content

The Local Redevelopment and Housing Law establishes the mandatory components of a Redevelopment Plan. These items, found in (N.J.S.A. 40A-12A-1 et seq.), are as follows:

- A. Relationship of the Redevelopment Plan to local objectives related to land uses, population density, circulation, public utilities, recreational and community facilities and other public improvements;
- B. Proposed land uses and building requirements in the project area;
- C. Provision for the temporary and permanent relocation, as needed, of residents within the project area, including an estimate of available housing that is safe, affordable, decent and sanitary to displaced residents within the existing local housing market;
- D. Identification of any property within the Redevelopment Area that is proposed to be acquired;
- E. Relationship of the Redevelopment Plan to the master plans of contiguous municipalities, the master plan of the county; and the State Development and Redevelopment Plan;
- F. Relationship of the Redevelopment Plan to pertinent municipal development regulations;
- G. An inventory of housing units affordable to low and moderate income households to be removed as a result of implementing the Redevelopment Plan; and
- H. A plan to provide, through new construction or substantial rehabilitation, one comparable, affordable replacement housing unit for each affordable housing unit which has been occupied at any time within the last 18 months and is to be removed as a result of implementing the Redevelopment Plan.

The *Woodbury Downtown Business District Redevelopment Plan* addresses these statutory requirements. But the plan goes beyond these requirements, offering additional guidance to the community and future redevelopers.

First, the *Redevelopment Plan* includes not only a building *Regulating Plan*, identifying proposed land uses and building requirements in the project area, it also includes a street *Regulating Plan*, public spaces plan, infrastructure plan and gateways plan.

Secondly, the *Redevelopment Plan* contains an implementation strategy, outlining the manner in which redevelopment is anticipated to occur, property is to be acquired and parking and circulation are to be addressed.

Third, the *Redevelopment Plan* contains an implementing ordinance – specifically a Form-Based Code - that is used in conjunction with the *Regulating Plans* to direct the form, function and land uses associated with future redevelopment.

Finally, the *Redevelopment Plan* addresses procedures for amending the plan and the zoning map, as well as the authorities and powers assigned to the City as the redevelopment entity.



Figure 3.1: Rendering of the Vision for Broad Street at Centre Street at night

3.0 | THE VISION

3.1 Community Opportunities & Issues

Community and stakeholder feedback provided a comprehensive inventory of issues and ideas to support the creation of a vibrant and revitalized Broad Street corridor. Participants in the public process were asked to define the issues that create roadblocks to revitalization and reinvestment in Woodbury. They were also asked what they thought should be done through redevelopment to spur revitalization in the business district. These are summarized below.

A. Opportunities:

1. **Traditional urban appeal.** Even though main street communities like Woodbury have suffered the impacts of suburbanization and the competition that suburban amenities present, regional and national trends toward smart growth, downsizing and energy conservation means renewed interest in small towns like Woodbury.
2. **Regional position.** Woodbury is uniquely positioned not only in terms of its small town appeal, but also as a community of regional significance. It is one of only 21 county seats of government in New Jersey. It is the location of two major employers. And through it passes a regionally significant rail corridor with planned community service and two station stops within downtown Woodbury.



Figure 3.2: Entrance to the County parking garage on Cooper Street



Figure 3.3: Municipal parking located behind businesses on the west side of South Broad Street



Figure 3.4: Underwood Memorial Hospital

3. **Traditional bone structure.** Woodbury has the “bone structure” of a traditional main street community. This refers to the higher density, mix of use, interconnected street network and multi-modal circulation system typical of such a place. It is certainly true that changes to buildings and building lots, densities, and the circulation system are essential to meet contemporary development needs and fuel economic revitalization. However, the foundation for these changes already exists. The City need not start from scratch.

4. **Public spaces.** The City has a tremendous opportunity to enhance its existing park system, interconnecting individual parks, activating them with programmed events and venues, and creating new parks and plazas to enhance downtown appeal.

5. **Community engagement.** Woodbury’s governing body is committed to facilitating downtown revitalization through implementing a Redevelopment Plan. Its constituents have shown a commitment to listen, learn and participate through community workshops. Public participation, community support and local leadership are essential ingredients of a successful redevelopment effort.

B. Issues:

1. **Overcoming real and perceived safety concerns.** Specific issues, such as pedestrian crossings on Broad Street; deficient pedestrian lighting on streets and in parking areas; and speeding traffic are seen as contributing to safety problems. More broadly, inadequate housing density on Broad Street and insufficient mix of uses are viewed as safety issues. With few households above shops on Broad Street, the corridor lacks the round the clock presence of people to keep an eye on the street. Without diverse, thriving businesses and sufficient housing density, the corridor lacks needed foot traffic to keep Broad Street vibrant, active and feeling safe day and night.

2. **Diversifying housing options.** Over half of the City's housing stock consists of single family homes, the house of choice for the traditional family unit. Yet, married couples with children comprise only 18.5% of the City's households. Rising energy prices, coupled with regional and national trends toward urban living, suggest the need for more diverse housing options - options to meet the needs of young professionals, couples without kids, empty nesters and seniors.



Figure 3.5: Lakefront and existing police station

3. **Improving the circulation system.** More convenient and accessible streets and parking are needed downtown. Presently, local and regional traffic converge on Broad Street, creating traffic problems that could be reduced with more travel routes and interconnections. Parking areas are poorly signed and inaccessible. Plans to reinstate a commuter rail line and construct two rail stations in Woodbury means there must be better circulation to and from the stations as well as parking facilities to serve rail passengers.



Figure 3.6: Vacant G.G. Green Block

4. **Addressing antiquated lot configurations.** Many properties in downtown Woodbury have an excessively long, narrow configuration. This limits the ability of existing buildings or new construction to be designed in a manner that accommodates today's commercial and mixed use development building, parking and loading requirements. This is a fundamental obstacle to economic revitalization.

5. **Addressing outdated building floor plates.** It is not uncommon to find older buildings in town that are only 25 to 35 feet wide and up to 200 feet deep. This type of configuration does not lend itself to contemporary office or residential development needs.



Figure 3.7: Existing gateway at the south end of downtown Woodbury

6. **Lack of central gathering places.** As noted above, Woodbury has a parks and open space system. Residents nonetheless observe the need to improve existing parks, construct

connections between parks, and create new parks or plazas, all within its downtown. Additionally, the City would benefit from gateway features – distinct physical features announcing the arrival into the downtown area

7. **The need to plan for transit hubs.** The City must consider redevelopment needs and opportunities, vehicular circulation and parking, and pedestrian circulation opportunities created by a reactivated commuter rail line running through Woodbury and two associated transit stops planned for downtown Woodbury.

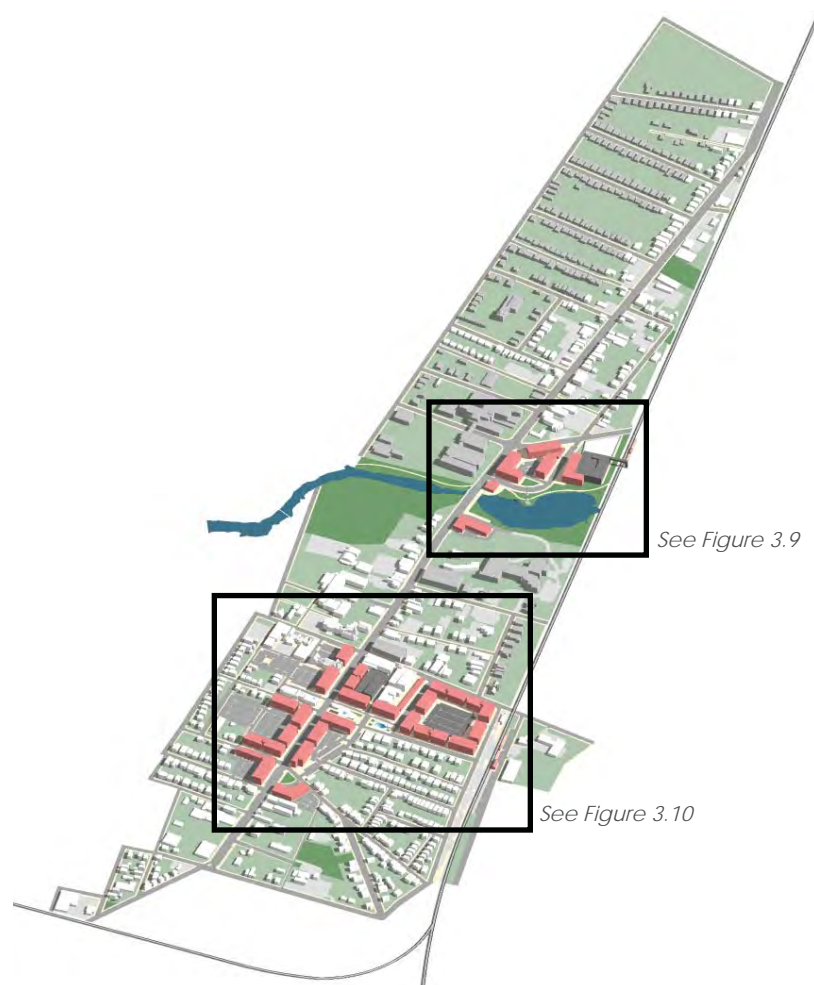


Figure 3.8: Vision for downtown Woodbury

3.2 The Vision

The vision statement presented below is a written and visual depiction of how downtown Woodbury will look and function when redeveloped. It consists of a general vision statement applicable throughout the area. It provides more detailed descriptions and imagery applicable to each of two sub-areas: the North Broad Street Corridor Redevelopment Area and South Broad Street Corridor Redevelopment Area.

The vision statement serves two important purposes. First, it provides a vivid description of the streetscapes, buildings, public plazas, parks and gateways. It is intended to be clear and unambiguous so that current and future decision-makers, residents and developers all have a comprehensive and level understanding of the City's expectations. Secondly, it is the foundation for the zoning regulations and design standards governing future development in downtown Woodbury.

The vision statement reflects the collective input of those who live and work in Woodbury. It responds to the ideas, preferences and opportunities for redevelopment expressed by City residents, workers and leaders during public workshops and stakeholder interviews conducted in 2009.

3.2.1 *Downtown Vision Statement*

Downtown Woodbury has re-emerged as a vibrant center of activity and desirable place to live. The streets are bustling with pedestrians who are shopping, going to and from work and socializing. Local retailers and restaurants thrive on Broad Street. Revitalization and redevelopment have contributed to an increase in jobs and commerce and this in turn has strengthened the community's tax base. People residing on Broad Street in new condominiums, townhomes, elevator buildings and flats above retail provide a built-in customer base for shops. A strong residential presence provides "eyes on the street." This contributes to a renewed sense of safety which, in turn, attracts local and regional visitors who patronize the shops and restaurants on a 24/7 basis. New offices attract workers who frequent local shops and restaurants during lunch breaks and after work. New and improved public parks and plazas draw residents and visitors who socialize, recreate, attend community events or simply relax. "Gateway" features are provided at key

locations around town. These are unique and attractive visual markers, such as signs, roundabouts, public art, hardscaping and/or landscaping that announces one's arrival into the City limits and its lively downtown.

New and diverse housing options are available in Woodbury. The condominiums, townhouses, elevator buildings and flats above retail provide diverse market rate units to supplement existing traditional single-family homes. These units attract young professionals, young couples, and empty nesters looking for easy-to-maintain, convenient and contemporary housing options.



Figure 3.9: Vision for North Broad Street Corridor

Downtown Woodbury's renaissance is accompanied by a comprehensive, interconnected system of streets, paths, sidewalks and bicycle lanes. New local streets parallel to Broad Street divert local trips off of the State-controlled road, reducing traffic pressures. The integrated street network creates alternative routes for vehicles and pedestrians. These improvements enable Broad Street to function as a two-lane main street with on street parking within the City's center, thus reinforcing its pedestrian friendly nature.

Broad Street has become a designated bicycle route, with bicycle lanes clearly marked and visible. Key intersections are redesigned to make crossings safer for pedestrians. "Bulb-outs" are installed at select intersections on Broad Street and within new streets to reduce the crossing distance for pedestrians

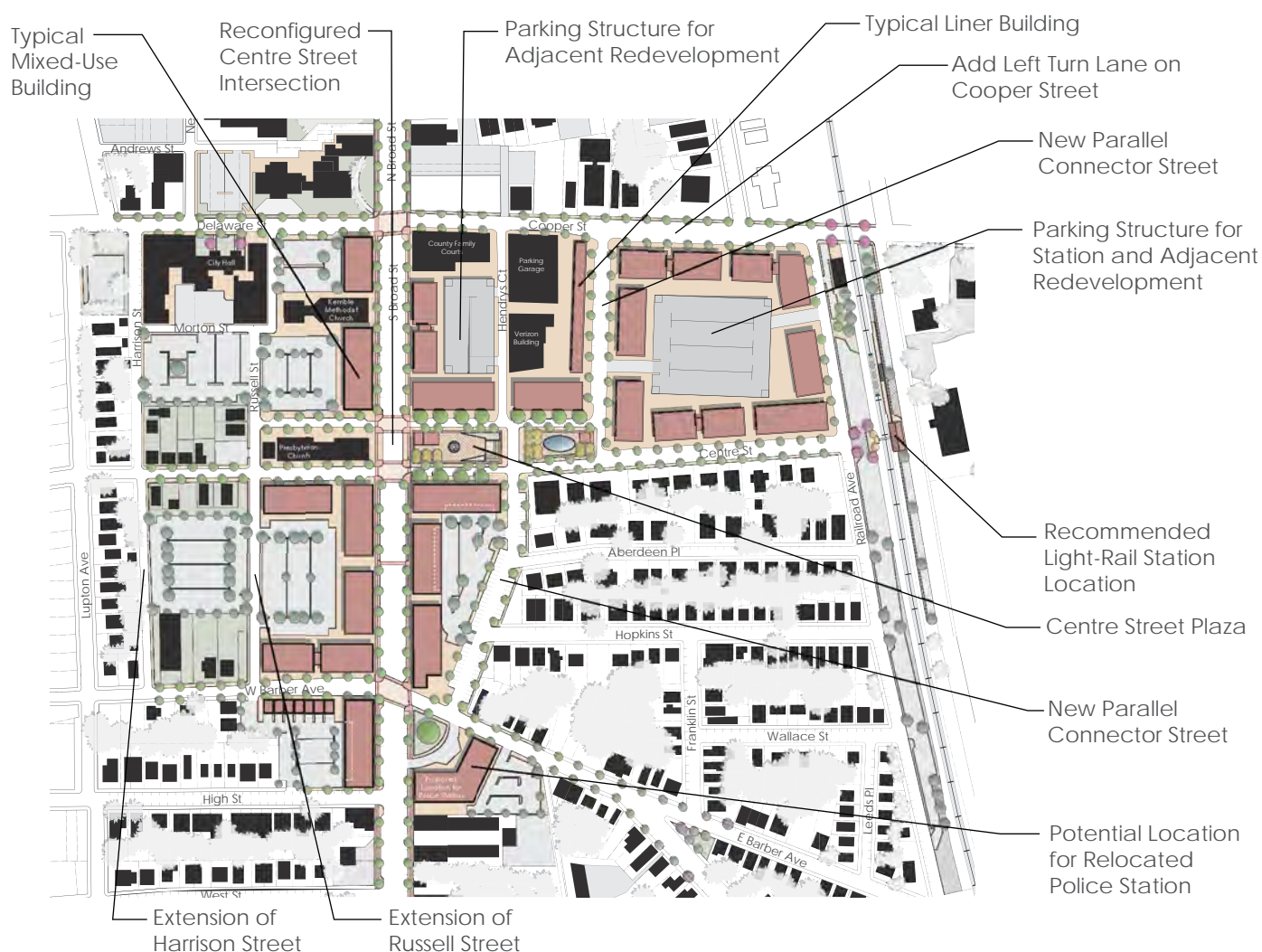


Figure 3.10: Vision for South Broad Street Corridor

within the travel lanes. Pedestrian paths and sidewalks are wide and well lighted, with attractive hardscaping and stately street trees and provide convenient connections to different activity centers. These improvements create a safe and enjoyable experience for people strolling through the City, or commuting from the new light rail transit stations into town.

Preservation of traditional urban form is an important City priority. As such, building setbacks, rooflines, fenestration, scale and materials are respectful of the traditional look and feel. Design standards are carefully crafted to achieve a desired result and avoid attempts at mimicking history with superficial façade treatments.

Building form, height and setbacks contribute to a pleasing pedestrian streetscape. Architectural features are designed to human scale with appropriate use of vertical and horizontal banding and articulation. Publicly visible building facades have plenty of windows and openings to break up what could otherwise be monotonous building walls. Building setbacks from the street respect the pattern of shallow setbacks that have existed historically along Broad Street and adjacent neighborhoods. Modestly-scaled but open, airy plazas form pleasant sitting and gathering areas between commercial and mixed use buildings.



Figure 3.11: Vision for Broad Street (not to scale)

New construction responds to modern day needs of commercial and residential construction. Obsolete and oddly shaped building lots are consolidated to enable the construction of functional floor plans appropriate for commercial and mixed use buildings. This is critical to successful economic revitalization. Redevelopment occurs incrementally, scaled and sequenced to respond to the building market.

State of the art environmental guidelines encourage water conservation and energy efficiency in new or restored commercial and mixed use buildings. This brings added value to developers, owners and tenants. People now realize that despite the upfront cost, there is a long term economic benefit realized through "green" design.

The City has become a pedestrian and bicycle friendly community of place. Furthermore, light rail commuter service from Camden to Glassboro has been restored, servicing Woodbury at two station stops. Pedestrian, bicycle and rail service, coupled with improved bus transit are convenient modes of transportation for local trips and commutes to work, respectively. While this has reduced reliance on the car, the



Figure 3.12: Typical gateway (not to scale)

automobile continues to maintain a presence downtown and as such, parking must be accommodated. Structured parking facilities are either located behind buildings or are wrapped around with retail on the first floor.

Surface parking lots are convenient to offices and stores for employees and shoppers alike but are located behind buildings to minimize their visibility from the street. They are accessible through new connector streets that avoid the need to travel through residential neighborhoods and park on residential streets. Interconnected parking lots reduce the number of curb cuts taken from the street and offer shared parking to reduce the number of parking stalls. Lots are well lighted and landscaped. The rear façade of mixed use and commercial buildings residing on the parking lot have a similar ratio of windows and entrances to that of the front façade. This enhances the visual appearance of the parking lot and provides easy access for the shopper or resident from the parking lot to the building. Such rear façade treatments also generate more “eyes on the street”, thus enhancing the perception of safety. Moreover, the parking lots offer open, safe pedestrian passage to the street via connector paths that are well lighted and landscaped.

3.2.2 North Broad Street Corridor Redevelopment Area

The northern Redevelopment Area is bounded by the railroad tracks to the east, Tatum Street to the west, Progress Avenue to the north and Lake Park to the south. Lake Park has become a major destination for social and recreational activities throughout the week. The lake is one of the City’s principle recreational amenities. Commuters arriving in Woodbury at the light rail station near East Red Bank Avenue can follow a newly created multi-purpose pedestrian/bicycle path that links



Figure 3.13: Cross-Section of Potential Redevelopment between Cooper and Centre Streets
The change in grade between the new parallel connector street and the railroad station may allow for multi-story parking accessed on both levels.

the rail station to Broad Street via Lake Park. Moreover, the Park is a link within a larger, greenway system, containing a continuous multi-purpose path that connects parks and green spaces west of Broad Street to the light rail station and parks further east. Employees of the medical offices in mixed use office/retail buildings immediately north of Lake Park take full advantage of the Park during lunch hours. Restaurants with outdoor seating and recreation-oriented shops have replaced the Police Station which has been relocated to a site south of Barber Avenue. These shops and restaurants attract employees of the Hospital and medical arts building during the week, and residents and visitors on weekends.

The North Broad Street Corridor and area immediately east have become far more interconnected and pedestrian-friendly as a result of street and intersection improvements. Green Street now extends south of East Red Bank Avenue, intersecting Broad Street near Lake Park. The extended Green Street parallels Broad Street and enables vehicular trips to retail and office destinations within the neighborhood to avoid Broad Street.



Figure 3.14: Vision for Red Bank Transit Hub (not to scale)

3.2.3 South Broad Street Corridor Redevelopment Area

The southern Redevelopment Area includes the area bounded by the rail line to the east, Harrison and Russell Streets to the west, Delaware and Cooper Streets to the north, and Barber Avenue to the south.

A new parallel road has been constructed one block east of Broad Street. It extends from East Barber Avenue to Cooper Street. Three former intersections at Broad Street now terminate at the new parallel road, reducing the number of intersection crossings on Broad Street that must be made by pedestrians and reducing vehicular traffic in residential neighborhoods. West of and parallel to Broad Street are two new streets, each connecting Delaware Avenue to W. Barber Street. These parallel road systems remove local trips from Broad Street and provide more direct and convenient pedestrian connections between homes, shops, civic buildings and parking areas.

East Centre Street has become a major pedestrian corridor because it functions as the main connection between a second light rail station and Broad Street. New commercial and mixed use development is situated along East Centre Street, generating additional foot traffic. One of the principle destinations for foot traffic in the southern Redevelopment Area is a grand public plaza that has been constructed along E. Centre Street. The plaza extends the length of one full block east from Broad Street. This 20,000 square foot plaza provides a multi-purpose community gathering area for all ages. In addition to providing an open air café, it is also programmed with activities throughout the week in every season, including a farmers market, movie screenings, ice skating, musical performances, and special community events. It offers space for active and passive recreation. It also provides indoor facilities, information kiosks and restrooms. The plaza is bounded by streets on all sides, and is highly porous to the surrounding mixed commercial and residential neighborhood.



Figure 3.15: Vision for Centre Street Plaza

3.3 Goal & Objectives

To fulfill the above future vision for the Business District and guide Redevelopment, a goal and series of objectives were established:

Goal: *Redevelopment shall facilitate sustained economic, social and environmental health and vitality in downtown Woodbury.*

Long term sustainability demands a careful and thoughtful approach to redevelopment, one that responds to the community's vision. It demands a strategic approach to public and private sector investment in infrastructure and development. In so doing, it builds upon community assets – human capital, built environment and natural environment – to create a holistic and lasting place. A place that is never stagnant.

A. Objective: Use density, a mix of uses and public spaces as building blocks to transform Broad Street into an active, vibrant main street corridor.

Density coupled with mixed use generates exciting destinations with a range and critical mass of activities that occur on a 24/7 basis. Sufficient density in the form of multi-family housing options and flats above ground floor retail provide an accessible consumer base that helps support local businesses on Broad Street. Moreover, residents in these units serve as “eyes on the street,” which heightens a sense of safety and security for pedestrians and businesses alike. This generates even more pedestrian activity, brings more business and shops, and builds needed confidence among business owners. Porous, visible, and highly accessible public spaces and plazas provide gathering places for people along Broad Street. Programmed activities ensure that these spaces are activated during the day and evening, workdays and weekends. These public space amenities facilitate social interaction among residents and visitors of all ages.

B. Objective: Expand the inventory of housing options to meet the needs of a diverse population.

Woodbury has the diversity of households that is representative of statewide and national trends. Married couples with children comprise a small portion of households (18.5%). Thirty-two percent (32%) of total households have children under the age of 18 while 68% of households are without children, half of which are single-person households. Even so, over half of the housing stock in the City (56.3%) consists of single-family detached units. Rising energy prices coupled with a regional and national trend toward urban living suggests the need for alternative housing options to meet changing housing needs and preferences. Condominiums, townhouses, elevator buildings and flats above retail will fill an existing gap. These new forms of owner-occupied and rental units will attract young single professionals, young married couples and empty nesters, creating a more diverse resident population in the City's downtown.

C. Objective: Capitalize on existing community assets to strengthen the City's position in the regional economy.

Woodbury is well-positioned to benefit economically by virtue of its place within the region. County government facilities maintain a strong presence on Broad Street, a reminder of the City's role as the County Seat of Government. Underwood Hospital is a significant regional employer that is also situated on Broad Street. Proximity to a major interstate highway (Route 295) coupled with the reinstatement of a light rail line linking Woodbury at two future station stops to Camden and Glassboro, offers tremendous regional accessibility. By capitalizing on these assets, redevelopment will attract more business to the community. Employees of these and other institutions will remain in the City at the end of the workday to take advantage of the downtown amenities. Redevelopment will also fuel the creation of a medical district with new office space, generating a larger employment base and more business for local retailers. Visitors will enjoy Woodbury's amenities in the evenings and on weekends. An expanding employment base, thriving local businesses, and

accessible transportation system will not only serve local residents but strengthen the City's regional draw.

D. Objective: Maintain a highly interconnected, multi-modal street network that is legible, porous and safe.

A multi-modal, integrated system of streets will enhance the Broad Street corridor while simultaneously reducing traffic in residential neighborhoods and encouraging healthier modes of travel—those taken on foot or bicycle. It will offer multiple and direct travel routes linking key destinations and activity centers, including the future train stations, public spaces, employment centers and shopping destinations. Parallel streets, located behind Broad Street, will divert local vehicular trips, reducing traffic pressures on Broad Street and making the corridor more desirable for pedestrian use. These local streets, moreover, will have a legible system of shared parking lots that are easily accessible to the commercial and mixed use buildings that they serve. Trips made to these commercial and mixed use buildings can avoid adjacent residential neighborhoods, reducing traffic impacts and competition for on-street parking. Sidewalks and multipurpose paths coupled with traffic calming measures will contribute to a safer pedestrian experience on Broad Street and interconnecting roads. At the same time, designated bicycle lanes situated on Broad Street will entice more people to travel to work or run small errands. Increased pedestrian and bicycle mobility in turn, will contribute to enhanced public health, as more people choose these modes of travel for many of their local trips. Facilities for the parking of bicycles will be provided throughout the Redevelopment Area.

E. Objective: Accommodate contemporary building needs while respecting traditional urban form.

Woodbury's existing urban fabric is defined not only by its street grid but also by building form. This includes the relationship of buildings to the street, and their massing, modulation, fenestration, and scale. Maintaining this urban fabric is central to preserving Woodbury's

character and unique sense of place. However, economic realities and revitalization demands the ability to accommodate contemporary building and circulation needs within the context of a traditional urban form. A close examination of numerous existing building lots and structures reveals numerous oddly shaped, obsolete lots and building floor plates. This presents a significant challenge to attracting modern rentals to many of these existing building lots. By the same token, the City's downtown presents an opportunity for lot consolidation and for contemporary building solutions that allow for new construction while still maintaining traditional forms. Shallow setbacks of buildings from the street, parking lots that are accessible but visually screened behind the buildings, utilization of façade articulation, appropriate façade and roof materials, fenestration and scale will achieve this desired outcome, while accommodating today's commercial needs.

F. Objective: Create a unique sense of place in downtown Woodbury.

Too often, we explore a place and come to the conclusion that "there is no there there." This is especially true of "sprawling" communities where one subdivision after another and strip malls seem to continue endlessly and where the boundary of one municipality is entirely indistinguishable from the next. Fortunately, Woodbury is in a different position. As a County Seat, it is one of only 21 municipalities statewide carrying that status. Woodbury is diverse in its population and workforce. It has a downtown whose traditional urban form is clearly distinguishable from suburban communities beyond the municipal border. Maintaining and enhancing its distinct character is an important priority to the City. Attention to streetscape, building form and public spaces will be extremely important. Establishing gateway features at key entry points to the City's downtown will draw further attention to the fact that one is entering a distinct and memorable place. How this is accomplished through the redevelopment process will be a combination of public and private sector actions, including implementation of a new type of zoning ordinance known as a Form-Based Code.

4.0 | THE PLAN

4.1 Overview

The *Regulating Plan* described below and its implementing Code (see Section 6) are the blueprint for the density, mix of uses, circulation and parking, and public spaces required to create a vibrant, sustainable business district in the City of Woodbury.

The *Regulating Plan* allows for up to 1.5 million square feet of development throughout the Redevelopment Area. While many factors will influence the mix of uses, the Plan offers a suggested blend of dwelling units and commercial square footages that will achieve the vision. The goal of the ultimate build-out is to introduce approximately 800 housing units, 280,000 square feet of office, and 220,000 square feet of ground-floor retail in the specific redevelopment sites identified in the Plan. This strategy would accommodate approximately 1,200 new City residents to help to ensure that Broad Street is active and vibrant through the evenings and weekends, to support local businesses and to contribute to the employment base in town.

Redevelopment is focused into two primary districts within the Redevelopment Area. Their locations are heavily influenced by both the traditional “downtown” of the City and by the opportunities presented by the two proposed rail stations at Red Bank Avenue and Cooper Street. Proposed redevelopment along North Broad is anchored by Underwood Memorial Hospital across the street, and adjacent to Stewart Lake. While the Hospital complex is one of the taller, more dense configurations of buildings in the City, the current arrangement on the redevelopment site is primarily auto-



Figure 4.1: Vision for downtown Woodbury

oriented, one-story retail uses. Redevelopment along South Broad is situated to link the proposed rail station with the downtown. Redevelopment sites on North and South Broad Street are organized to create a series of development “pods” that allow for phased redevelopment.

This is a long-term plan for growth and redevelopment for the City. It is not anticipated to be implemented all at once. While it is intended to adhere to the *Regulating Plan* and *Form-Based Code*, there is enough flexibility built into the Plan and Code to enable some deviation while still respecting the overall intent. The Plan is designed to evolve over time. As public and private sector proposals are presented, they will be measured against this Plan and its objectives.

Sections 4, 5, and 6 cover all aspects of Redevelopment Plan implementation. Section 4 begins with the *Regulating Plan* which collectively comprises five plan components (see Sections 4.2). Section 5 follows with a discussion of a four-prong implementation strategy. Administrative and procedural aspects of plan implementation are also covered in Section 5. Section 6 is a Form-Based Code to guide implementation.

4.2 Regulating Plan

The *Regulating Plan* contains five elements including *Land Use/Transect Plan*, *Street Regulating Plan*, *Public Spaces Plan*, *Gateway Plan* and *Infrastructure Plan*. These are to be used in conjunction with the *Form-Based Code*, found in Section 6, to guide redevelopment in the downtown business district.

4.2.1 Land Use/Transect Plan

The *Land Use/Transect Plan* establishes three transects and seven frontage types (*Figure 4.2* and *4.3*). Transects dictate permitted uses and building types, while public frontages determine site planning and architectural considerations. Regulations governing these considerations are contained in the *Form-Based Code*.

4.2.2 Transects

The *Land Use/Transect Plan* for Woodbury is comprised of three transects: Downtown, Live-Work, and Neighborhood. Downtown is the most intense of the three, representing the areas of greatest density. Most of the Redevelopment Planned for the City is contained within the Downtown Transect. The Live-Work Transect acknowledges the transition area between the Downtown and the edge neighborhoods in the City. Finally, the Neighborhood Transect helps to conserve the existing character of the residential community that surrounds and supports the Downtown. Each Transect works with adjacent transects to transition between densities through the use of rules determined by Street Frontages.

- A. Downtown Transect:** The Downtown Transect is intended to create enough density to support a vibrant, commercial “Main Street” along South Broad and to support rail service at both proposed passenger rail stations. There are two non-contiguous areas within this transect, each of which effectively creates a redevelopment activity node. These are North Broad at Red Bank Avenue and South Broad between Delaware/Cooper Streets and Barber Avenue. The location of each area within the Downtown Transect responds to existing nodes of activity and reflects existing commercial and professional office overlay zoning. Intended to serve as the City’s downtown shopping, professional office, medical, cultural and entertainment district, the Downtown Transect has the highest density, and consists of mixed use buildings up to five stories in height. This responds to the need to draw a critical mass of housing units and office space to the downtown area. Building height, moreover, reflects the precedent set by Underwood Memorial Hospital at Red Bank Avenue as well as the Green Block Building at Broad and Center Streets. Mixed use buildings are encouraged to host ground floor commercial uses with upper level residential and offices. The plan is flexible enough that the actual mix or upper floor uses may be determined by the market. All buildings are required to face street frontages and, through the requirement of storefront windows and entryways, to directly engage the sidewalk. This is designed to promote walkability and, in turn, activate the street environment and



Figure 4.2: Transects

bring with it all its associated benefits of social and economic vitality. Similarly, to maintain continuous building facades along street frontages, parking is restricted to the rear of buildings.

- B. Live-Work Transect:** The Live-Work Transect is primarily an urban residential district, though it also reflects the existing pattern of converted homes to retail and professional office uses that is evident along North and South Broad as well as on streets like Cooper, Newton, and Euclid. It takes a cue from the classic model of shopkeeper apartment above the retail store. This transect is a mid to high density area, consisting largely of attached residential building types with some ground floor neighborhood commercial uses. The Live-Work Transect allows buildings to be set back from the street, respecting existing setbacks, but still prohibits front yard parking. Side-yard parking is permitted, while rear-yard parking is encouraged.
- C. Neighborhood Transect:** The Neighborhood Transect conserves sections of the City's residential communities that should be retained during redevelopment. The building types permitted in the transect primarily reflect the current lower density neighborhood pattern, but also reflect some increased density to support the business district as properties redevelop in the private market.

4.2.3 *Frontage Types*

- A. Downtown Broad Street:** Downtown Broad Street is the principal spine of the downtown commercial and governmental area. It is both a local main street and a regional arterial. The street must accommodate large volumes of traffic, but equally important, the streetscape must accommodate a high level of pedestrian activity. In order to create a comfortable, well-proportioned and well-designed streetscape for the pedestrian, the grand scale of the street, coupled with the desire for heavy pedestrian usage, necessitates wide sidewalks, generous building heights, high quality façade materials and design, and minimal to no front yard setbacks from the street. To maintain an uninterrupted streetscape, there are no front

yard parking lots – all parking lots are required to be located to the rear of the buildings. On street parking is provided.

- B. **Uptown Broad Street:** This frontage type is the primary spine of the City's commercial and medical area on North Broad Street. Similar to Downtown Broad Street, the streetscape accommodates a high amount of pedestrian activity and reflects similar sidewalk, building façade, setbacks and parking considerations.
- C. **Collector:** Collector roads serve as the primary feeders to Broad Street. On-street parking is provided. Existing right of way constraints limit the size of the pedestrian realm. Nevertheless, the streetscape is intended to accommodate a fair to high amount of pedestrian activity. Thus, new development should provide the opportunity for additional pedestrian space. The plan encourages moderate to generous building heights, high quality façade materials and design, and shallow front yard setbacks. Off-street parking must be set back from the front building façade, generally located to the rear of the building.
- D. **Special:** This frontage type applies to the pedestrian oriented streets that will be added to the existing street grid and existing streets that will be substantially modified as a result of redevelopment. These master planned streets will have streetscapes providing for a moderate to high degree of pedestrian activity. Wide sidewalks will have high quality materials and design. Buildings will have generous heights, high quality façade materials and design, and shallow front yard setbacks. Off-street parking is restricted to the rear of buildings.
- E. **Service:** Service frontage accommodates the parking-oriented streets that will be added to the street grid. These master planned streets are to have 90 degree head-in on-street parking. While the pedestrian realm is scaled down to accommodate such parking, Service streets provide for a high quality pedestrian experience. This is done by providing for moderate to generous



Figure 4.3: Frontage Types

building heights, quality façade materials and design, shallow front yard setbacks and screened front yard parking.

- F. **Local:** The Local frontage type applies to existing residential streets that have minimal to moderate through traffic and will be largely unchanged through redevelopment. The streetscape provides for minimal to moderate pedestrian activity. New development introduces varied building heights, high quality façade materials and design, varied front yard setbacks and front yard parking.
- G. **Small:** The small frontage type applies to existing alleys and residential streets with extremely narrow rights of way and new alleys that will be provided to service redevelopment. Their design constrains pedestrian opportunities. New development provides for varied building heights, moderate setbacks and screened parking.

4.3 Street Regulating Plan

The *Street Regulating Plan* addresses the need for the roadway network and parking network to work in tandem. It is not the goal to create a system that gives priority to car and truck traffic over the pedestrian. The intent of the *Street Regulating Plan* is to ensure that all users are accommodated equally in order to maintain a successful business district. Therefore, the roadway network must be accessible, interconnected and feed designated parking areas, creating a circulation system that is predictable, and easy to navigate and locate for all users.

For the street network, the *Street Regulating Plan* creates a street hierarchy that is a subset of the Frontage Types identified in the *Land Use/Transect Plan*. The *Street Regulating Plan* identifies roads that will likely remain unchanged by redevelopment as well as those that would require or benefit from reconfiguration. The proposed classification system acknowledges both of these road types, and establishes the ideal street cross section and pedestrian realm. While the classification system addresses general conditions across the Redevelopment Area, the *Street Regulating Plan* also proposes site-specific intersection improvements and key roadway enhancements.

4.3.1 New Roadways

The circulation system in the Redevelopment Area allows for continued movement along Broad Street but also provides for a slower moving parallel and perpendicular circuit around and across Broad Street. This system is proposed along both North and South Broad to different extents. A parallel network provides connectivity, diverts local traffic off of Broad Street, and offers access to a series of interconnected parking lots that serve redevelopment sites. Important to a parallel road network is clarity and predictability. The system must be easily navigated by experienced and new users to maintain the convenience of living, working, and visiting Broad Street. Below is a description of proposed new roadways:

- A. East of Broad Parallel Streets:** A parallel street network is critical to the successful implementation and revitalization of South Broad Street. It includes creating a new street from Cooper Street to East Barber Street as necessary to accommodate parking access. The new parallel street is to be located on the east side of South Broad Street in response to both existing traffic movement and a desire to reduce traffic impacts on the residential neighborhood centered on Hopkins Street. From Cooper Street, the parallel connection needs to



Figure 4.4: Rendering of Proposed Parallel Connector Road off of Cooper Street

be situated far enough to the east to be clear of the stacking at the light at Broad Street and allow for left turns both into and out of the new street. From Barber Street, the primary factor in locating a parallel street was to minimize adverse impacts on the adjacent residential neighborhood. Parking is planned for this stretch, from Centre to Barber, to increase the availability of parking close to Broad Street.

The parallel connector system east of Broad allows portions of Curtis, Aberdeen, and Hopkins to be vacated. This strategy serves several purposes. For traffic, it reduces the number of intersections where turns can be made onto and off of Broad Street, facilitating a more consistent flow of cars and reducing crossings for pedestrians up and down the street. Vacated street portions create opportunities for more continuous building frontage along Broad Street, accommodating modern retailers and upper floor uses more easily. The configuration also reduces the convenience of “cutting through” the adjacent neighborhood when traffic is heavy along Broad Street, keeping residential streets safer.

- B. East Centre Street Plaza:** The Centre Street Plaza is the nexus of the parallel street network. It serves to not only create the full parallel connection from Cooper to Barber, but also provide access to Broad Street, while creating a significant public space for the community. The Plaza acts as a rectangular traffic “circle” with a one-way network of streets to split traffic and allow multiple movements through, around, and out to Broad Street. Centre Street is currently signalized at Broad Street. It is proposed that the one way system can operate as a single signal. As such, Center Street will become a primary access point to parking facilities on both the westerly and easterly sides of Broad Street.
- C. West Centre Street Extension:** To complement the East Centre Street Plaza and improve intersection movements at Broad Street, the new westbound Centre Street is extended across Broad through to Russell Street. This configuration eliminates the need to turn left onto Broad in order to continue west on Centre Street. It also enhances the connectivity and clarity of the parallel street network and creates direct linkages between parking on either side of Broad Street. This

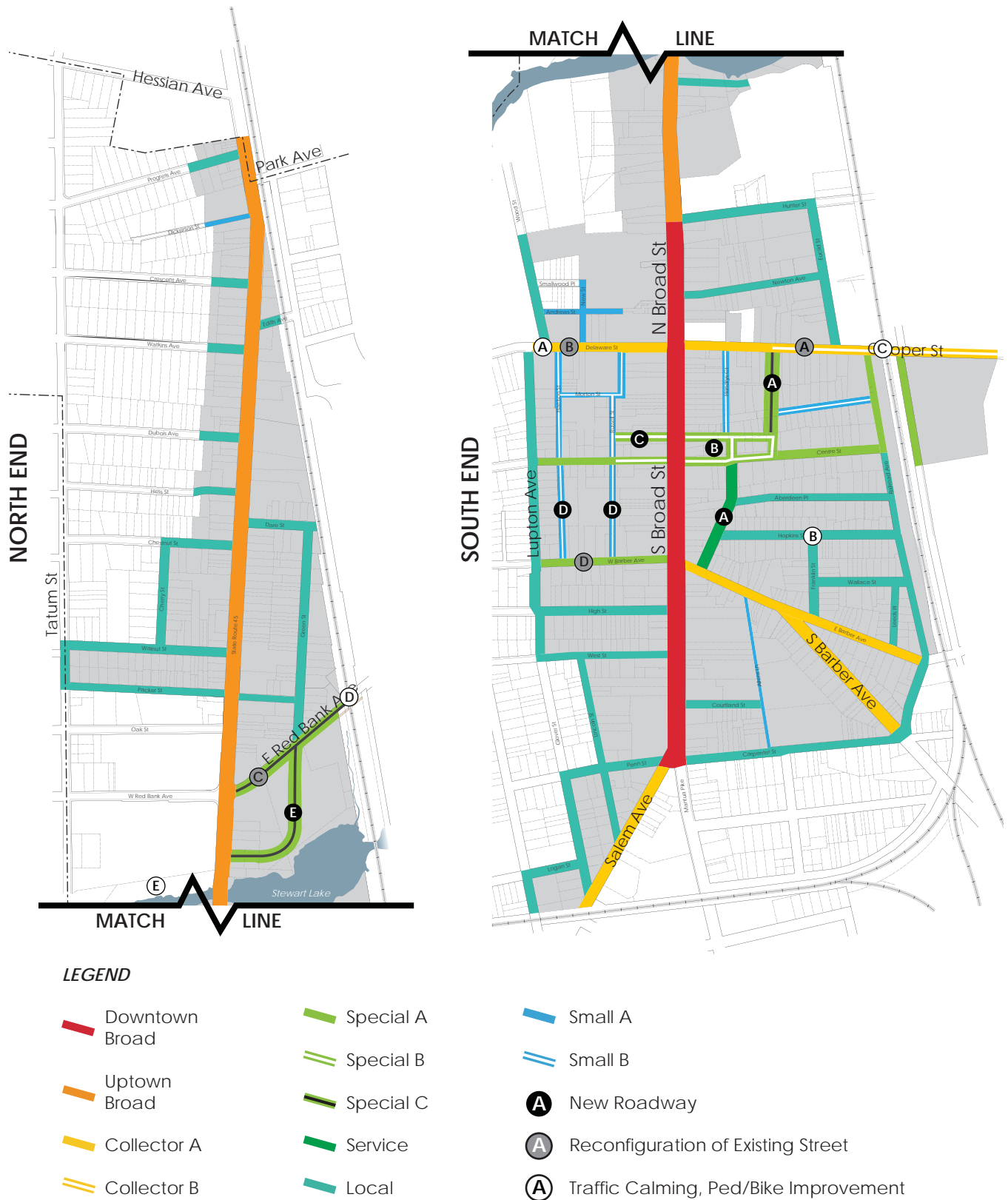


Figure 4.5: Street Regulating Plan Map

layout also allows for one traffic signal to regulate the intersection instead of two in close proximity. Crosswalks at these intersections are safer as well, with less traffic turning into the path of pedestrians.

- D. **Russell and Harrison Street Extensions:** To improve connectivity, the plan proposes utilizing the existing parallel alleyways between Delaware and Centre Streets and extending them south towards Barber Street. This improvement will facilitate ease of movement between parking lots, improving the wayfinding abilities of users trying to navigate the parking network. These alleyways also help to mark the transition between the taller, mixed-use buildings along Broad and the residential neighborhood to the West. Harrison Street can also provide rear-access to the properties facing Lupton Street, reducing the need for curb-cuts and front-loaded garages.
- E. **Green Street Extension:** The Redevelopment Area on the Southwest corner of Broad and Red Bank Avenue currently relies on curb-cuts, and private driveways to reach the interior of the block. Parking lots are somewhat interconnected and a private sloping access drive along Stewart Lake connects the CVS to Broad Street. In order to better utilize the entire site, provide public lakefront access, and improve connectivity, redevelopment should be anchored around an extension of Green Street south through the site and curving west towards Broad Street. The resulting intersection with Broad will likely be restricted to “right in, right out” turn movements to avoid conflict with the intersection at Red Bank.

4.3.2 *Street Reconfigurations*

The following are reconfigurations of existing streets:

- A. **Cooper Street Right-of-Way Widening:** Redevelopment of the properties along the south side of Cooper Street between the parking garage and the eastern extent of the Redevelopment Area should incorporate an expansion of the right-of-way. Expanding the existing right-of-way by 28 feet will allow for a left-turn lane into the new parallel street, retain the on-street parking, and create a generous, yet necessary, 20-foot wide sidewalk on the south side of the street. By intercepting

cars before they reach the Broad Street/Cooper Street intersection, the new parallel street will remove traffic from Broad Street and improve the level of service at the intersection.

- B. Delaware Street Re-Striping:** Utilizing the existing right-of-way and removing on-street parking on Delaware Street in front of City Hall will create enough room to add a left turn lane in the westbound direction onto Harrison Street. This connection provides an entrance to the parallel street network at enough of a distance from Broad Street to reduce conflicts with stopped traffic at the intersection.
- C. East Red Bank Widening:** Redevelopment along North Broad should accommodate widening the south side of the Red Bank Avenue right-of-way between Broad and the rail underpass. An 80-foot right of way will accommodate on-street parking on the south side, a turn lane to access Green Street in both directions, and an opportunity to widen the rail underpass.
- D. West Barber Street Widening:** As redevelopment occurs on either side of West Barber Avenue, between Russell Street and Broad Street, the right of way should be expanded to 60 feet. This dimension will ultimately accommodate on street parking on the north side of the street, as well as two travel lanes and a left turn lane onto Broad Street.

4.3.3 *Traffic Calming and Pedestrian and Bicycle Connections*

The following are traffic calming and pedestrian and bicycle connection improvements:

- A. Delaware Street at Harrison Street Traffic Calming:** As drivers approach the Business District along Delaware Street, coming out of the curve at Lupton Street creates a sudden transition for drivers into a much more urban area. While the speed limit is 25 miles per hour for the entire stretch, the need to be alert for slowed traffic and crossing pedestrians intensifies at Harrison Street. Installing textured pavement at this intersection will signal the approach to the Business District and alert drivers to the changing environment.

- B. Hopkins and Franklin Neighborhood Traffic Calming:** To lessen potential impacts from increased car traffic through the neighborhood between Broad Street and Railroad Avenue, traffic calming strategies should be examined for the intersection of Hopkins and Franklin Streets. This may include introducing a traffic circle or speed table to slow traffic through this residential intersection.
- C. Rail Crossing at Cooper Street:** With the reintroduction of passenger rail service to Woodbury, pedestrian and bicycle traffic at at-grade crossings must be safely accommodated. At-grade access across the tracks at Cooper Street is essential not only for station use, but also for the overall connectivity of the City and movement on Cooper Street. If needed, pedestrian scaled warning lights and signs should be implemented to ensure safe crossing.
- D. East Red Bank Train Underpass:** Presently, the westbound direction of Red Bank Avenue under the rail bridge is the only side with any pedestrian accommodation. Pedestrians, however, can regularly be seen using the small shoulder and cart way of the eastbound lanes to connect to the adjacent retail and walk towards Underwood Hospital. The distance between traffic lights at Evergreen Avenue and Broad Street make crossing the street to use the westbound sidewalk difficult to anticipate. With redevelopment on the south side of the street and a proposed new passenger rail station, expanded pedestrian and bicycle accommodation will be necessary through the underpass.
- E. Pedestrian and Bicycle Path:** The City is currently planning a pedestrian and bicycle path along the waterfront that connects Stewart Lake Park to Wood Street. The path is planned to traverse the railroad tracks near the Lake, run west along the Lake, crossing Broad Street and connecting to an existing path between the Creek and the Hospital parking garage. This path connects to the baseball fields at the end of Frances Avenue and crosses the pedestrian bridge to Wood Street, potentially linking to the park system at Wood and New Streets. The new path segments and path improvements will not only improve connectivity between and access to

existing parks and open spaces, it will also help to promote the City's commitment to recreation and fitness.

The *Street Regulating Plan* sets forth specifications for all street cross sections in the Redevelopment Area. All development and redevelopment must provide the necessary right-of-way, cartway, sidewalk and planting strip improvements as prescribed by the applicable cross section. In the event that such improvements are not practical at the time of development or redevelopment, the Planning/Zoning Board may require an applicant to dedicate easements and/or contribute fees to the City for future improvements. The *Street Regulating Plan Map* identifies each Street Type (Figure 4.5), and is followed by a written description, illustrated cross section, illustrated pedestrian realm, diagram of pedestrian realm requirements and diagram of permitted street trees for each Street Type. The following illustration and diagram describes the components of a street right-of-way that are regulated by the *Street Regulating Plan*:

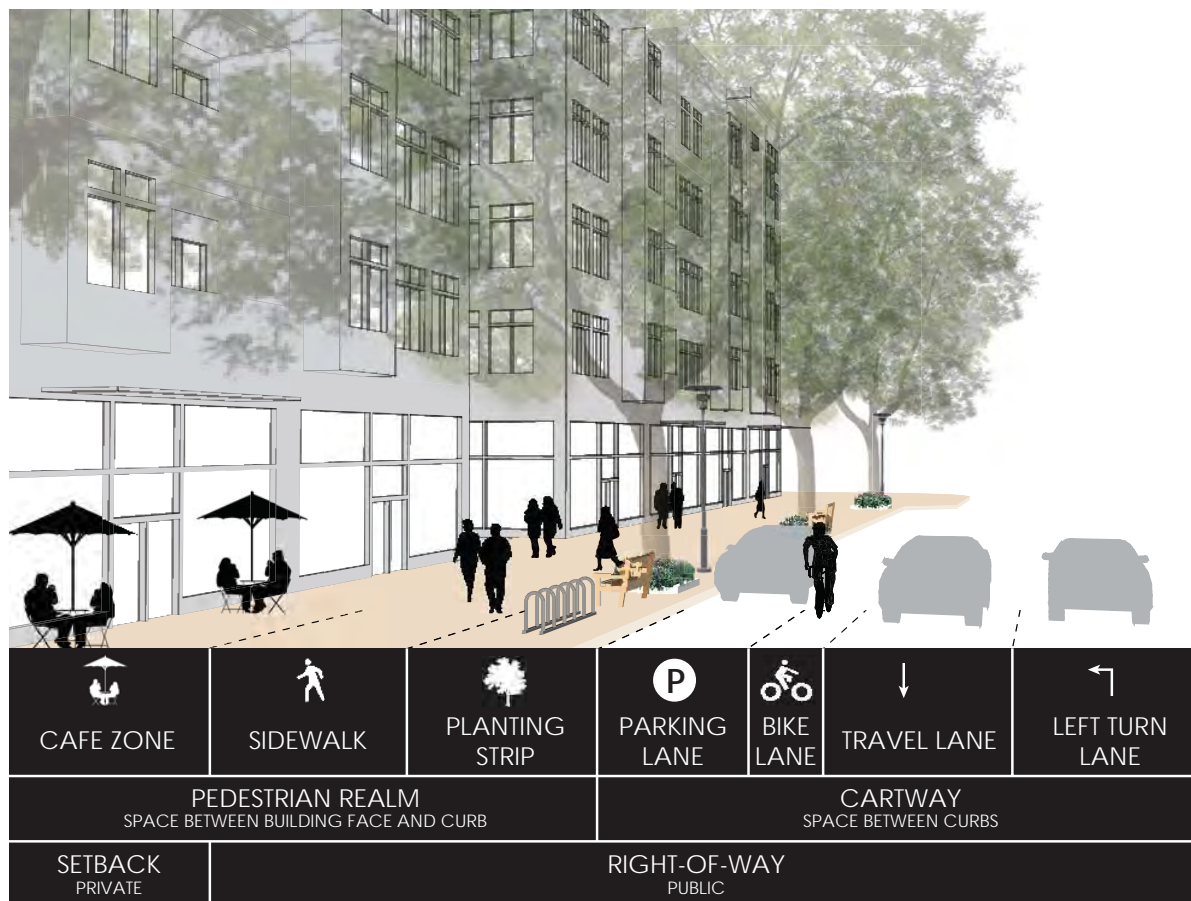
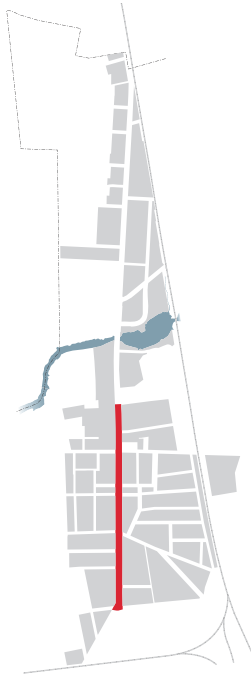


Figure 4.6: Components of a street right-of-way as regulated by the *Street Regulating Plan*

4.3.4 Street Types

The following Street Types comprise the hierarchy of the *Street Regulating Plan*:

DTB



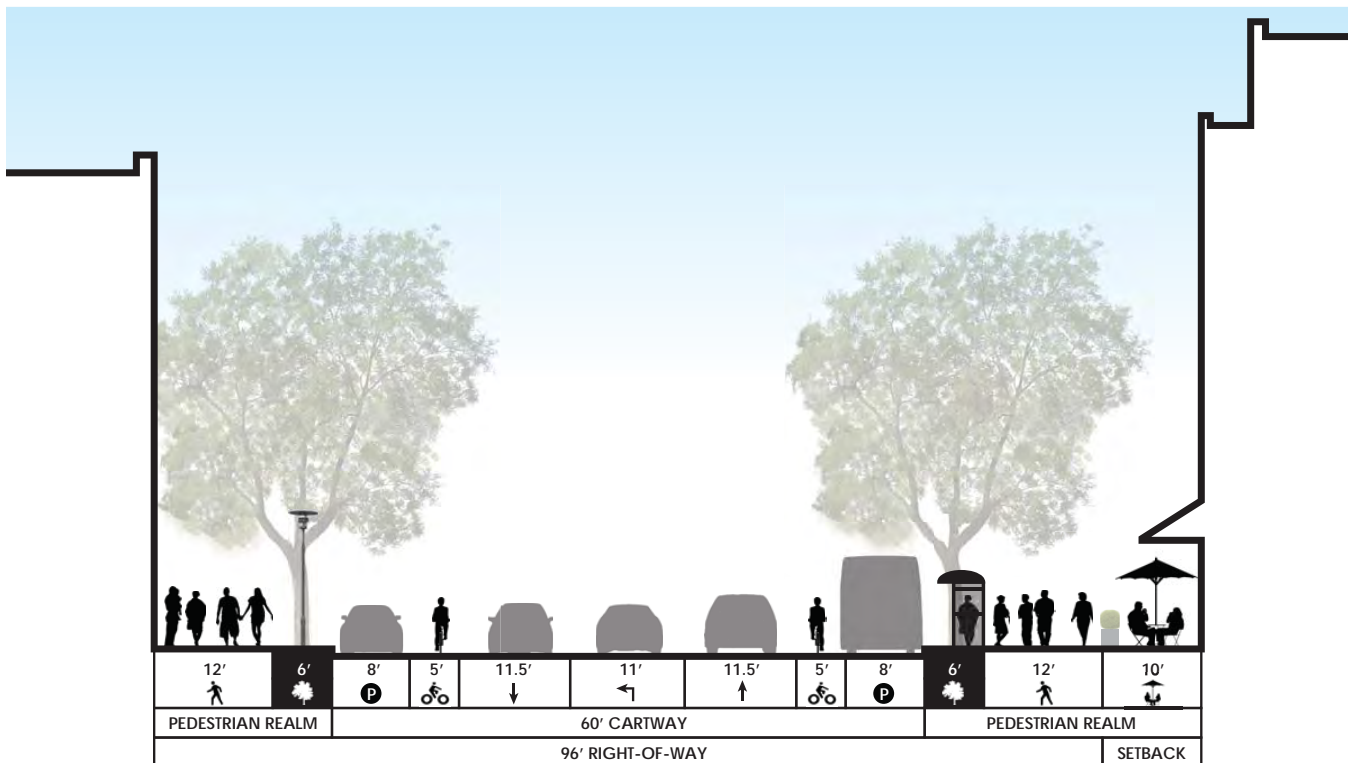
Street Type

4.3.4.1 Downtown Broad Street

A. Purpose/Definition

Intended to serve as the primary spine for Woodbury's downtown commercial and governmental area. As such, the cartway and pedestrian realm must provide capacity for a high amount of automobile and pedestrian traffic. This necessity, along with the grand scale of the street, warrants a wide cartway and sidewalks with high quality materials and design, zero curb cuts and no front yard parking lots.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



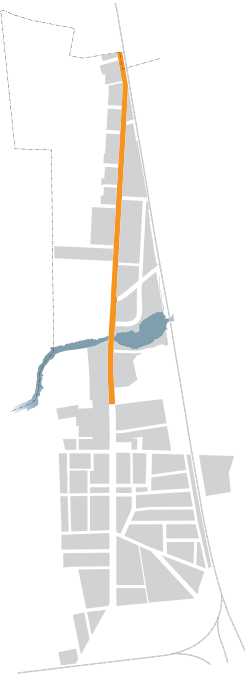
D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|----------------------------|
| 1 | Street Tree Spacing | Avg. 40' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 100 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Bus Stop |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak. In general, tree types may be alternated except between Carpenter and Barber Ave where Green Ash trees have been selected and between Barber and Delaware/Cooper where Willow Oak trees have been selected, both for specific intent (per Street Tree Plan in Section 4.4.1).

UTB



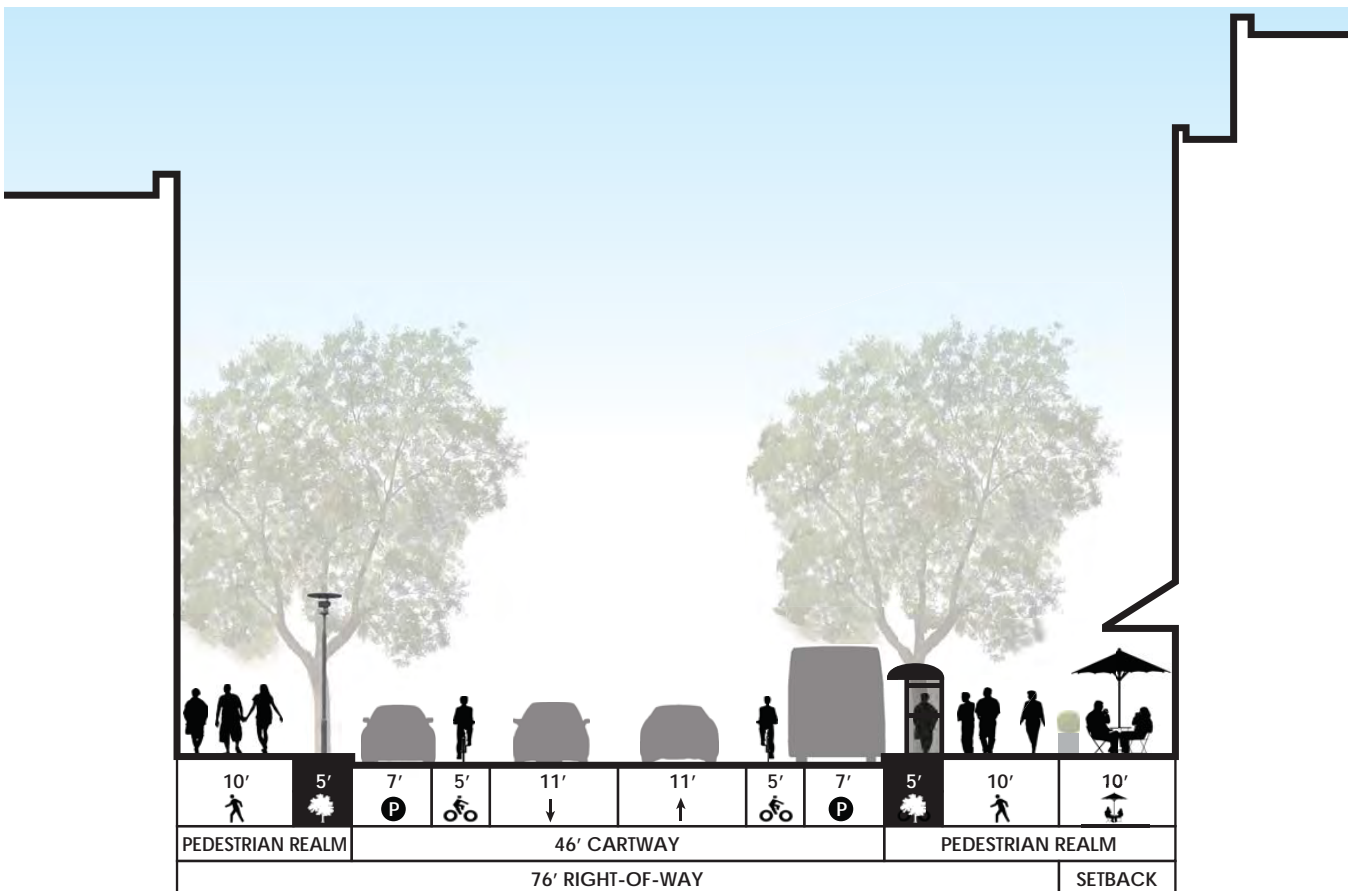
Street Type

4.3.4.2 Uptown Broad Street

A. Purpose/Definition

Intended to serve as the primary spine of Woodbury's uptown commercial and medical area. As such, the cartway and pedestrian realm must provide capacity for a high amount of automobile and pedestrian traffic. This necessity, along with the grand scale of the street, warrants a wide cartway and sidewalks with high quality materials and design, zero curb cuts and no front yard parking lots.

B. Typical Street Cross Section



C. Typical Pedestrian Realm and Associated Elements



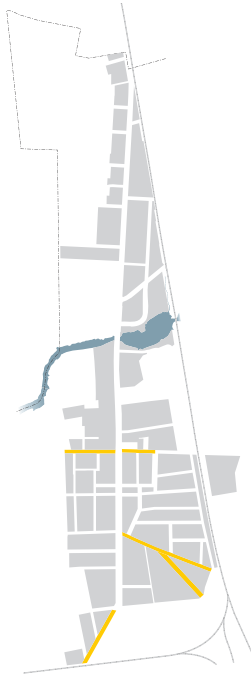
D. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|----------------------------|
| 1 | Street Tree Spacing | Avg. 40' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 100 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Bus Stop |
| 7 | Planter | Permitted |

E. Permitted Street Trees

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak. In general, tree types may be alternated except between the City boundary and Edith Street where Green Ash trees have been selected and between Red Bank Avenue and the bridge where Willow Oak trees have been selected, both for specific intent (per Street Tree Plan in Section 4.4.1).

COL



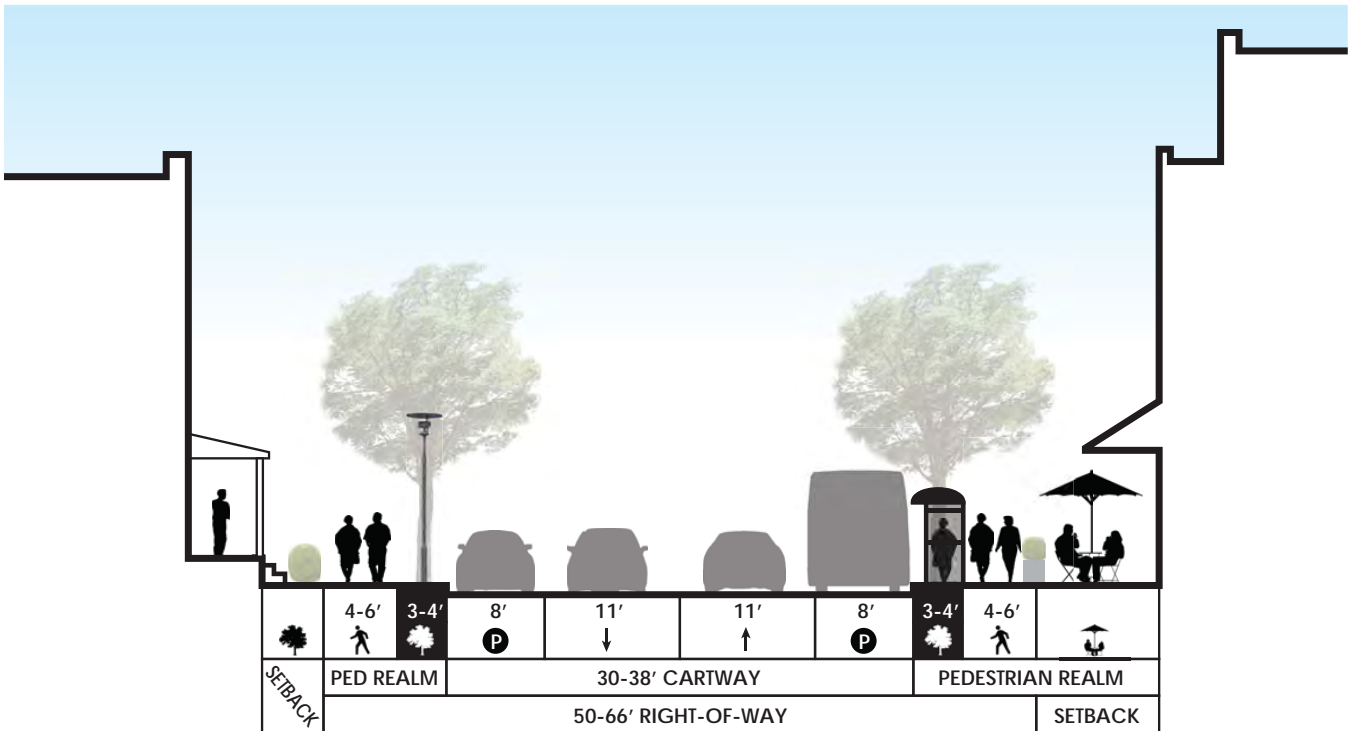
Street Type

4.3.4.3 Collector A

A. Purpose/Definition

Intended to serve as the primary feeders to Broad Street, some are subject to County jurisdiction. The cartway and right-of-way widths that are subject to the County Road Plan are likely remain unchanged. However, new development may provide the opportunity for additional pedestrian space. New development is to provide moderate minimal curb cuts and no front yard parking.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



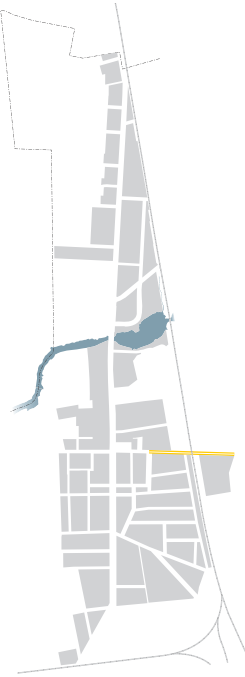
D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|----------------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Bus Stop |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash, Tulip Poplar, Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak, and Little Leaf Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

COL



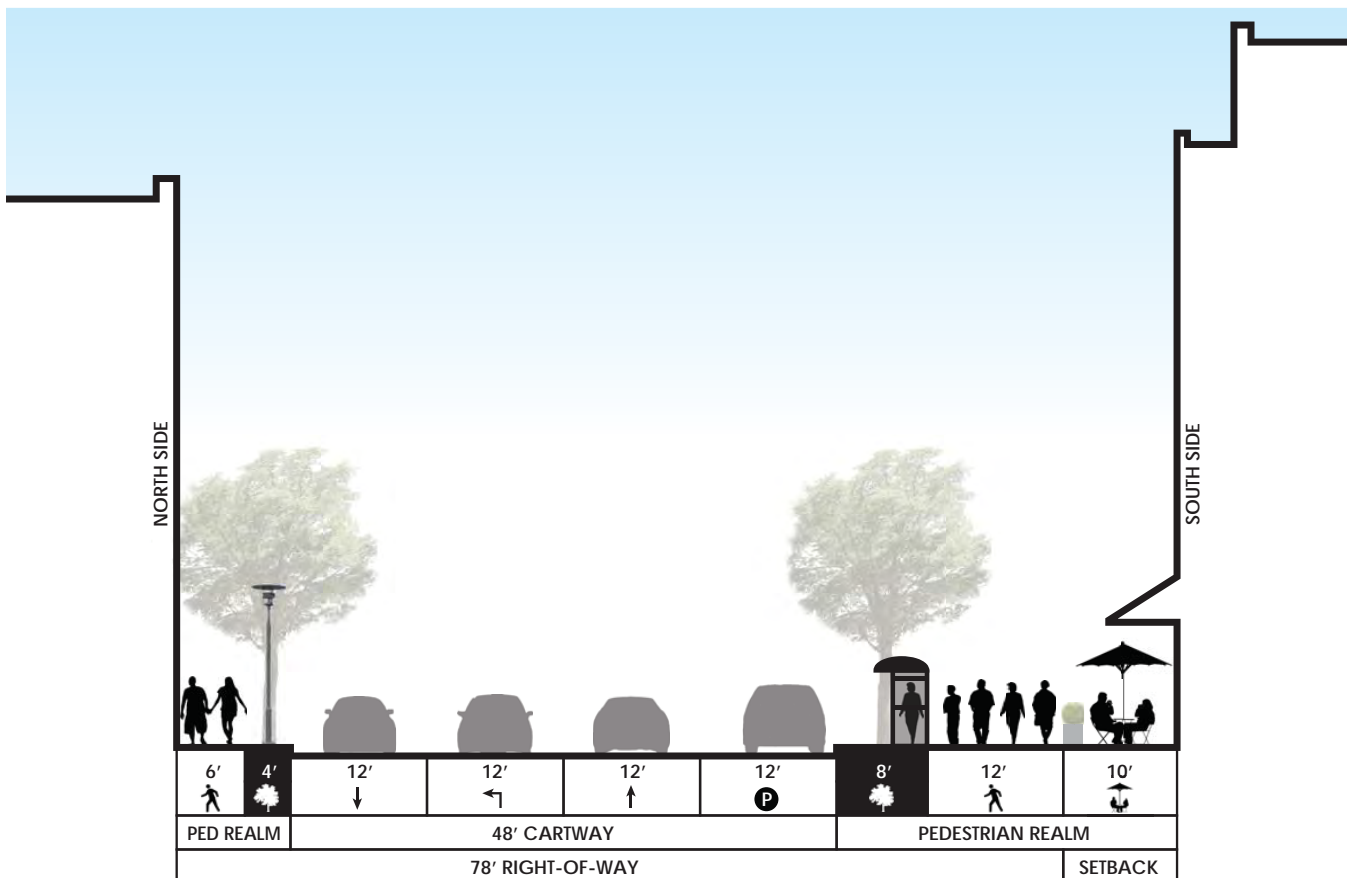
Street Type

4.3.4.4 Collector B

A. Purpose/Definition

Intended to serve as the primary feeders to Broad Street. This Street Type is subject to cartway and right-of-way widening through redevelopment. As such, new development is to provide a cartway and pedestrian realm that can accommodate a high amount of automobile and pedestrian traffic. There are to be no curb cuts. Access to parking is through adjacent streets and alleys. Parking is prohibited in front and side yards.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|----------------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Bus Stop |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash, Tulip Poplar, Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak, and Little Leaf Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

SPC



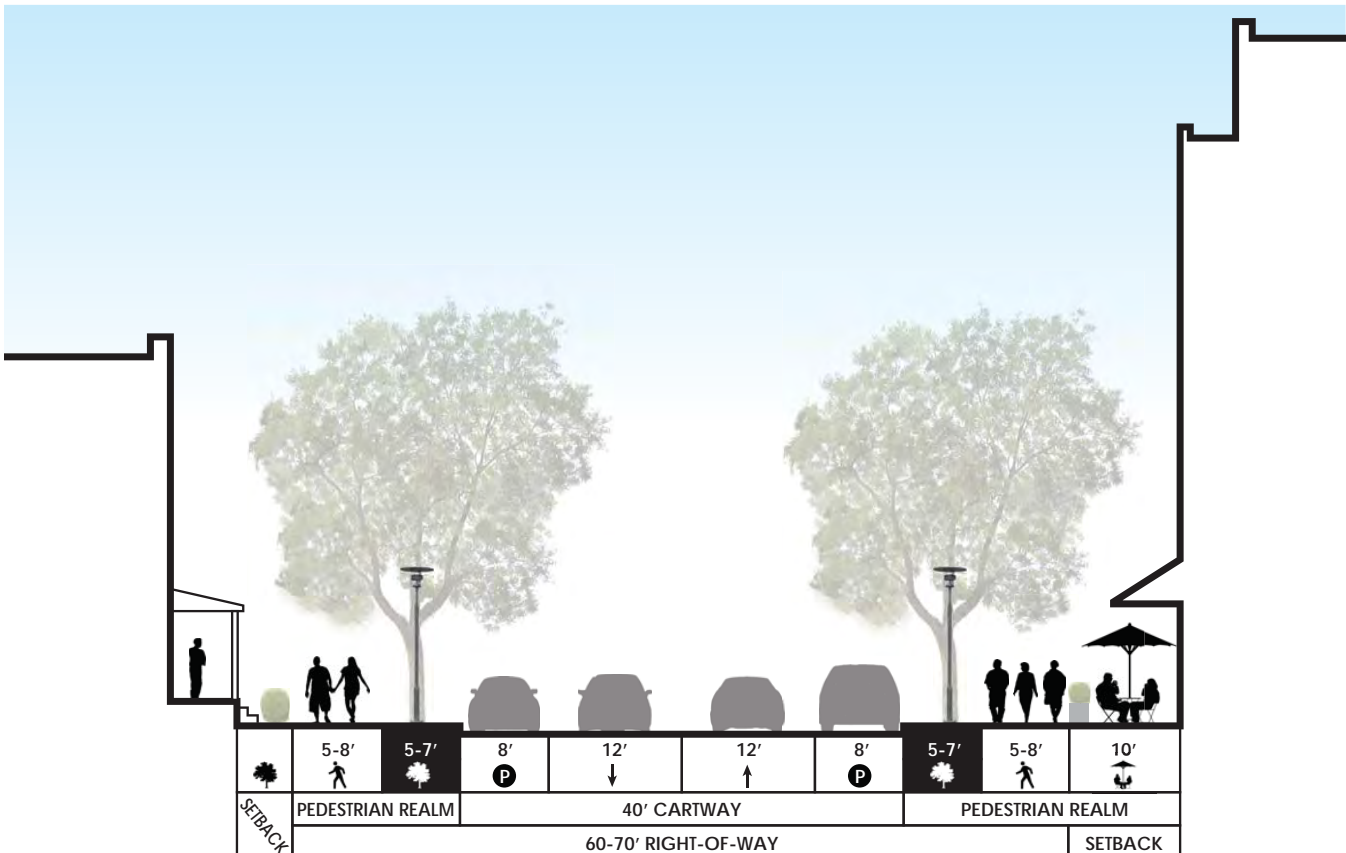
Street Type

4.3.4.5 Special A

A. Purpose/Definition

Intended to serve as the existing residential streets that will be modified through redevelopment. As such, the cartway and pedestrian realm must provide capacity for a moderate to high amount of automobile and pedestrian traffic. New development is to provide widened cartways and sidewalks, minimal curb cuts and no front or side yard parking.

B. Typical Street Cross Section



C. Typical Pedestrian Realm and Associated Elements

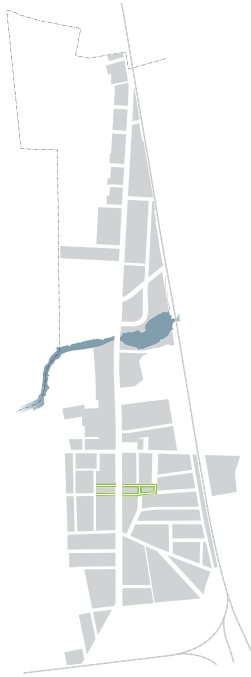


D. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. Permitted Street Trees

Permitted trees for this frontage were selected for their large size and ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except along Centre Street where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent. The male Ginkgo tree is planned for the sections between Lupton and Russell and the new street and Railroad Avenue.



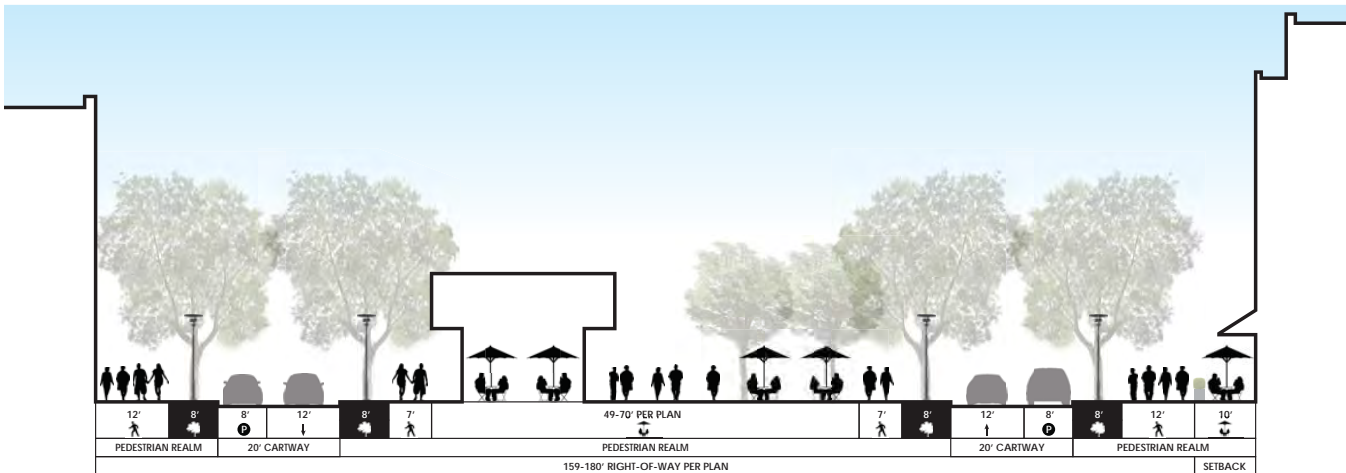
Street Type

4.3.4.6 Special B

A. Purpose/Definition

Intended to serve as one of the pedestrian-oriented street types to be added to the street grid, these master planned streets will frame Centre Street Plaza and are to have streetscapes that provide capacity for a moderate to high amount of pedestrian activity. New development provides the opportunity for wide sidewalks that shall have high quality materials and design. There are to be no curb cuts. Access to parking is through adjacent streets and alleys. Parking is prohibited in front and side yards.

B. Typical Street Cross Section



C. Typical Pedestrian Realm and Associated Elements



D. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. Permitted Street Trees

Permitted trees for this frontage were selected for their large size and ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except along Centre Street where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent. The London Plane tree is designated for this section of Centre Street.



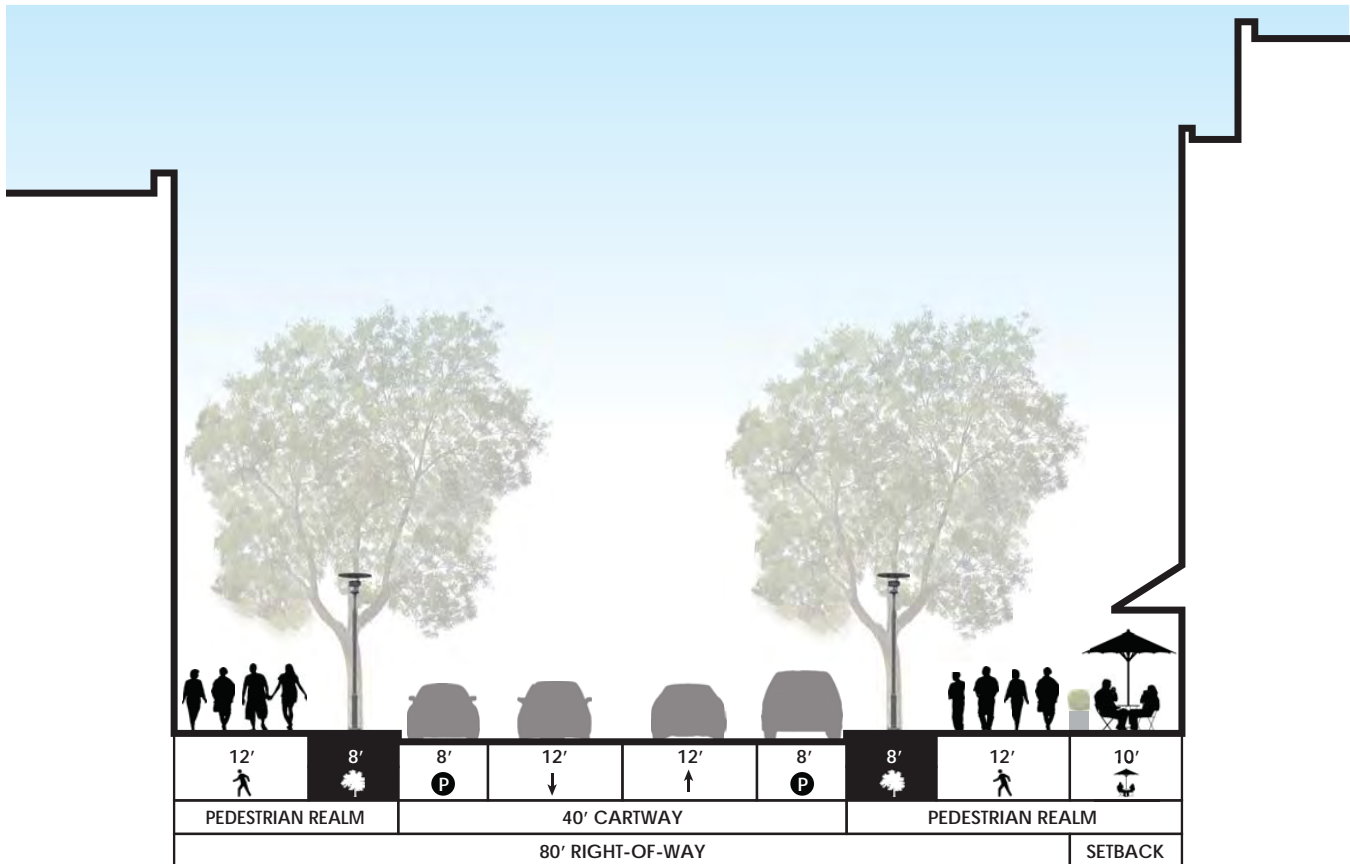
Street Type

4.3.4.7 Special C

A. Purpose/Definition

Intended to serve as one of the pedestrian-oriented street types to be added to the street grid, this street also serves as part of the proposed parallel street system that will direct traffic to parking facilities. This master planned street is to have a streetscape that provides capacity for a moderate to high amount of pedestrian activity. New development provides the opportunity for wide sidewalks that shall have high quality materials and design. They should be accompanied by minimal curb cuts and no front or side yard parking.

B. Typical Street Cross Section



C. Typical Pedestrian Realm and Associated Elements



D. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. Permitted Street Trees

Permitted trees for this frontage were selected for their large size and ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except along the new parallel connector road where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent. The Fastigate English Oak tree is proposed for the new street, part of the parallel system.

SRV



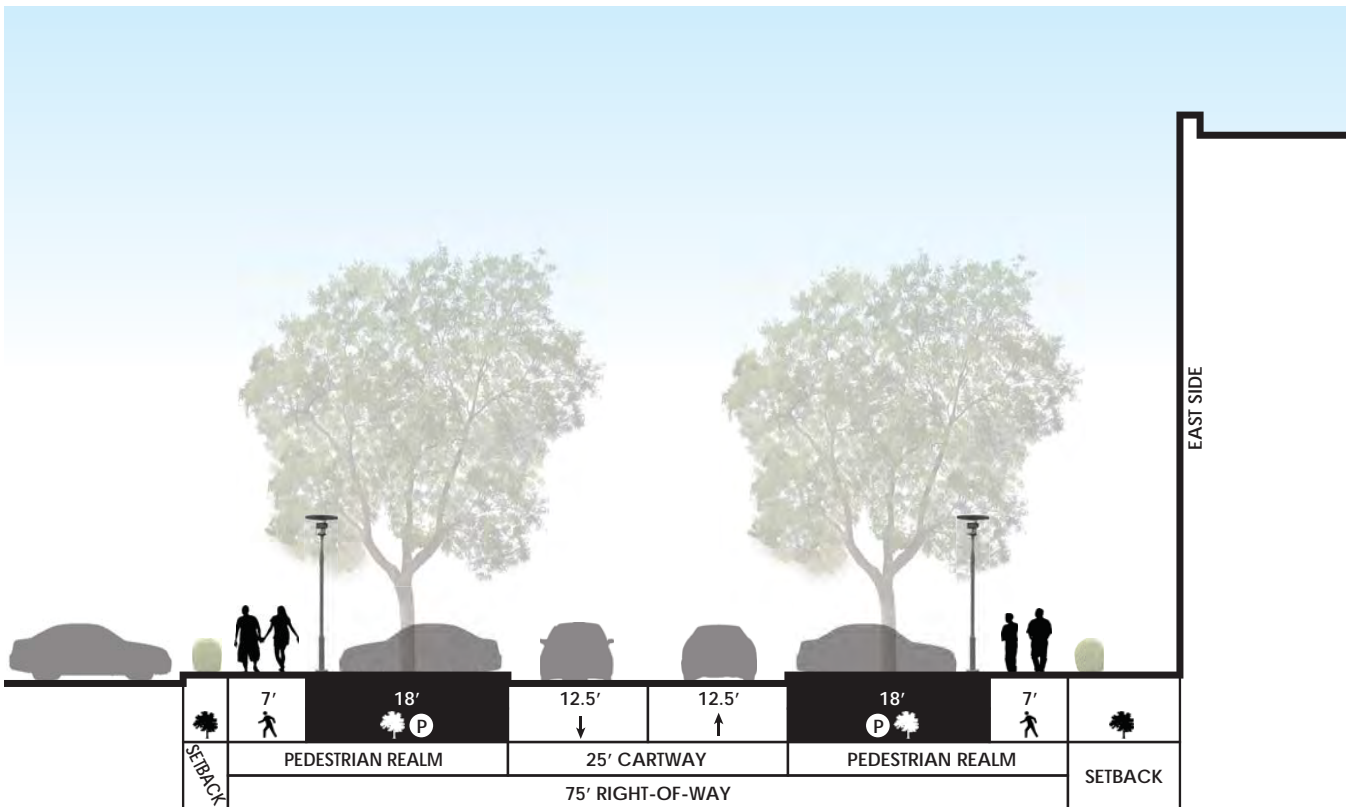
Street Type

4.3.4.8 Service

A. Purpose/Definition

This street is part of the proposed parallel street system designed to direct traffic to parking facilities. It is intended to serve as a parking-oriented street to be added to the street grid. This master planned street is designed to maximize on-street parking through 90-degree stalls. While the pedestrian realm is scaled down to accommodate parking, it is to be enhanced by textured parking stall paving, minimal front and side yard setbacks, minimal curb cuts and screened front yard parking. The textured parking stall paving is intended to reduce the visual scale of the space. No tax lots or buildings are intended to front directly on to the service street.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



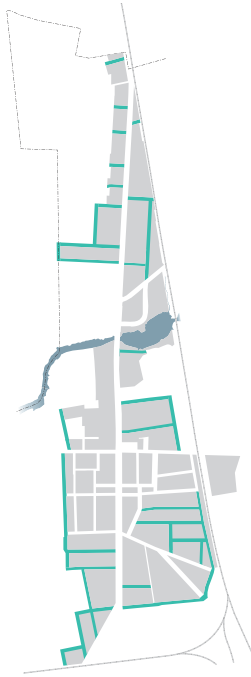
D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 60' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their large size and ability to provide shade to sidewalks while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: Sugar Maple, Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

LOC



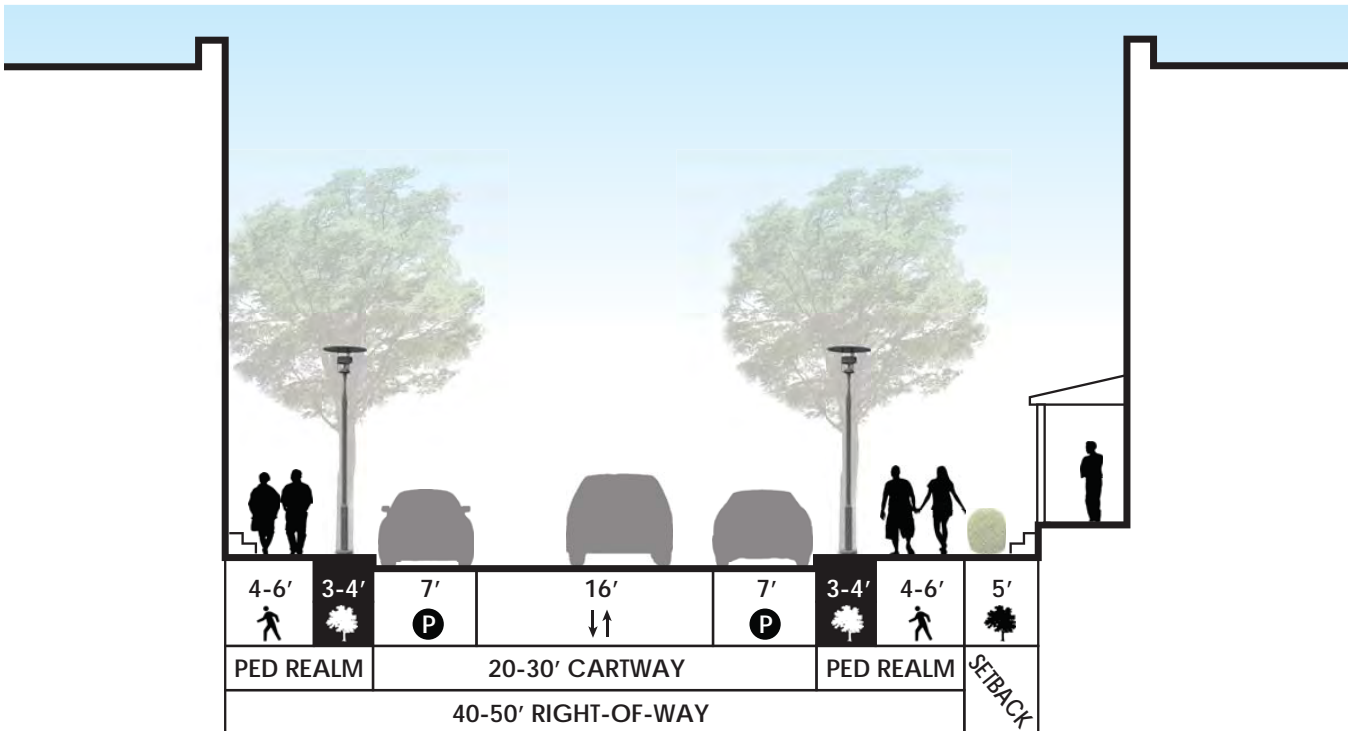
Street Type

4.3.4.9 Local

A. Purpose/Definition

Intended to serve as existing residential streets that have minimal to moderate through traffic and will be largely unchanged through redevelopment. The streetscape provides for minimal to moderate pedestrian activity. New development may provide the opportunity for additional pedestrian space. Curb cuts are permitted where alley access is not provided.

B. Typical Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



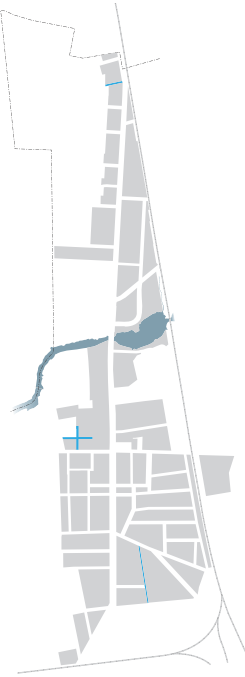
D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|--------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | Permitted |
| 4 | Bicycle Rack | Permitted |
| 5 | Trash Receptacle | Permitted |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Not Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to sidewalks and grow in a confined space while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: Tulip Poplar, Little Leaf Linden 'Chancellor', and Silver Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

SM



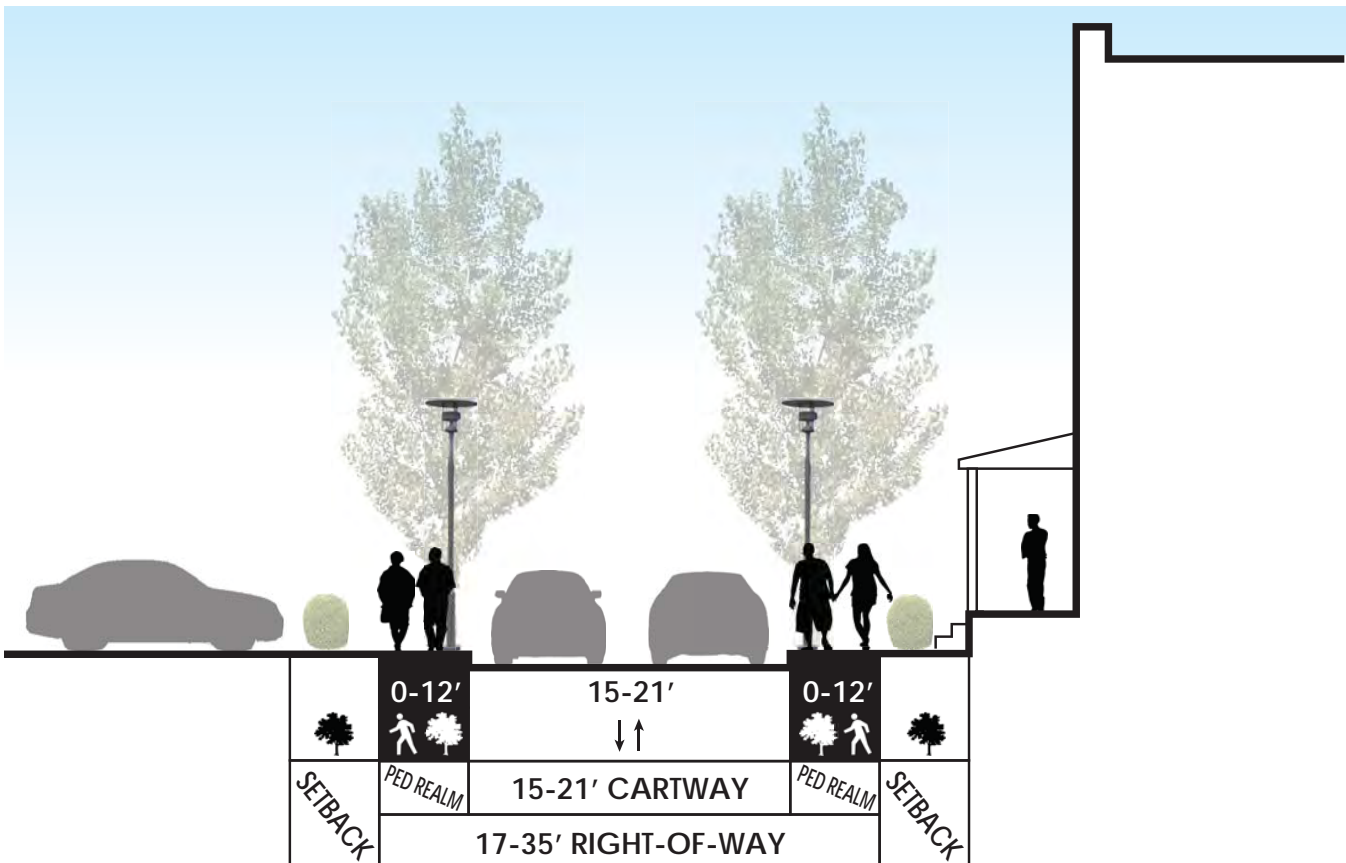
Street Type

4.3.4.10 Small A

A. Purpose/Definition

Intended to serve as existing alleys and residential streets with extremely narrow rights-of-way. Right-of-way dimensions constrain pedestrian realm opportunities. They are intended to provide vehicular and service access to residential driveways, public and private garages, and parking lots.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*

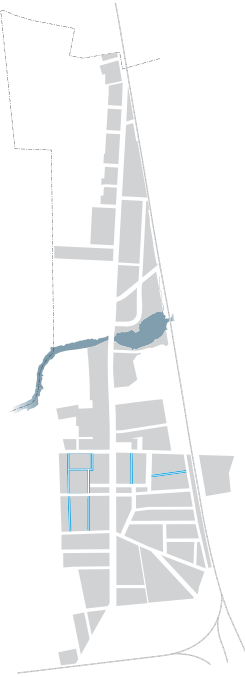


D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|--------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | Permitted |
| 4 | Bicycle Rack | Permitted |
| 5 | Trash Receptacle | Permitted |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to shade sidewalks and grow in a confined space, while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: male Ginkgo, Pin Oak, Sargent Cherry, and Silver Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.



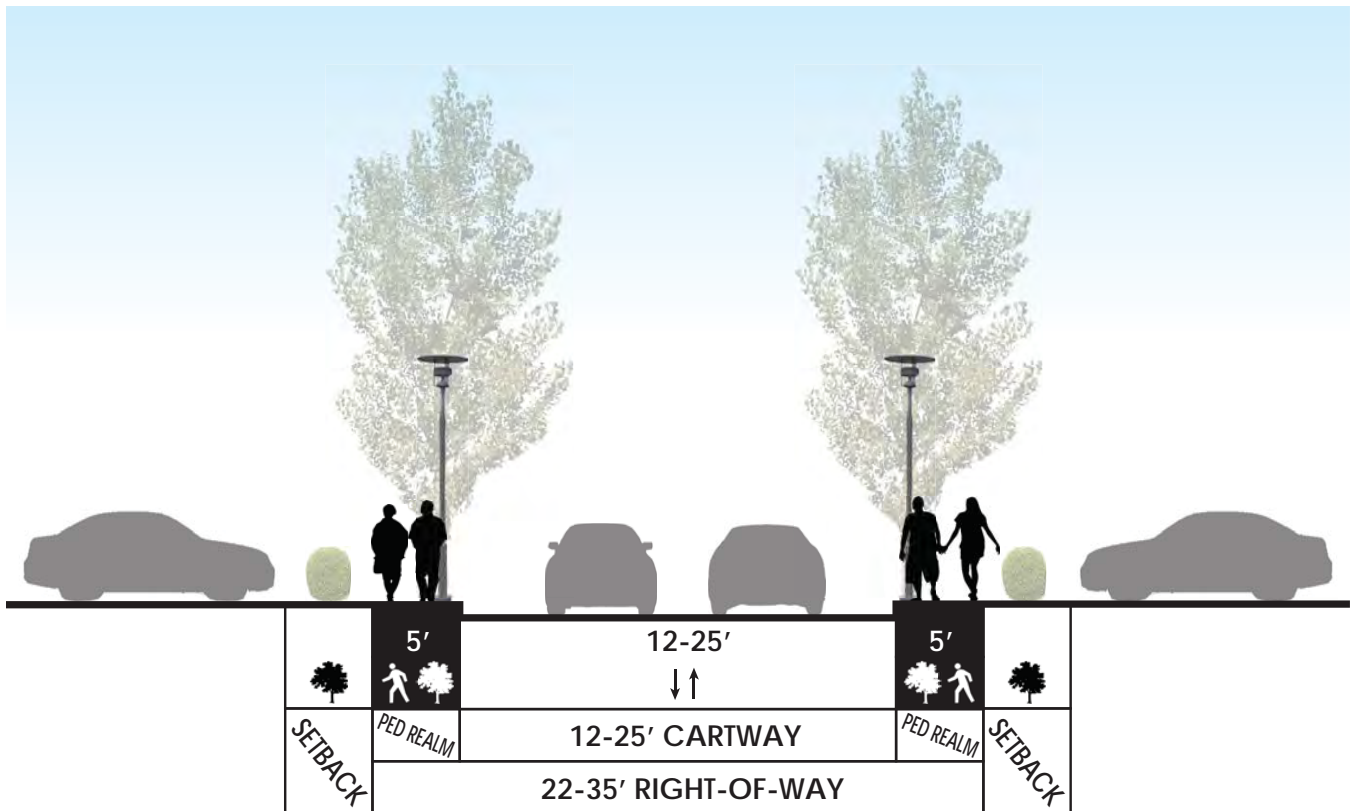
Street Type

4.3.4.11 Small B

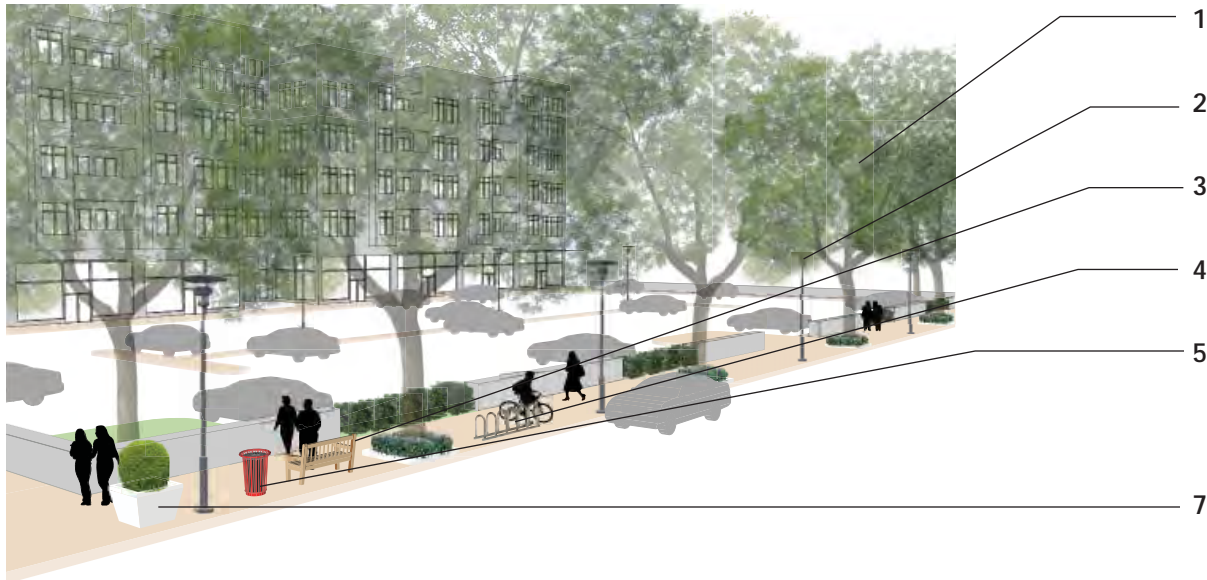
A. Purpose/Definition

Intended to serve as new alleys that will be provided to service redevelopment. Right-of-way dimensions constrain pedestrian realm opportunities. They are intended to provide vehicular and service access to residential driveways, public and private garages, and parking lots.

B. Typical Street Cross Section



C. *Typical Pedestrian Realm and Associated Elements*



D. *Pedestrian Realm Amenity Requirements*

| | AMENITY TYPE | REQUIREMENT |
|---|-------------------------|--------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | Permitted |
| 4 | Bicycle Rack | Permitted |
| 5 | Trash Receptacle | Permitted |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |

E. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to shade sidewalks and grow in a confined space, while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: male Ginkgo, Pin Oak, Sargent Cherry, and Silver Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

4.4 Public Spaces Plan

Public spaces are central to a community's quality of life. In a downtown area, it provides a social gathering area, visual and psychological relief from the built up environment, helps to define a sense of place and contributes to the vitality of the adjacent real estate market. The Public Space Plan is a plan for the public realm. The proposed improvements go beyond parks and plazas to include streets, sidewalks, intersections, pedestrian paths and mid-block cut-throughs, and the landscaping and programming required to create activated, vibrant public spaces. The Plan proposes not only what should be physically constructed but also how these spaces and places should be maintained over the long-term.

Centre Street Plaza: The featured public space in the Redevelopment Area on South Broad Street is a public plaza that runs perpendicular to Broad Street, along Centre Street. Many members of the community expressed a need for a place for the City to gather for festivals, holidays, and activities. While Broad Street is the "Main Street" for the City, its status as a state highway limits the community's ability to



Figure 4.7: Public Spaces Plan Map



Figure 4.8: Centre Street Plaza Rendering

use it as a place to assemble. Currently, large gatherings, such as the City's National Night Out celebration, are often held in parking lots off of Broad Street.

A plaza along Centre Street allows for a public space along Broad that will be activated and kept safe by adjacent ground floor retail and upper floor population on a daily basis. Not only will the plaza provide a much needed gathering space, it will also be a focal point for the surrounding redevelopment. Programming the plaza with elements such as a café, stage, fountain, and a pond/ice rink, will ensure that the space is also a destination and remains active throughout the year. Much of the plaza is designed as "hardscape" in order to foster flexibility to allow for: art exhibits, craft shows, farmers markets and live music events. Some of these elements are deliberately revenue-generating to help to pay for maintenance of the plaza.

The plaza's location along Centre Street will help to connect Broad Street with the proposed rail station, funneling traffic back and forth between the business district and the train station. While Cooper Street also provides this direct linkage, Centre Street offers an opportunity to provide a pedestrian-focused connection.



Figure 4.9: Centre Street Plaza Renderings

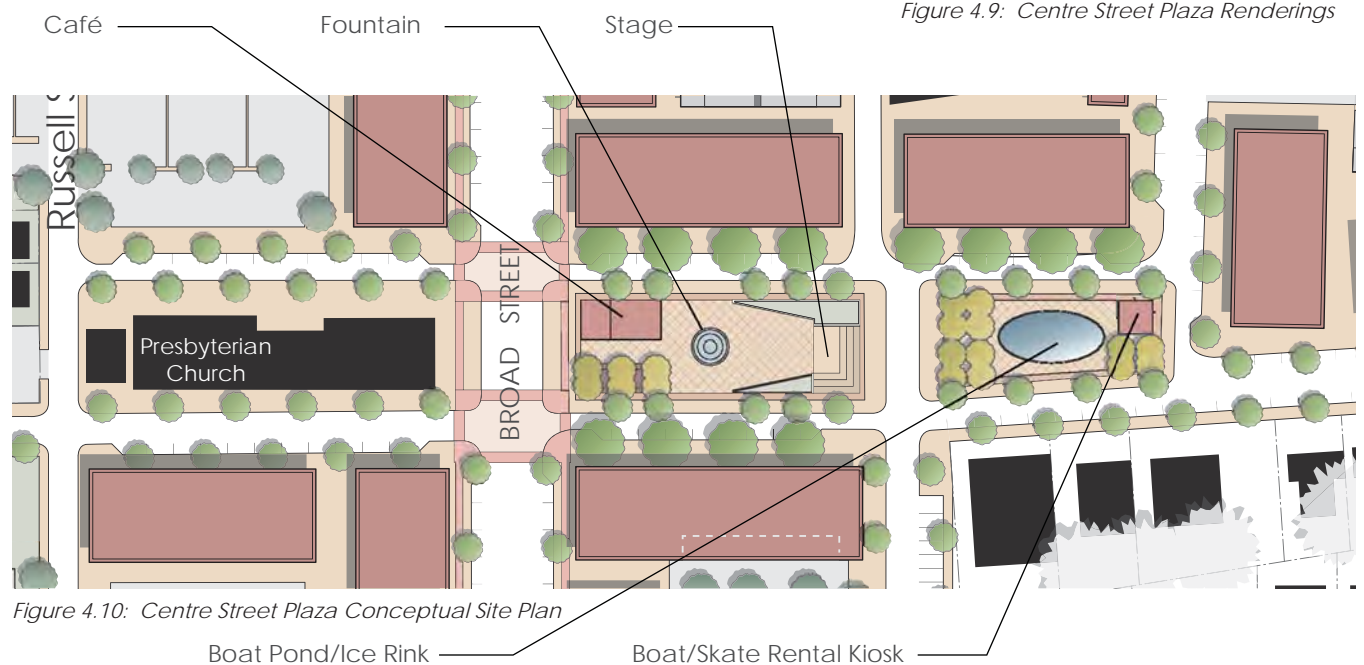


Figure 4.10: Centre Street Plaza Conceptual Site Plan

Stewart Lake Park: Stewart Lake is an amenity throughout its entire length through the City. A boat launch at Stewart Park increases accessibility to the waterway. Currently, at Broad Street, however, the Lake has limited public access and offers little in the way of a public amenity. During warmer months, people can be found fishing at the foot of the electrical high-tension tower. The Lake's banks are largely paved parking lots and access drives, to within only a few feet of the water. By relocating the police station and enabling redevelopment on both sides of the Lake, new walkways and piers can be constructed, increasing and improving access to the water. If permitted by the New Jersey Department of Environmental Protection, kayak or canoe rental kiosks could be established near Broad Street in the summer months, generating revenue for the City and opening up access to the entire Lake system.

Mid-Block Cut-Through: In order to ensure that parking is easily and safely accessible both by car and on foot, creating breaks within blocks will be necessary. Improving pedestrian access to and through buildings will improve the convenience of visiting Broad Street and adjacent streets with ground floor retail. Providing safe, attractive "short-cuts" for pedestrians will also make users more comfortable with parking further from their destination.



Figure 4.11: Stewart Lake Park Conceptual Site Plan

In addition to convenience, these passageways are an opportunity to provide additional public space for the community. They should be more of an amenity than a utility. Storefronts are encouraged to turn the corner and face the passageways to create active, safe spaces. Restaurants and cafes can take advantage of the extra sidewalk space to provide additional outdoor seating away from the street.

Connections shall be created at least every 200 feet along stretches where public parking lots are available behind the buildings. Where possible they should be linked with street crosswalks to further improve their relationship with nearby destinations. These connections can be constructed either as breaks between buildings or as passageways through buildings. All connections shall be generous in width, a minimum of 25 feet and be well lighted. Where connections pierce through buildings, ground to ceiling clearance shall be a minimum of 16 feet. They should be as shallow in depth as possible to provide a strong visual connection between the street and the parking lot and provide a safe pedestrian environment.



Figure 4.12: Mid-block cut-through provides pedestrian access to parking lots located to the rear of buildings



Figure 4.13: Street Tree Plan Map

4.4.1 Street Tree Plan

The *Street Tree Plan* proposes a specific planting scheme for the City. To enhance the City's thoroughfares, specific tree types were selected for all streets in the Redevelopment Area. This strategy creates a series of grand entries into the City's business district and will provide a striking image of the City.

The Green Ash tree was selected for the northern and southern approach to the City's business district for its height, upright orientation, fast growing nature, and stunning yellow leaf highlights. It is designated for South Broad between Carpenter Street and Barber Avenue and North Broad between the City's boundary and Edith Avenue.

The Willow Oak was chosen for the highest intensity areas of the business district due to its broad canopy and upright shape. It is planned for South Broad between Barber Avenue and Delaware/Cooper Streets and North Broad between the bridge and Red Bank Avenue.

The male Ginkgo tree is proposed for sections of Centre Street that form connections between the business district, the train station, and the West End Neighborhood. The male Ginkgo was selected for its tall and narrow shape and longevity as well as its brilliant yellow fall color.

London Plane trees were selected for the new public plaza and split sections of Centre Street on both sides of South Broad Street. The trees were chosen for their dramatic height, light grey bark, architectural column-like shape, upright branching, and high canopy.

For the new parallel connector street that is perpendicular to Cooper Street, striking Fastigate English Oak trees were selected for their slender column-like growth. Their use will create a dramatic entry to the new circulation system from Cooper Street.

Tree specifications for every roadway can be found in the *Street Regulating Plan* (Section 4.3) and in the *Form-Based Code* (Section 6). Trees identified for each street may be used alternately unless within one of the designated areas on the Street Tree Plan.

4.5 Gateway Plan

The *Gateway Plan* identifies potential locations for special aesthetic improvements throughout the Redevelopment Area. Gateway improvements can be implemented individually as funds are available and are not necessarily dependent on the redevelopment timeline. Gateways can represent entrances to the City and points of transition and arrival within the community. They can be marked with any combination of significant buildings, signage, paving materials, traffic calming elements, and landscaping. In Woodbury, community members identified a series of locations where gateways should be marked to complement the redevelopment and revitalization efforts of the City and to help reinforce Woodbury's identity.

Northern Gateway: The proposed northern-most gateway is located near the City's shared boundary with West Deptford Township where Park Avenue meets with North Broad Street. The intent of this gateway is to create iconic signage that marks the entry into the City and the Northern Business District. There are already businesses near this node that bring activity and have made it a destination unto itself such as Bellia and the Colonial Diner. Gateway improvements at this location should include installing an obelisk object with signage and lighting that acts as a beacon for the entrance to Woodbury. This object can be repeated in the Southern Gateway to bookend the City's business district.

Stewart Lake Gateway: A number of participants in the planning process pointed to the bridge on Broad over Stewart Lake as the transition from North Broad to South Broad Street, even though the technical split between the two sections of roadway is at Delaware and Cooper Streets. They recognized that this point is the threshold to the traditional downtown. After the bridge, buildings are closer to the road, ground-floor retail and restaurants begin to line the street and quickly the County Court buildings mark your arrival to "downtown." Presently, there is a large billboard type sign in the southbound direction which highlights the historic founding of the town. In the northbound direction, a rotary sign marks the Lake.

Gateway improvements to this area should include bridge repainting and maintenance, improved pedestrian lighting, and a pedestrian and bicycle crossing to connect two

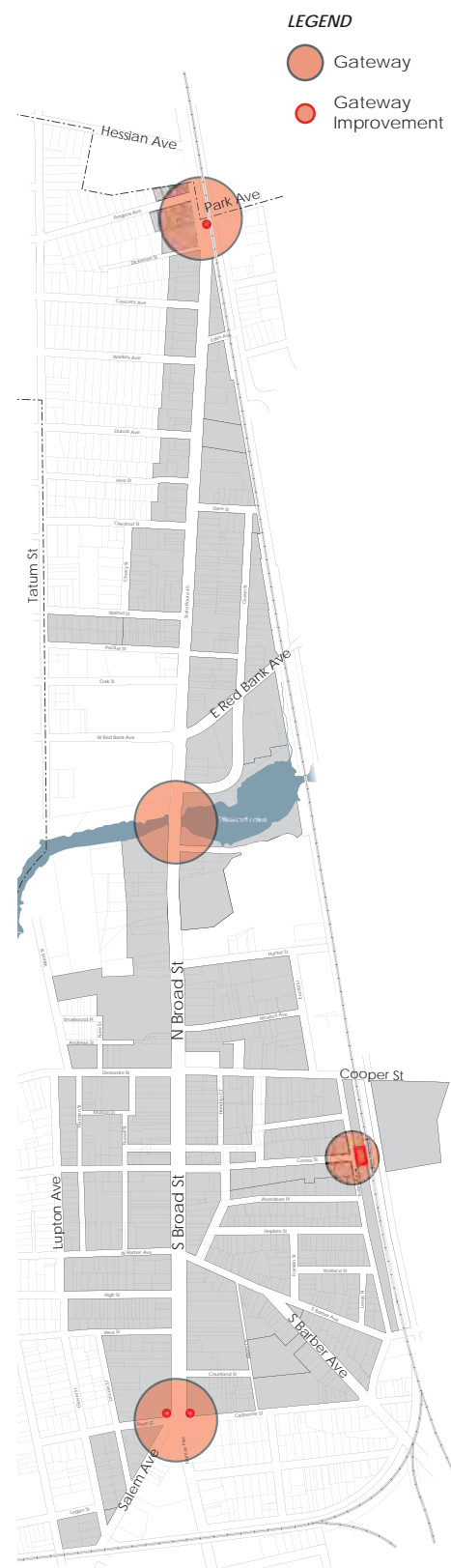


Figure 4.14: Gateway Plan Map

sections of a planned path for the waterfront. In addition to lighting for the street and sidewalk, LED lighting to accentuate the iron railing of the bridge is proposed. Expanding the sidewalk on the eastern side of the bridge is proposed to connect either side of the Lake front, thereby improving accessibility and connectivity to the Lake.

Centre Street Rail Station Gateway Plaza: The proposed reintroduction of passenger rail to Woodbury will likely enable many of the City's visitors to reach their destinations via the southern rail station at Cooper Street. Centre Street is the natural gateway to the downtown for pedestrians given the traffic on Cooper Street and the proposed public plaza at Broad Street. As a result, creating a gateway plaza at Centre Street and the rail station is proposed, provided that pedestrian access is available to the station on the west side of the tracks.

A gateway plaza at Centre Street and the tracks will allow the opportunity to provide an open, paved area near the train station to accommodate the flow of people as they arrive and depart the City. This is an area of the City in which we recommend that the street intersection be constructed of brick pavers. It should offer seating and landscaping to create a retreat adjacent to the station, as well as provide orientation for those arriving to the City. Wayfinding maps and signage should be present to direct people towards their destination as well as alert them to relevant information about events and businesses in Woodbury.

Southern Gateway: The Southern end of the Redevelopment Area was selected as a Gateway because of the convergence of Salem Avenue (Kings Highway) and Mantua Pike (Route 45) to become South Broad Street. Auto-oriented businesses begin to transition into the pedestrian-oriented downtown of Woodbury. As also recommended at the Northern Gateway, the South Southern Gateway should be characterized with a pair of obelisks, one on either side of the roadway. The obelisks should incorporate lighting, identity signage for the City, and potentially a method to convey alerts or information relevant to the business district.

4.6 Infrastructure Plan

The Redevelopment Plan requires the support of a functional and sustainable infrastructure that services the growing needs of the Redevelopment Area residents and businesses. This infrastructure element addresses the following support systems:

- A. Utility supply and distribution, including stormwater;
- B. Parking supply and street network; and
- C. Immovable infrastructure elements.

These aging and often obsolete systems will not only be impacted by the goals and objectives of the proposed Redevelopment Plan but will benefit from its anticipated rehabilitation and revitalizing efforts.

4.6.1 Utility Supply and Distribution

The utility infrastructure component within Woodbury's targeted Redevelopment Area consists of public water, sanitary sewer and stormwater facilities. The objective of any proposed improvement to these facilities to support redevelopment shall be to maintain the integrity of the existing infrastructure, provide adequate public supply, avoid leaks and losses, protect natural resources and provide extensions and refinements where required. Regional strategies shall be incorporated to reduce stormwater runoff, improve water quality, protect water supplies from pollution and over use, and improve supply capacities.

Water

Public water for Woodbury is supplied by the Woodbury City Water Department through subterranean water wells. Three of the five wells that serve the city's residents and businesses are located within the city limits. Woodbury is permitted to draw a maximum of 1.28 million gallons of water per day (467.27 million gallons a year) from these wells under their NJDEP water supply allocation permits. Any shortfall of allocation is made up by NJAWC contracted at 62 million gallons a year. Currently the City has an overall water allocation deficit of 1.164 million gallons per year based on its total current and committed peak water demands. Aside from this projected shortfall there is concern that the supply

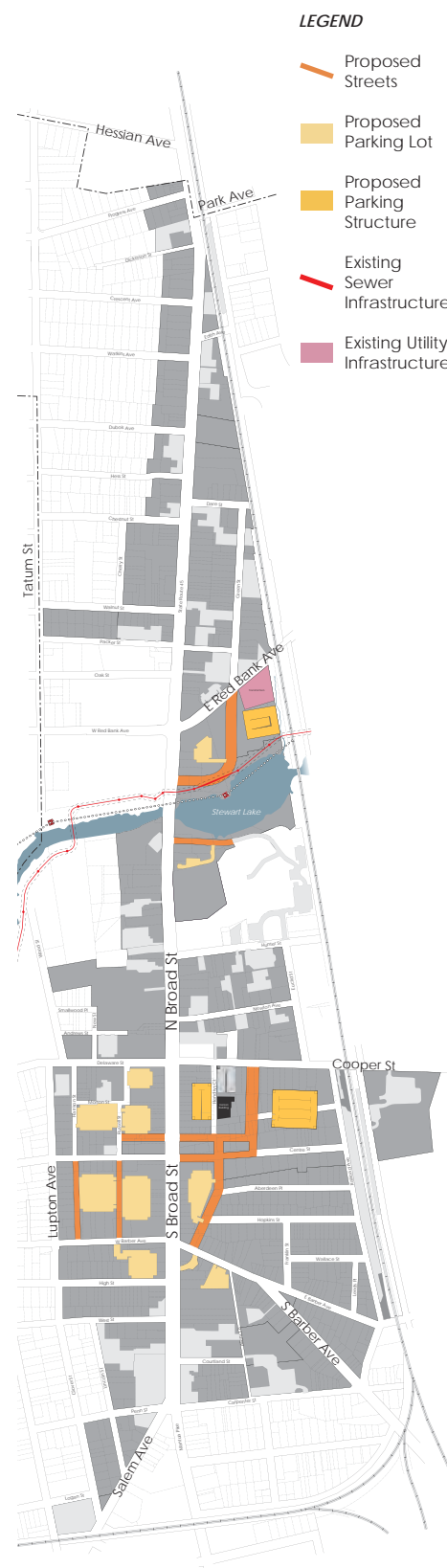


Figure 4.15: Infrastructure Plan Map

of groundwater to these heavily used wells is not sufficiently being replenished through precipitation recharge and induced infiltration. The Redevelopment Plan recognizes this concern and encourages the use of onsite strategies that increase recharge and infiltration opportunities. The water distribution system within the Redevelopment Area is approximately 50-60 years old, original cast iron piping ranging in size from 4" to 16" diameter. These piping networks generally reside within existing street right of ways. The Redevelopment Plan proposes some restructuring of the existing circulation and will require the relocation and rerouting of affected water mains and or services in order to maintain adequate water supply and infrastructure support.

Sewer

The sanitary sewer collection system for Woodbury is owned and maintained by the Woodbury Water and Sewer Department. All collected wastewater flows are pumped to a regional wastewater conveyance system and fed to treatment facilities that are part of the Gloucester County Wastewater Treatment System regulated by the Gloucester County Utilities Authority. The average daily flow from the city for 2006 was 1.159 million gallons per day. There is currently no flow restriction from Woodbury per their contract with the GCUA.

The proposed Redevelopment Area is part of GCUA's Woodbury Creek Service area. The Warren Street pump station is the primary pump station in the Woodbury Creek system. All subsystems within the Woodbury Creek System flow to this pump station which ultimately discharges to GCUA's Waste Water Treatment Plant. The sub-system most affected by the proposed redevelopment will be the Woodbury Creek interceptor. This county interceptor system generally runs adjacent to the Woodbury Creek, within a required 30 foot wide easement.

The City's Sewer collection system is over 100 year old original terracotta piping. The existing sewer lines have reached their maximum life expectancy. The disintegrating and failing system will need to be replaced throughout most of the redevelopment corridor. The sewer mains exceed 10-15' depths with vertical stack laterals. Redevelopment activities in the vicinity of this fragile aged infrastructure will most likely compromise its remaining integrity. The Redevelopment Plan includes rebuilding the existing sewer infrastructure and corrective actions as needed. Additionally, there are several

areas in Woodbury that experience sanitary sewer overflows. One such area directly affects the proposed Redevelopment Area. The county interceptor near the High School on Broad Street has experienced backups and has overflowed into the Woodbury Creek. The GCUA currently has no planned upgrades but continues to investigate corrective actions and needed improvements to the County's collection systems to accommodate increased wastewater flows and support the expected growth and development in the region.

Stormwater

Management of the stormwater is essential to control flooding and avoid potential property damage. The existing stormwater collection and conveyance system within the proposed Redevelopment Area utilizes conventional methodology typical of an older downtown environment. The development footprint of this urban environment consists largely of impervious surfaces. The goal of the Redevelopment Plan is not to increase the overall impervious coverage but in fact increase development density through strategies that actually reduce overall impervious coverage. State and Federal stormwater regulations have implemented new standards and best management practices to address the adverse impacts to human activities and land uses on groundwater recharge, stormwater runoff quality and quantity. Reducing impervious cover directly reduces stormwater runoff volume and increases water quality. The Redevelopment Plan acknowledges the goals and objectives of this regulatory oversight and encourages the use of sustainable, state of the art, low impact development techniques that reduce runoff, increase recharge and promote infiltration. Redevelopment is to be guided in such a way as to have a net positive impact on the environment.

Environments that directly benefit by stormwater impacts are nearby floodplains and wetlands. The Woodbury Creek is situated between the upper and lower corridors of the area to be redeveloped. The Redevelopment Plan proposes reduced impervious surfaces and improved vistas along this scenic water feature and parkland. Proposed improvement activities along this regulated waterway shall be in accordance with New Jersey's Flood Hazard and Wetlands Land Use regulatory guidelines and permitting requirements.

4.6.2 Circulation and Parking

The Redevelopment Plan requires a circulation and parking element that works together to continue movement along Broad; create a slower moving network to move residents and patrons around and across this main arterial and provide easily navigated access and interconnected parking facilities to promote a convenient live, work, and destination environment.

Circulation

A parallel road network is vital to the successful revitalization of South Broad Street. Key reconfigurations necessary to implement this parallel system include a new street from Cooper to East Barber Street and portions of Curtis, Aberdeen and Hopkins to be vacated. Cutting off these side street connections to South Broad reduces interruptions along the main retail corridor. The proposed Centre Street plaza provides a centrally placed connection between South Broad Street and the new parallel road system. Aside from the obvious removal of existing cartway infrastructures, existing utilities will be required to be rerouted to provide the continuation of these services.

Improved connectivity on the west side of South Broad includes the extension of existing parallel alleyways known as Russell and Harrison Streets from Delaware to Barber Street and the extension of centrally placed westbound Centre Street through to Russell Street. This improved intersection across Broad Street will require coordination with New Jersey's Department of Transportation for design of its controlling signalization.

Revitalization of North Broad Street is advanced by an extension of Green Street. This extension provided improved connectivity and access to the interior of the block.

Parking

An improved, convenient public parking system that is easy to navigate and feels safe and inviting is critical to the success of the Redevelopment Plan. A shared parking solution consisting of both on and off street parking environments is illustrated in the plan. It is anticipated that multi-level parking will be utilized to achieve maximum densities. In

addition to creating a walkable environment, adequate parking will support a vibrant, revitalized mixed-use business district for the City.

4.6.3 Immovable Infrastructure

The Redevelopment Plan acknowledges that some of the utility infrastructure in the City should be considered immovable without great expense and effort. Included in this list are: the high tension lines running over the Creek and Lake, connected to a tower on the Lake; the transformer field adjacent to CVS on Red Bank Avenue; and the Verizon Building on Curtis Street. Where possible, the Plan proposes masking these utilities through liner buildings and landscaping. Where it would be difficult to hide something, such as the high tension tower on the Lake, care should be taken to orient buildings such that views are directed away from the tower. While its current placement is unfortunate, at the same time, the tower should not warrant keeping development and public space away from an incredible amenity such as the Lake.

5.0 | PLAN IMPLEMENTATION STRATEGY

5.1 Project Site Phasing Strategy

The *Redevelopment Plan* consists of a dozen project sites or “pods” situated within two discrete locations. One set is located on South Broad Street and the other on North Broad Street. Individual project sites are roughly one square block in size, each functioning effectively as a stand-alone unit. Because each site is an independent unit, one location can be redeveloped and fully functional whether or not any or all of the other sites are ultimately redeveloped. That is, successful redevelopment of one project site will not be affected by whether or not other sites redevelop.

The intent of this Plan approach is to introduce needed flexibility both in terms of timing/sequencing of project site development and in terms of the entities that take control over each site so that redevelopers will be able to proceed in a manner that is “tailored to the economy.” Thus, redevelopment may be phased one project site at a time. Alternatively, two or more project sites may redevelop simultaneously. The Plan enables a single redeveloper to assemble all project sites. It is also possible for different redevelopers to assume control of individual sites.

5.2 Project Site Assemblage Strategy

Typical parcels in downtown Woodbury have very long, narrow configurations that do not lend themselves well to redevelopment. As a result, lots must be assembled so that they are large enough to create a marketable development parcel. The *Land Use* and *Transect Plans* provide for an



Figure 5.1: Woodbury redevelopment “pods”

estimated parcel size of approximately two acres in the Downtown Transect, one acre in the Live-Work Transect and one acre in the Neighborhood Transect.

Land assemblage and acquisition will occur through a combination of public and private sector funding. If necessary, Woodbury will utilize available funds to help acquire key parcels in order to aid redevelopment. Funds will be recouped when a redeveloper purchases the site from the City. In this way, a revolving fund will effectively be created, enabling the City to continue to acquire land if deemed necessary and to sell the property to the redeveloper/s.

Among the mandatory components of a Redevelopment Plan prepared pursuant to the Local Redevelopment and Housing Law is an inventory of all properties within the Redevelopment Area proposed to be acquired in accordance with the Redevelopment Plan (N.J.S.A. 40A:12A-7a(4)). The inventory needs to include all parcels that are earmarked for acquisition as well as those that the redevelopment entity considers as possible acquisitions.

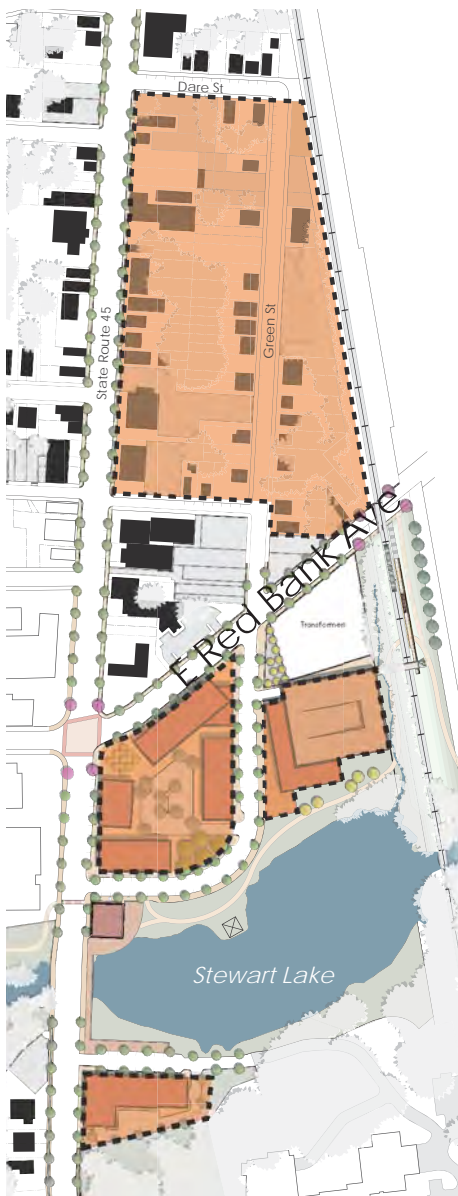


Figure 5.2: North Broad Street redevelopment "pods"

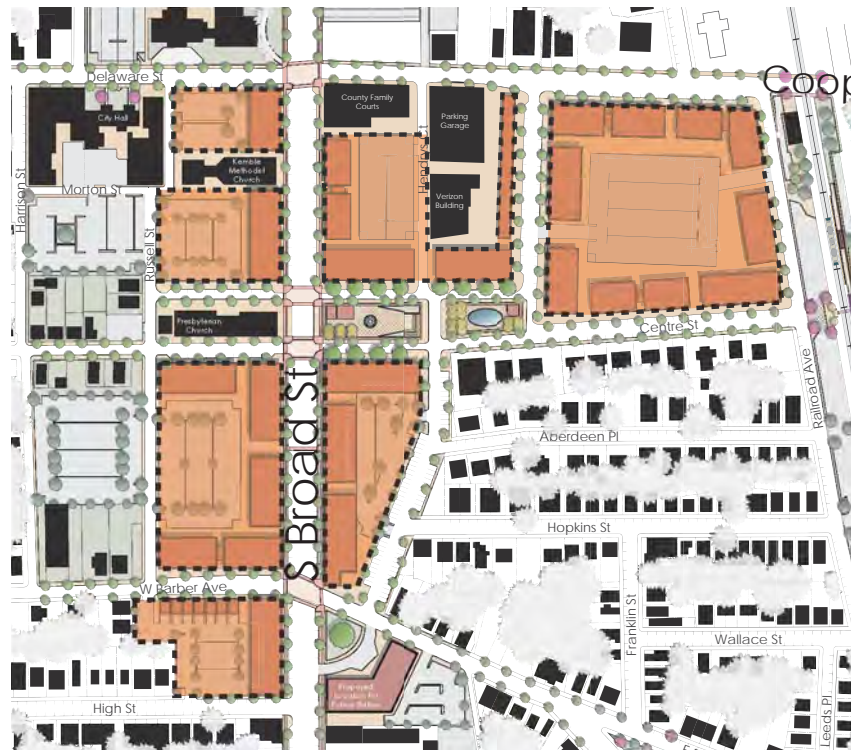


Figure 5.3: South Broad Street redevelopment "pods"

Including parcels on the inventory does not mean that the redevelopment entity is required to take possession of them. It is certainly possible that some landowners may choose to develop or rehabilitate their properties voluntarily in a manner consistent with the Redevelopment Plan. Likewise, it is possible that certain properties are acquired or under contract with a designated redeveloper.

It is anticipated that most property acquisition by a designated redeveloper will take place via a market transaction. The redevelopment entity may be able to secure the property through a voluntary transaction with the property owner. On the other hand, if the redevelopment entity decides in the future that it needs to acquire property and the property is not on the inventory, an amendment to the Redevelopment Plan must be made before acquisition takes place.

Acquisition does not have to follow a proscribed timeline for potential purchase. Before properties are acquired, the redevelopment entity will be required to create a Workable Relocation Assistance Program (WRAP, see section 5.8), to ensure that property owners are fairly compensated and provided assistance to relocate their household or business.

5.3 Circulation and Parking Strategy

Today's parking inventory and configuration and street network are problematic. A shortage of alternative routes limit travel options and place unnecessary traffic pressures on Broad Street. The street network must be made more predictable and provide multiple routes in a highly interconnected system.

In addition, the existing inventory of parking areas is deficient, especially east of Broad Street. Parking areas are located on small, chopped up lots, a reflection of ownership patterns. Most parking areas in the heart of downtown offer reserved parking, which affects retailers whose customers are unable to find parking. In order to preserve the perception of parking convenience to retail customers and thus improve the viability of main street retail, premium parking areas must be available for general community use, with safe and direct pedestrian access between parking areas and Broad Street. Furthermore, to create an efficient system of parking,

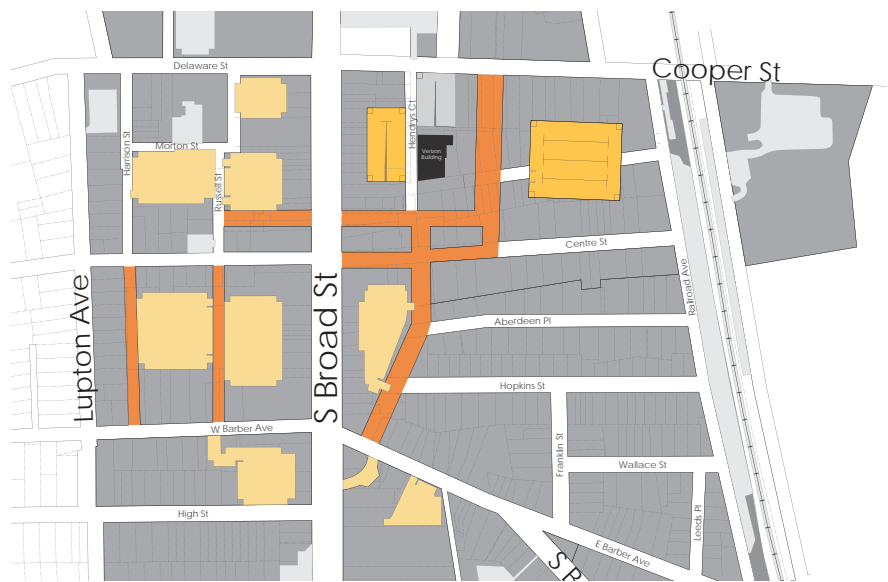


Figure 5.4: South Broad Circulation and Parking Strategy

shared parking should be encouraged, enabling spaces to be utilized throughout the day and evening.

The *Street Regulating Plan* identifies new streets that will move traffic through downtown and serve new parking areas. A combination of public funding and developer contributions will be needed to acquire future road right of way and build the planned streets. Implementation of a PILOT Program (see Section 5.7 for discussion on PILOTS) is a useful approach the City can establish to help leverage needed funding.

Residential development will have one dedicated off-street parking space assigned to each unit. All other parking will become community parking. While privatizing parking may be appropriate for residential units, the strategy for commercial uses should be to create a larger pool of public spaces to share. This will allow the opportunity to employ shared parking strategies and ensure that the entire system is easy and convenient to use. In order to introduce the amount of density necessary to maintain a vibrant business district, it will likely be necessary to build parking structures to satisfy demand.

The plan suggests three locations for new parking garages. The first garage is located on the east side of South Broad Street, along Hendry's Court, behind the buildings facing Broad. This structure will absorb the demand from nearby commercial office development as well as retail along Broad. Depending on how much parking the mix of uses contained in adjacent

buildings demands, this garage will range from two to five levels, representing between 160 and 400 parking spaces. The second parking structure is in the block that includes the vacated portion of Curtis Street. This location has the potential to be linked with the reintroduction of passenger rail service at Cooper Street, providing commuter parking to that station. At the minimum, to support adjacent redevelopment, the garage will be two levels, potentially with an upper floor entrance from the west and a lower floor entrance from the east. This configuration takes advantage of the slope of the site from west to east and would create at least 450 parking spaces. If additional parking is needed for transit, additional floors can be added at approximately 225 spaces per floor. The third location is also linked to the rail line, at Red Bank Avenue. This garage will help to make redevelopment on the CVS site feasible, as well as provide access up and over the tracks to the new station. The garage should be masked with a liner building and depending on demand will likely need to create between 630 and 880 parking spaces on five to seven levels.

The proper management of on-street parking and community parking lots and structures will be essential within the Redevelopment Area. Comprehensive oversight will enable economies of scale to be realized, creating a more affordable system of parking. The City of Woodbury can choose from several administrative approaches to parking management. These include parking authorities, parking agencies and parking departments. The following description is derived from the Parking Authority Law and from *Parking Matters: Designing, Operating and Financing Structured Parking in Smart Growth Communities* (Leonard Bier, et. al., November 2006):

- A. Parking Authority.** Woodbury can establish a parking authority in accordance with the State's Parking Authority Law (N.J.S.A. 40:11A et seq.). Under this statute, the City must create the parking authority by resolution or ordinance. Its functions include construction, provision of operation of off-street parking projects and management and operation of on-street and other parking meters and related facilities. Additionally, it can acquire by gift, buy, sell or lease property as a lessee or lessor; build facilities; borrow funds; issue bonds; mortgage property or assets; enter into contracts with the State, or any county or municipality. Parking authorities may also exercise the power of eminent domain to acquire property for purposes for which is

created. The governing body may exercise control over the authority's power of eminent domain through the resolution or ordinance that creates the authority. A provision may be included which states that the power of eminent domain shall only be exercised with the governing body's consent.

Among the advantages of a parking authority is the fact that it funds its operations not through taxes but rather, through revenue generated from parking fees. It can amass debt outside of municipal bonding limits. Since authority members are appointed rather than elected, the parking authority is removed from the political process. By the same token, it enables the authority to make decisions, such as raising parking rates, which may not be supported by the governing body. Effectively, the governing body has little direct control over its parking authority.

- B. Parking Utility.** A parking utility retains a level of independence from the municipality, though not nearly as great as a parking authority. Much like an authority, a utility has an operating budget and debt service independent of the municipality. It can accumulate annual surplus funds and hold onto its earnings. It can establish its own rates and fees. However, the governing body assumes more direct control over a parking utility. It controls rates, fees, operating budget, personnel, and capital projects. Moreover, if there are any revenues generated that exceed annual operating expenses, they are usually assigned to the municipal general fund. The responsibilities of a parking utility are far more limited than that of an authority. Its sole responsibility is the provision of public parking.
- C. Parking Department or Bureau.** Of all options, a parking department or bureau is the least independent from the municipality. The parking department/bureau is responsible for managing and operating municipal parking facilities. The municipality assumes direct control over its operations. Unless there is a bond agreement, the municipality's general fund receives all parking fees and revenues, which can be applied toward any local expenses. The municipality must approve any parking fee or rate increases. Unlike a parking authority, a parking department/bureau cannot incur debt in excess of the municipality's bonding limit.

5.4 Built Form Strategy

The *Form-Based Code* found in Section 6 is the regulatory mechanism guiding future redevelopment of downtown Woodbury. It incorporates by reference the development requirements set forth in the regulating plans. Specifically, this ordinance dictates not only uses, densities and intensity of development; it also regulates building form and establishes streetscape standards to ensure that both form and function of new development is fully consistent with the *Redevelopment Plan* vision, goal, and regulating plans.

5.5 Zoning Map Amendment

The *Redevelopment Plan* contains land development regulations (in a Form-Based Code) that will supersede the regulations that are currently in place and as such the zoning map must be amended to delineate the boundaries of the Redevelopment Area.

5.6 Procedure for Amending the Plan

The *Redevelopment Plan* may be amended from time to time to adjust for changes in the real estate market, to incorporate changes in local development objectives and land use policies, or to recognize emerging development trends. The process for amending the *Redevelopment Plan* is the same as the process undertaken for its initial adoption, culminating with the amendment's adoption by ordinance by the City Council.

5.7 Structure and Powers of Redevelopment Entity

In New Jersey, the redevelopment entity is designated by the governing body and authorized to implement the Redevelopment Plan and oversee the redevelopment projects within the designated area in need of redevelopment. Once designated, the redevelopment entity has certain authorities applicable to the redevelopment process, which are at its disposal.

A redevelopment entity can be one of four public bodies. It can be the governing body, a municipal redevelopment agency, a municipal housing authority or the County Improvement Authority. More than one redevelopment entity can be

designated within a single Redevelopment Area, so long as no more than one is involved with a single redevelopment project.

There is a distinct advantage to designating the governing body as the redevelopment entity. It allows the elected officials to control and ensure implementation of the community's vision. It is the elected officials' adopted plan, and they will likely take ownership of the plan vision. Thus, while there are advantages and disadvantages associated with each of the possible redevelopment entities, the City of Woodbury has determined that designation of the governing as the redevelopment entity will be in its best interest. Thus, Woodbury City Council is assigned this responsibility.

As the designated redevelopment entity, City Council has available a tremendously helpful set of tools available to facilitate redevelopment. Those that are most pertinent to the successful implementation of the *Woodbury Downtown Business District Redevelopment Plan* are described below.

5.7.1 Selecting the Redeveloper

As the redevelopment entity, the City has the authority to designate the redeveloper or redevelopers. This can be done either through issuing a request for proposals or by entering into direct negotiations with a specific redeveloper. Once the redeveloper is selected, a redeveloper agreement is prepared and executed. The agreement sets forth roles and responsibilities in completing the redevelopment project and implementing the *Redevelopment Plan*.

5.7.2 Long Term Tax Exemptions

The City is authorized to grant long term property tax exemptions for redevelopment projects and for projects involving relocation of residents who have been displaced by the redevelopment. This is authorized by the Long Term Tax Exemption Law (N.J.S.A. 40A:20-1 et seq.). The recipient of the tax exemption pays a yearly service charge instead of local property taxes. This yearly service charge – called a Payment in Lieu of Taxes or PILOT – only applies to the value of the new improvements associated with the redevelopment project. There are two methods that can be used to calculate a PILOT. The City can charge a percentage of the total project cost, not to exceed two percent. Otherwise, it can charge a percentage of the

project's annual gross revenue. Regardless of method used, the payment will not be less than the total tax revenue generated prior to redevelopment.

Long term tax exemptions can extend for up to 30 years from the time that the redevelopment project is complete or no more than 35 years from the time that a financial agreement between the municipality and the developer is executed. By the end of the exemption period, the PILOT amount must be phased in to full taxation.

Long term exemptions are generally used when the cost of redevelopment is prohibitively high, due to the estimated assessed value of the redevelopment project and the property tax rate. Therefore, they are used to aid redevelopment when it is clearly demonstrated that redevelopment will not otherwise occur. One of the benefits of PILOTs is that the City will have more flexibility in how the revenues are used. While revenues may be shared with the school district, it is not mandatory. In Gloucester County, the law requires that a portion of the payment go to the County. Requiring a fiscal impact study by the redeveloper will assist the City in determining the impacts of redevelopment on infrastructure and public services, including schools. From that, it can develop a strategic plan for how to use PILOT funds.

5.7.3 Five Year Tax Exemptions and Abatements

The Five-Year Exemption and Abatement Law (N.J.S.A. 40A:21-1 et seq.) authorizes municipalities to adopt an ordinance permitting exemptions and/or abatements over a five-year period.

An abatement is a tax exemption on the portion of a property's assessed value based on the property value prior to construction, improvement or conversion of a building or structure. While abatements can be applied to residential units, they are not permitted for commercial and industrial uses.

An exemption is the portion of the assessor's full and true value of any improvement, conversion, alteration or construction that does not increase the taxable value of the property. A municipality may exempt all or a portion of the added assessed value from property taxes. Like abatements, exemptions can be authorized for residential dwellings. Unlike exemptions, they are also available to commercial and industrial uses.

These shorter term exemptions and abatements entail far less complex arrangements than do long term exemptions. Furthermore, they allow single family homeowners and small businesses seeking to rehabilitate their properties to receive the benefits of these options.

5.7.4 Revenue Bonds

The redevelopment entity is authorized to issue revenue bonds to help finance the redevelopment project. The principal and interest are payable from the income and revenues resulting from projects financed with proceeds of these bonds.

5.7.5 Property Acquisition and Transfer

The redevelopment entity may acquire or convey property or an interest in property in a Redevelopment Area, including property needed to relocate residents, industry and businesses displaced from the Redevelopment Area. Additionally, in conjunction with the *Redevelopment Plan*, it is authorized to lease or convey public property or improvements without the need for public bidding. Public bidding is otherwise required when selling public lands. See Section 5.9, Property Acquisition, for the strategy employed in this Plan.

5.7.6 Eminent Domain

Eminent domain, or condemnation, is a power available to public entities. While eminent domain is a tool available for redevelopment purposes, it does not have to be utilized. When it is pursued, it is usually done after good faith efforts by developers to acquire private property have failed. At that point, a redevelopment entity may step in to acquire private property, compensate the property owner based on fair market value, and convey the property to a redeveloper for redevelopment purposes.

While eminent domain is not necessarily going to be pursued by Woodbury, it is an option that will be available, if necessary, to facilitate redevelopment in the downtown Business District.

5.8 Relocation Strategy

The LRHL requires that a redevelopment plan make “adequate provisions for the temporary and permanent relocation, as necessary, of residents in the project area.” Per Section 5.9 – Property Acquisition, it is estimated that of the 934 potentially affected properties in the Downtown Business District Redevelopment Area, 63 are listed as “to be acquired,” 838 are identified as “may be acquired,” and 33 properties are “not to be acquired.”

Provisions for adequate relocation require an estimate of the extent to which decent, safe and sanitary dwelling units, affordable to displaced residents, will be available to them in the existing and applicable local housing market. This also requires assistance to those commercial and industrial businesses that may be displaced by the redevelopment process.

The properties identified as “to be acquired” include a mixture of land uses that affect Commercial Retail, Parking Lots, Vacant Open Space, Single Family Residential, Multi-family Residential, Institutional Secular, Retail – Office – Institutional Secular and Commercial Office properties. Ample and comparable housing and/or business space will be identified prior to the City commencing negotiations with property owners and acquisition of their subject property.

Properties delineated as “may be acquired” include a variety of land uses that are dispersed throughout the redevelopment area. In accordance with all applicable State and Federal statutes, the City of Woodbury will provide all displaced tenants and landowners with the appropriate relocation assistance. This assistance will be provided by the designated relocation entity, who will carry out all obligation established in the Relocation Assistance Law of 1967 (N.J.S.A. 52:31B-1 et seq.) and the Relocation Assistance Act of 1971 (N.J.S.A. 20:3-1 et seq.).

Those few properties identified as “not to be acquired” include land uses identified as Government property, Cemeteries, Parking Lots, Vacant Open Space and Garage structures, and therefore, are not subject to this relocation strategy or any prospective Workable Relocation Assistance Plan (WRAP).

Comparable replacement housing will be provided within local housing markets for the temporary and permanent relocation of all residents who are displaced as a result of this redevelopment plan. The City of Woodbury will make substantial efforts to analyze local housing markets within Woodbury City, as well as surrounding communities, as permitted by State law. Similarly, the City will identify comparable locations for the commercial and industrial businesses displaced by the acquisition and development needs of this redevelopment plan.

Prior to the acquisition of any properties within the Downtown Business District Redevelopment Area, the City will be required to develop a WRAP, which must be reviewed and approved by the New Jersey Department of Community Affairs (NJDCa) before relocation activities can be initiated. The WRAP must outline and identify the number of residents, tenants, property owners and businesses that are impacted by the relocation plan and must demonstrate that there are enough comparable replacement housing units or business sites in the area. The WRAP must also determine the following:

- whether the displacement period is temporary or permanent;
- the extent of the need of the displacees;
- whether there is coordination with other displacing agencies;
- how hardships to displacees are minimized; and
- the source, amount and availability of funds necessary for relocation.

Procedurally, the City and its designated relocation case manager will conduct a survey in order to identify the residents and business owners and develop a dialogue with them as to their relocation needs. The survey is conducted by mailing certain affected businesses and residents an explanation letter, along with a survey. This process also includes conducting door-to-door surveys, where needed. These surveys are designed to gather information from businesses, residents and property owners that would assist the City in planning for the relocation process. Residents and businesses that do not respond to the survey will be able to meet with the City and its designated relocation representatives, as the project progresses.

In preparation of the relocation, General Information Notices regarding potential relocation benefits will be sent to all affected businesses and households. All notices will be personally served or sent by first class mail, in English and, other appropriate

language(s) as necessary. A relocation case manager will be assigned to each business or household, where an initial interview will be conducted, and a case file established.

The initial interviews will be conducted to discuss the business' or tenant's relocation needs and to complete a Site Survey for each displacee. Each business or household will be assigned a case manager to assist them through the relocation process. The relocation case manager will be responsible for ensuring that residents and businesses are relocated in accordance with the requirements of the New Jersey State Relocation Assistance Law.

During the interviews the relocation case manager will explain the effect of the pending acquisition and the relocation benefits that will be available once the property acquisition process has commenced. A copy of the flyer, Relocation Assistance to Tenants Displaced from Their Homes, Relocation Assistance to Homeowners Displaced from Their Homes, or Relocation Assistance to Displaced Businesses will be reviewed with each displacee during the initial interview. The Residential Site Survey or Business Site Survey Form will be completed at each interview. Furthermore, case managers will work to identify any individual/family social service needs, provide resources, and ensure that supportive services are available, provided they remain within the 50-mile relocation area established by State statutes.

Information collected in each interview will include:

- The current contact information and composition of the affected household;
- The number of rooms, types of rooms, monthly rent and utility costs, occupancy date, and lease information for each household;
- The household's financial preparedness for relocation, along with their preparedness for the timing of the relocation;
- Displacees' needs relative to location, size, type, amenities, and cost of comparable replacement housing, as well as, health, transportation, religion, and education services; and

- The nature of the business conducted, clientele served, geographic location of service area, number of employees, size of space needed, equipment to be relocated all will be discussed at the initial interview.

The City will establish a relocation project field office to be located in the Redevelopment Area. The field office will be used to conduct interviews and to house the project's relocation staff so as they are available to answer questions. This field office shall be open by predetermined hours each week that are convenient for the displacees and shall remain open until the project is closed out and completed.

In order to determine eligibility for residential displacees, the relocation case manager will utilize the information provided during the individual initial interviews and will make a determination of the resident's anticipated eligibility for relocation assistance.

Official Notices of Eligibility for Relocation Assistance will be sent to the tenants or homeowners upon completion of the individual initial interview. The official letters will include flyers outlining the specific relocation assistance benefits available to all tenants and will identify at least two (2) comparable replacement units. Ninety (90) and thirty (30) day Notices to Vacate will be sent subsequent to the Official letter, as needed.

The replacement housing payment, rental assistance payment, and down payment assistance payment will be calculated as necessary for each displacee. The City's relocation case manager will then review the calculations with each affected household. The relocation case manager will then prepare all required documentation necessary to secure approvals for the payments to each displacee, such as moving expense payments and tenant rent supplement payments. The relocation case manager will also obtain three moving cost estimates for households requesting actual moving costs in lieu of the fixed fee moving allowance. Estimates will be reviewed with each eligible household. The City and its designated relocation case manager will establish a payment system to ensure the timely disbursement of funds.

The case manager will also coordinate with the City the procedures for identifying and assisting in the relocation of tenants that are not officially eligible for relocation assistance.

Such tenants will include those that were not in residence 90 days prior to the initiation of negotiations and those that may have moved into the unit, legally or illegally, after eligible tenants have been relocated.

In order to identify adequate and comparable replacement housing sites, the City's relocation case manager will work with the homeowners and tenants on an individual basis to provide assistance within the local housing market or assist and advise them as to their opportunity to purchase or rent a home in the area once it has been redeveloped. Potential landlords will be contacted and local real estate agencies will be enlisted to assist in finding replacement units. All replacement units will be inspected to ensure they are comparable, decent, safe, and sanitary.

The relocation case manager will develop a list of realtors who are knowledgeable about the needs of relocating residents and who will be able to assist them in finding replacement homes. In addition, a list of preferred lenders will be established to ensure that residents are working with responsible community lenders and are not subject to predatory lending practices.

Once the City has submitted the official written offers to purchase of affected commercial properties, notices will be sent to all businesses advising them that they are officially eligible for relocation assistance benefits. The official letters will include flyers outlining the specific relocation assistance benefits available to them. A preliminary survey of the businesses indicates that all of the businesses prefer to relocate rather than apply for a payment in lieu of moving. Thirty (30) day Notices to Vacate will be sent subsequent to the Official letter, as needed.

In preparing the final redevelopment plan for the area, the City and its redeveloper will review the need to provide housing and/or commercial space to the residents and businesses that need to be relocated. The following guidelines have been established and will be implemented, as needed, pending the adoption of the final redevelopment plan and developer selection.

- Relocated households and businesses will be asked by their relocation case manager whether they want to return to the redeveloped area. In addition, all displacees will be mailed information announcing the availability of new housing.

- Residential relocatees must meet any eligibility requirements associated with the new rental units, such as income limits for subsidized rental units and good credit history. In the case of for-sale housing, residential relocatees must have the ability to qualify for a first mortgage and meet the income limits for affordable units, if applicable.
- This “right of first opportunity” for new residential units will be offered at least three months in advance of construction completion and must be exercised at least two months prior to the completion date to ensure the developer has occupancy upon availability.
- The City and the selected Redeveloper will work with each business individually to determine whether they will be afforded an option to relocate back into the redeveloped area.

Funding for this project, including acquisition, demolition, and relocation assistance benefits paid to the displacees, will come from the City and/or its designated redeveloper.

The City and its designated relocation case manager will work closely with the community to seek its input, provide feedback and maintain an open dialogue on the progress of the relocation process and the need to alter or clarify relocation policies, practices, or benefits. As provided above, the relocation case manager will maintain their field office within the Downtown Business District Redevelopment Area and be available to residents should questions or concerns arise. Special meetings and/or workshops will be scheduled to review how relocation benefits will be calculated, what assistance will be provided in locating replacement sites and identification of purchasing or renting opportunities within the redevelopment area.

Once NJDCA approves the WRAP, the City will inform all affected residents, tenants, property owners and businesses and begin to work with them to identify other places to reside or conduct business. Nonetheless, the process of relocating the affected businesses will receive the careful attention and consideration of local City officials and their designated professionals.

5.9 Property Acquisition

The classification of properties within the Downtown Business District Redevelopment Area as “to be acquired” or “may be acquired” followed a specific strategy. Those properties where key circulation infrastructure is required to effectuate the parallel road system are classified as “to be acquired.” Properties in the remainder of the Downtown Transect are listed as “may be acquired.” Properties in the Live-Work and Neighborhood Transects, as well, are listed as “may be acquired.” These rules had a few exceptions. County properties were designated “not to be acquired;” whereas City properties were left on the acquisition list to give the City flexibility to locate their buildings and services in the most appropriate sites. Non-profit, tax-exempt properties, such as churches and schools were listed as “may be acquired” in order to give those organizations the opportunity to participate in redevelopment if so desired. Parks and cemeteries were listed as “not to be acquired.”

As stated in Section 5.2, including parcels on the inventory does not mean that the redevelopment entity is required to take possession of them. Nor does it mean that properties will necessarily be acquired. Including properties on an acquisition list allows that owner to participate in the programs available for Redevelopment Areas. The resulting list includes 934 properties, of which 63 are listed as “to be acquired,” 838 that “may be acquired,” and 33 properties “not to be acquired.”

5.10 COAH Obligations and Relocations

5.10.1 COAH Redevelopment Obligation

Assembly Bill A-500 specifically requires a 20% set aside for affordable housing units in any redevelopment project that receives state funding. However, a market driven development has no specific requirement unless it generates new jobs or housing units then it will generate a growth share. The current administration is re-evaluating COAH and the whole process for the entire State. As a result, it is uncertain what methodology will be used to address this growth going forward. At this time, if there is funding attached from the State then there is a mandatory 20% set aside for affordable units. Any other developments will be subject to the State’s requirements at the time the development is approved.

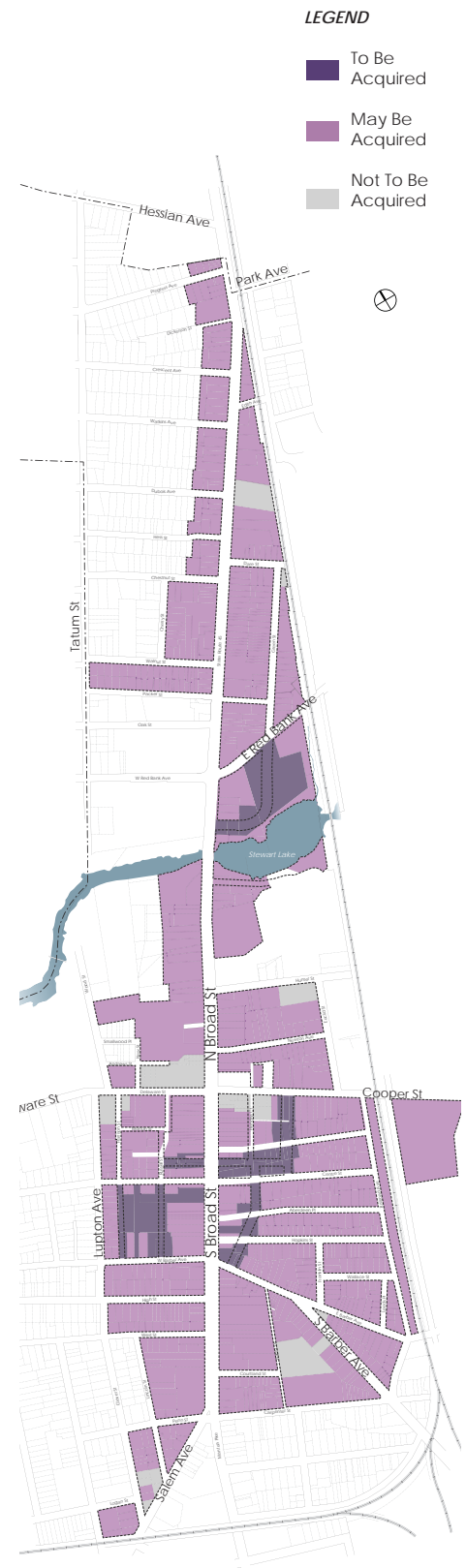


Figure 5.5: Property Acquisition Map

5.10.2 COAH Relocations

If implementation of the Redevelopment Plan includes COAH units that have been occupied anytime during the past 18 months, a strategy must be developed to account for replacing the units in question in addition to any new obligation generated. COAH units in the Redevelopment Area have been identified and included in Appendix C. The list includes both units created to meet the City's obligation as well as those created as a part of a Regional Contribution Agreement (RCA) and contains 37 properties representing 58 "credits."

6.0 FORM-BASED CODE

6.1 Purpose and Legislative Intent

The purpose and legislative intent of the *Form-Based Code*, referred to herein as "*FBC*", is to integrate the *Land Use/Transect Zone Plan*, *Street Regulating Plan*, *Public Spaces Plan* and *Gateway Plan* into a set of cohesive design regulations intended to implement the goals and objectives contained in the *Downtown Business District Redevelopment Plan*, referred to herein as the "*Redevelopment Plan*." The *FBC* is intended to create an active mixed-use, walkable downtown core that is able to support a variety of housing types; day, night and weekend commercial uses; government services; active public spaces and fixed rail transit service. It is also intended to preserve the character of Woodbury and create appropriate transitions to outlying neighborhoods, while fostering Woodbury's growing economic and residential base. Unlike traditional zoning codes, the *FBC* encourages a mix of uses and building types within transect zones and places a large emphasis on the form and function of public and private realms.

The *FBC* is not an architectural style code. It is not intended to advocate any one style in particular. Nor is it intended to promote false historicism. Rather, it is intended to guide design that promotes the development and/or redevelopment of urban fabric through the regulation of: building massing; rhythms of solid to void; building materials; parking locations; streetscapes and provisions for public spaces. In so doing, the protection and enhancement of Woodbury as a walkable and vibrant City can be advanced.

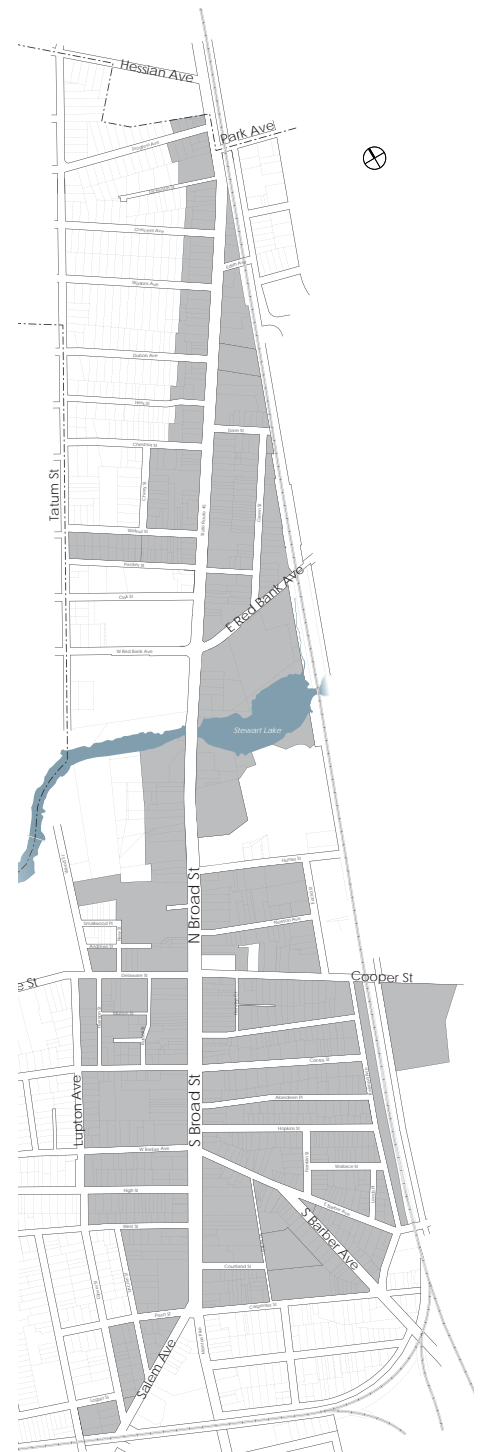


Figure 6.1: Woodbury Downtown Business District Redevelopment Area (Grey)

6.2 General Provisions

The *FBC* shall supersede any and all of the other provisions of the City Zoning Code and the Subdivision and Land Development Ordinance. Among other regulations, the *FBC* contains a regulating plan, sections on *Transect Zones*, *Building Types*, *Frontage Types* and *Development Standards and Design Standards*. In the event of any inconsistencies or contradictions, the *FBC* shall be deemed as controlling over those lands located in the Redevelopment Area and its transects. The *FBC*, as incorporated herein, shall be deemed to represent the basic land use structure of the *Redevelopment Plan* and cannot be modified without the approval of the City Council and/or the Planning/Zoning Board depending on the extent of the deviation from the provisions contained herein.

Any applicant seeking to develop properties located within the Redevelopment Area shall be required to provide a phasing schedule, which shall be subject to the review and approval of the Planning/Zoning Board. The intent of the phasing schedule shall be to establish parameters within which permitted land uses shall be allowed to construct. The Planning/Zoning Board shall have the discretion to develop a suitable mechanism to insure a balanced development of the Redevelopment Area so as to minimize fiscal impacts on the City which may otherwise be created by the proposed construction. A methodology may be established by the Planning/Zoning Board based upon a ratio of residential dwelling units to non-residential floor area or other suitable calculations which the Planning/Zoning Board deems appropriate in light of the intended goal of minimizing and modulating fiscal impacts.

This code shall not be construed to alleviate the necessity of any applicant or property owner from obtaining any and all other approvals which may be required from outside agencies, be it municipal, county or state, which otherwise may have jurisdiction over the development or redevelopment of a site.

6.3 Application Review: City Planner and/or Redevelopment Subcommittee

The City Council can choose to use the services of the City Planner and/or establish a Redevelopment Subcommittee

to review applications for the development of lands or alteration of buildings located within the Redevelopment Area. For the purposes of this ordinance, the reviewer shall be referred to as "The Planner". The Planner is hereby established and acknowledged, the purposes of whom shall be to review all plans for development and building alterations as well as variances, waivers and deviations from the *FBC Regulating Plan* as incorporated in this ordinance by reference. The Planner shall be responsible for reviewing all proposed development and building alterations for conformance to the *FBC* and providing recommendations to the Planning/Zoning Board for their approval or denial of deviations from the *FBC Regulating Plan* and any regulations contained in the *FBC*.

The Planner shall have the right to prepare and require conformance to a submission checklist, provided the same is adopted by the City Council by ordinance. The Planner shall be responsible for the review of architecture, including all materials and colors, signage, landscaping and all other design aspects.

6.4 Variance, Waiver and/or Deviation Requests

The approval or disapproval of any portion of any development or building alteration that deviates from the *FBC* shall be based on an interpretation of the effect of the proposed development on the surrounding properties, the Transect Zone and the Redevelopment Area. All requests for variances, waivers and/or deviations must also be evaluated in terms of the goals and objectives of the *Redevelopment Plan*. The regulations as established in the *FBC* shall be deemed to be the minimum standards applicable to development within the Redevelopment Area. In the event of a request for deviation from the regulations for any portion of any development, the Planning/Zoning Board, depending on the extent of the deviation, may require more stringent standards than contained in the *FBC* and conditions of approval based on the proposed effect of the deviation on the overall *Redevelopment Plan* or on specific sections of the Redevelopment Area that may be impacted by the proposed deviation, in order to protect the health, safety and welfare of the citizens of the City of Woodbury and the overall integrity of the *Redevelopment Plan*.

In those matters involving variances, waivers and/or deviations, the applicant shall be responsible for demonstrating to the Planning/Zoning Board the nature of the variances, waivers and/or deviations sought, the amount of land or building lots effected and the impact of the variances, waivers and/or deviations on the surrounding properties and neighborhoods, the Transect Zone and the Redevelopment area as a whole. Specifically in the case of variances, the applicant must provided proofs as are required by Section 40:55D-70 of the Municipal Land Use Law.

The Planning/Zoning Board may grant deviations from the regulations contained within this *Redevelopment Plan*, where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to the *Redevelopment Plan*, would result in peculiar and exceptional practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Planning/Zoning Board may also grant such relief in an application relating to a specific piece of property where the purposes of this *Redevelopment Plan* would be advanced by a deviation from the strict requirements of this Plan and the benefits of the deviation would outweigh any detriments. No relief may be granted under the terms of this section unless such deviation or relief can be granted without the substantial detriment to the public good and will not substantially impair the intent and purpose of the *Redevelopment Plan*.

Changes to the City's street pattern are identified in the *Street Regulating Plan* of the *Redevelopment Plan*. The Road Types and associated right-of-way dedication and street improvements identified in the *Street Regulating Plan* are a subset of the Frontage Types contained in the *FBC* and shall be deemed to be fixed. Any application for deviation from this pattern shall be subject to the review and approval of the Planning/Zoning Board unless said deviation is attributed to on-site engineering constraints certified by the City Engineer and results in only a minor relocation of the street pattern. In the event of such certification by the City Engineer, the applicant shall only be required to submit the proposed deviation to the Planning/Zoning Board for its review and action.

Any application seeking a deviation from the provisions of the *FBC* shall first and foremost be treated as a permitted use which may be subject to conditions at the time of approval by the Planning/Zoning Board. All applications for such deviations shall be evaluated in terms of the vision articulated in the *Redevelopment Plan* and the reasons for said deviation shall be established in any determination made by the Board. Proposed deviations shall be reviewed and approved or denied by the Planning/Zoning Board based upon the following criteria:

- A. The design and improvement shall be in harmony with the Plan Purpose of this Plan.
- B. The design and improvement shall generally enhance the street and/or building regulating plans, or in any case, not have an adverse impact on its physical, visual, or spatial characteristics.
- C. The design and improvements shall generally enhance the streetscape and neighborhood, or in any case, not have an adverse impact on the streetscape and neighborhood.
- D. The proposed modification(s) shall not result in configurations of lots or street systems which shall be impractical or detract from the appearance of the Redevelopment Area.
- E. The proposed modification(s) shall not result in any danger to the public health, safety, or welfare by making access by emergency vehicles more difficult, by depriving adjoining properties of adequate light and air, or by violating the purposes for which land development ordinances are intended to be enacted.
- F. The lot size of any lot to be created shall only be reduced below the requirements of the Code by approval of the Planning/Zoning Board.
- G. Landscaping and other methods shall only be used to insure compliance with the design standards and guidelines of the Code.

- H. The applicant shall be required to demonstrate that the proposed modification will allow for equal or better results and represents the minimum modification necessary.
- I. In the event of the granting of a deviation, the Planning/Zoning Board may impose such conditions it deems necessary to permit the deviation while insuring the integrity of the overall Redevelopment Plan.

6.5 Interpretation of Development Standards and Guidelines

The development standards as contained in the *FBC* are both written and illustrated. The illustrations and written text are intended to be complementary and in the event that an apparent inconsistency exists, the applicant may apply to the Planning/Zoning Board for interpretation of the ordinance regarding the regulation in question. Any interpretation made by the Planning/Zoning Board shall be made in conjunction with the overall intent and character of the *Redevelopment Plan* as codified in the *FBC*.

6.6 Geography

The *FBC* applies to the whole of the *Downtown Business District Redevelopment Area*. The *Redevelopment Area* comprises a large portion of Woodbury along the Broad Street Corridor from the North End of the City near Progress Avenue to the South End near Salem Avenue. The North and South Ends of Woodbury are separated by Woodbury Creek and Stewart Lake. The North End is anchored by Underwood Memorial Hospital. Uses adjacent to Underwood include medical offices, low-density “Main Street” commercial and small lot single family dwellings. A light rail station along the proposed Camden to Glassboro route would be located on Red Bank Avenue, just east of Broad Street.

The South End of Woodbury is anchored by Gloucester County government facilities and “Main Street” retail uses along South Broad Street. Adjacent uses include law and other professional offices, auto oriented commercial, multifamily apartment buildings and small lot single family dwellings. A light rail station along the proposed Camden to

Glassboro route would be located near Cooper Street, just east of Broad Street. *Figure 6.2* shows an aerial perspective of the Redevelopment Area. A comprehensive list of the parcels located within the Redevelopment Area is contained in *Appendix A* of the *Redevelopment Plan*.

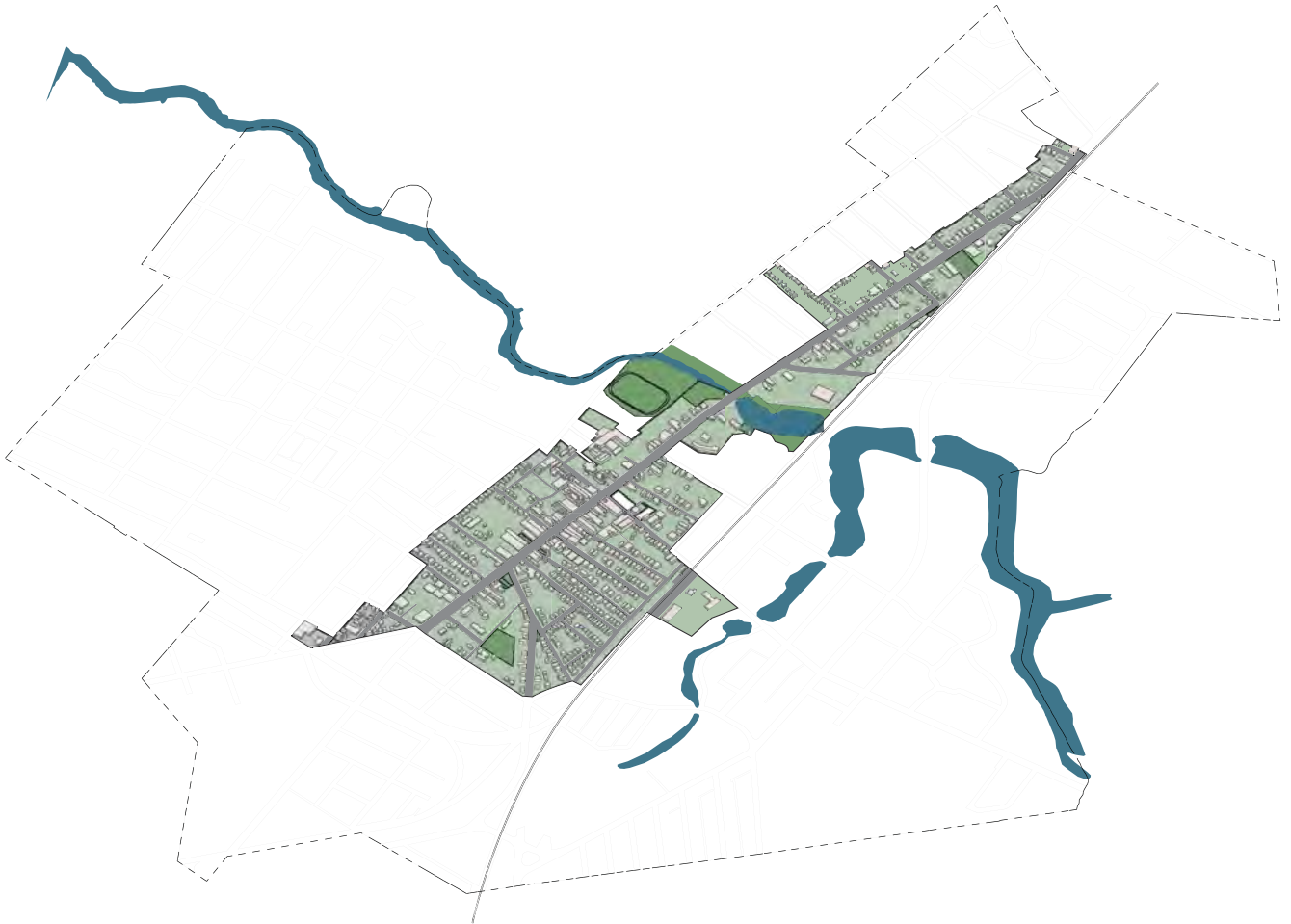
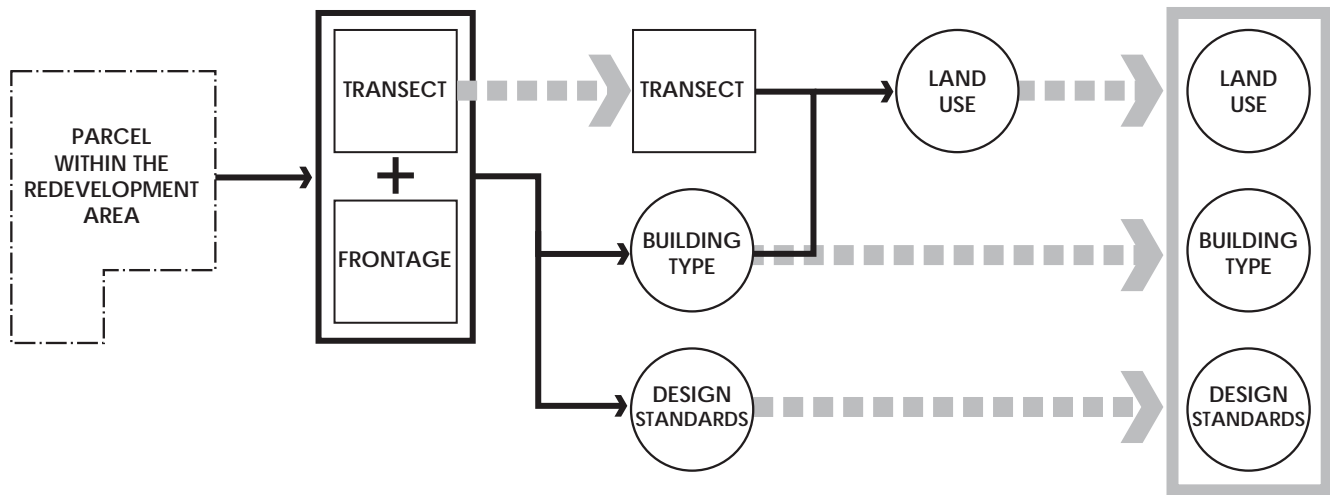


Figure 6.2: Aerial perspective of the Woodbury Downtown Business District Redevelopment Area



1.

Locate your parcel within the Redevelopment Area in the REGULATING PLAN (sec 6.8) to determine the TRANSECT in which it lies and the type of street FRONTAGE it faces.

2.

Using the DEVELOPMENT STANDARDS Table for the appropriate TRANSECT (sec 6.15), use the FRONTAGE column to determine the BUILDING TYPES that are allowed as well as the DESIGN STANDARDS for all buildings along the FRONTAGE.

3.

Review the LAND USES allowed in each TRANSECT in Permitted Uses (sec 6.9) AND use the BUILDING TYPE descriptions (sec 6.13) to determine the USES generally allowed in each BUILDING TYPE.

4.

Use the LAND USE, BUILDING TYPE, DEVELOPMENT STANDARDS, and DESIGN STANDARDS (sec 6.15-6.18) as the regulatory requirements for development within the parcel.

Figure 6.3: User instructions

6.7 User Instructions

The *FBC* is applicable to development of new structures, additions to existing structures and exterior renovations of existing structures on all parcels located within the Redevelopment Area.

The regulations presented within the *FBC* address the location and density of buildings types; their interaction with public right-of-ways; and site planning and architectural considerations that help to implement the Vision of the *Redevelopment Plan*. These regulations and standards are determined by Transect, Building Type, and Street Frontage. *Figure 6.3* provides a flow chart describing how to determine Land Use, Building Type, Development Standards and Design Standards from the location of a property within the Redevelopment Area. There are four steps, marked along the bottom of the chart, to move from location to regulations.

The property's dominate street frontage should be used to determine both the transect location and the street frontage type. Once the regulations for the specific site have been determined, the General Design Standards should be reviewed to understand the rules that apply across the Redevelopment

Area, regardless of Transect, Frontage, and Building Type. Additionally, if the site abuts multiple street frontage types on secondary or tertiary building sides, pedestrian amenity regulations for those respective frontages must be followed (see Section 6.12).

6.8 Form-Based Code Regulating Plan

While the *Redevelopment Plan* presents a comprehensive vision for the entire Broad Street Corridor, it is recognized that the Redevelopment Area is comprised of various districts and public right-of-ways that have numerous forms and functions. The *FBC Regulating Plan Map* presented in *Figure 6.4* codifies the location of the “Transect Zones” and “Frontage Types” of the Redevelopment Area, which determine the permitted building types and design regulations for any given parcel in the Redevelopment Area. All Transect Zones and Frontage Types are deemed to be fixed.

The Redevelopment Area shall be comprised of three individual Transect Zones which have been planned and designed based upon their respective existing and desired road layout, utilities, densities and land uses so as to promote the vision contained in the *Redevelopment Plan*. The Transect Zones are set forth in the *FBC Regulating Plan Map* which is appended hereto and incorporated herein by reference. The permitted uses for each Transect Zone of the *FBC* are set forth in *Section 6.9, 6.10, and 6.11* of the *FBC*. The permitted Building Types for Transect Zone of the *FBC* are set forth in *Section 6.15* of the *FBC*.

Changes to the City's street pattern as identified in the *Street Regulating Plan* of the *Redevelopment Plan* are incorporated into the *FBC Regulating Plan*. The Road Types identified in the *Street Regulating Plan* are a subset of the Frontage Types. The Redevelopment Area shall be comprised of seven individual Frontage Types which have been planned and designed based on their respective public right-of-way's existing and desired width, road hierarchy, building scales and pedestrian amenities so as to promote the vision contained in the *Redevelopment Plan*. The Frontage Types are set forth in the *FBC Regulating Plan Map* which is appended hereto and incorporated herein by reference.



Figure 6.4: Form Based Code Regulating Plan Map

6.9 Permitted Uses

The following uses are permitted by Transect within the Downtown Business District Redevelopment Area. A mix of uses is permitted within Building Types designated as such. Unless expressly permitted in this list, all other uses are deemed to be prohibited:

| | | TRANSECT | | |
|----------------------------------|--|----------|----|----|
| USE | | DT | LW | NB |
| Civic/Institutional | | | | |
| 1 | Community garden | P | P | P |
| 2 | Emergency services | P | P | NP |
| 3 | Green/plaza/square/piazza | P | P | NP |
| 4 | Hospital/medical center | P | NP | NP |
| 5 | Cultural uses (museum/gallery/library) | P | P | NP |
| 6 | Park | P | P | P |
| 7 | House of worship | P | P | P |
| 8 | Public/private/parochial school | P | P | NP |
| 9 | Recreational facility | NP | P | NP |
| P – Permitted NP – Not Permitted | | | | |

| | | TRANSECT | | |
|--|--|----------|----|----|
| USE | | DT | LW | NB |
| Commercial | | | | |
| 10 | Business services | P | P | P* |
| 11 | Instructional activities | P | P | P* |
| 12 | Laboratory (medical/dental) | P | P | NP |
| 13 | Entertainment/amusement | P | P | NP |
| 14 | Childcare center | P | P | NP |
| 15 | Hotel | P | P | NP |
| 16 | Financial services | P | P | P* |
| 17 | Office (professional, dental, medical, veterinary, administrative) | P | P | P* |
| 18 | Restaurant (full service/sit down, carry out, fast food) | P | P | NP |
| 19 | Retail sales | P | P | NP |
| 20 | Retail services | P | P | NP |
| 21 | Theater (motion picture, stage play) | P | P | NP |
| Government | | | | |
| 22 | Judicial | P | P | NP |
| 23 | Law enforcement | P | P | NP |
| 24 | Office | P | P | NP |
| 25 | Emergency Services | P | P | NP |
| 26 | Public works | NP | P | NP |
| Industrial | | | | |
| 27 | Artisanal workshop | P | P | P |
| 28 | Light industry | NP | P | NP |
| 29 | Research and development | P | P | NP |
| P – Permitted NP – Not Permitted | | | | |
| *Permitted in Live/Work Building only and only along Collector Streets | | | | |

| | | TRANSECT | | |
|----------------------------------|--|----------|----|----|
| USE | | DT | LW | NB |
| Parking/Transportation | | | | |
| 30 | Bus stop/transit shelters | P | P | P |
| 31 | Rail station | P | NP | NP |
| Residential | | | | |
| 32 | Flats or Lofts (elevator fed) | P | P | NP |
| 33 | Single family detached unit | NP | NP | P |
| 34 | Special unit (elder-care, extended care, special care) | NP | P | NP |
| 35 | Townhouse unit | NP | P | P |
| 36 | Twin unit | NP | P | P |
| Other | | | | |
| 37 | Club, lodge or fraternal organization | P | P | NP |
| 38 | Farmers Market | P | NP | NP |
| 39 | Live/work | P | P | P |
| P – Permitted NP – Not Permitted | | | | |

6.10 Accessory Uses

The following accessory uses are permitted in the Downtown Business District Redevelopment Area. Unless expressly permitted in this list, all other uses are deemed to be prohibited as accessory uses:

| | USE | TRANSECT | | |
|----------------------------------|--|----------|----|----|
| | | DT | LW | NB |
| 1 | Accessory Apartment | NP | NP | P |
| 2 | Carport | NP | NP | P |
| 3 | Private Garage | P | P | P |
| 4 | Storage Shed | P | P | P |
| 5 | Parking Structure | P | NP | NP |
| 6 | Surface Parking Lot | P | P | NP |
| 7 | Sidewalk Café | P | P | NP |
| 8 | Home Occupation | NP | P | P |
| 9 | Street Furniture | P | P | P |
| 10 | Fences and Walls | P | P | P |
| 11 | Porch, Deck, and Patio | P | P | P |
| 12 | Arbor and Trellis | P | P | P |
| 13 | Signs | P | P | NP |
| 14 | Loading Zones | P | P | NP |
| 15 | Garbage Collector Areas | P | P | NP |
| 16 | Satellite Dish Antenna | P | P | P |
| 17 | Drive-Through Facility | P | P | NP |
| 18 | Living Fence | P | P | P |
| 19 | Wind, Solar or Photovoltaic Energy Facility or Structure | P | P | P |
| 20 | Wireless Antenna on Roof | P | P | NP |
| 21 | Private Swimming Pool | NP | NP | P |
| P – Permitted NP – Not Permitted | | | | |

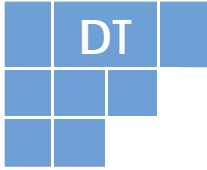
6.11 Prohibited Uses

The following land uses are prohibited in all transects and any uses determined by the Planning/Zoning Board to be of a similar nature to the prohibited uses listed below:

Tattoo parlors, massage parlors, sexually oriented businesses, pawn shops, drug paraphernalia shop or head shop, drug rehabilitation clinic or substance abuse treatment facility or similar shops, uses or services.

Funeral home or mortuary, boardinghouse, rooming house, gasoline service station or filling station, motor vehicle sales, service and repair, self-storage facility or mini-warehouse, laundromats and wireless telecommunication towers or similar shops, uses or services.

6.12 Transect Zones



Transect Zone

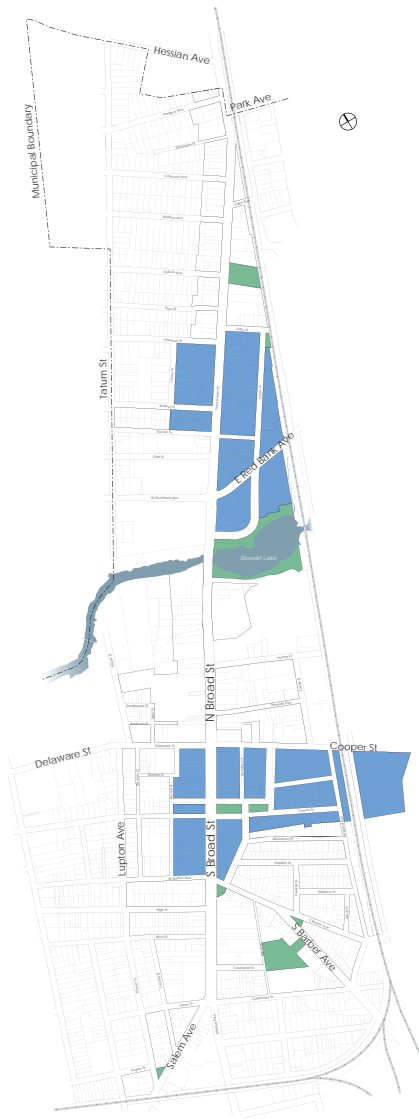
6.12.1 Downtown Transect

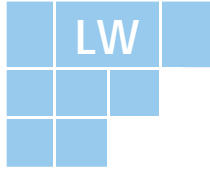
A. Purpose/Definition

Intended to serve as the primary shopping, professional office, medical, cultural and entertainment district of the City. This transect is envisioned to be the highest density district of the City and consist of mixed-use buildings up to five (5) stories high with active ground floor commercial uses that support a full spectrum of community needs and upper floor residential and commercial uses. Such density is essential to support fixed rail transit and day, night and weekend commercial uses. Streetscapes are intended to accommodate heavy pedestrian activity and as such are to be characterized by wide sidewalks, sidewalk cafes and high quality pedestrian amenities.

For redevelopment purposes, a minimum parcel or tract size of 80,000 square feet is required. Once redeveloped, parcels may be subdivided into tax lots as prescribed in Section 6.13, Building Types.

B. Typical Block Densities





Transect Zone

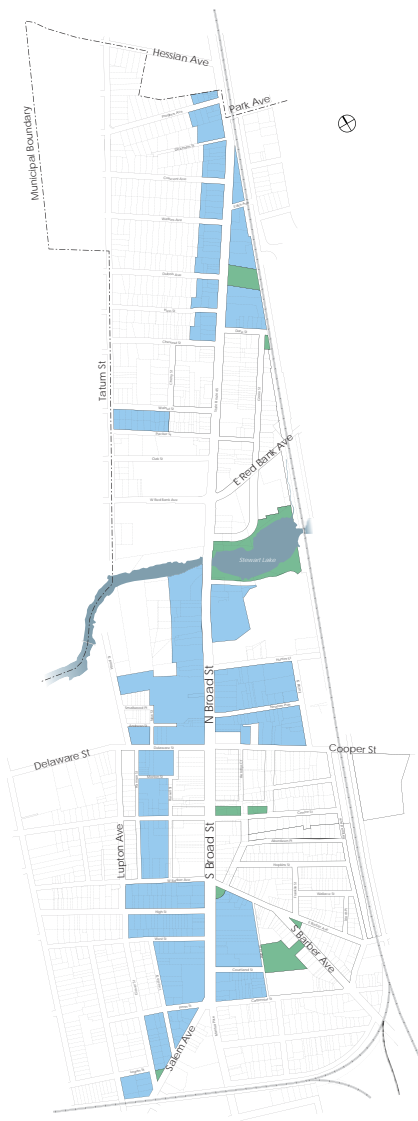
6.12.2 Live-Work Transect

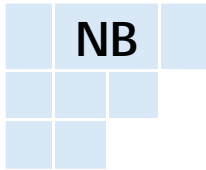
A. Purpose/Definition

Intended to serve as a primarily urban residential district with accommodation for some small scale neighborhood commercial functions. The transect is generally located at the periphery of the Downtown Transect and is purposed to act as a transition to lower density residential neighborhoods. It is envisioned to be a mid to high density area of the City and consist primarily of attached residential building types with some ground floor neighborhood commercial functions. Streetscapes are intended to accommodate moderate pedestrian activity.

For redevelopment purposes, a minimum parcel or tract size of one acre is required. Once redeveloped, parcels may be subdivided into tax lots as prescribed in Section 6.13, Building Types.

B. Typical Block Densities





Transect Zone

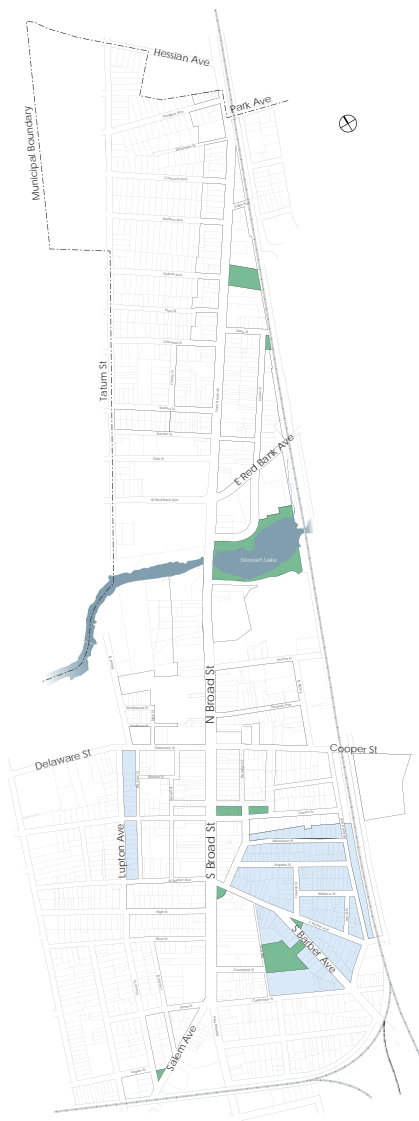
6.12.3 Neighborhood Transect

A. Purpose/Definition

Intended to serve as a low density residential district with some small scale neighborhood commercial functions and also act as a transitional zone between mid to high density uses and existing neighborhoods on the City's periphery. This transect is envisioned to be a low to mid density district of the City and consist primarily of single family dwellings and semi-detached residential building types with some ground floor and auto-oriented neighborhood commercial functions. Streetscapes are intended to accommodate low to moderate pedestrian activity.

For redevelopment purposes, a minimum parcel or tract size of one acre is required. Once redeveloped, parcels may be subdivided into tax lots as prescribed in Section 6.13, Building Types.

B. Typical Block Densities





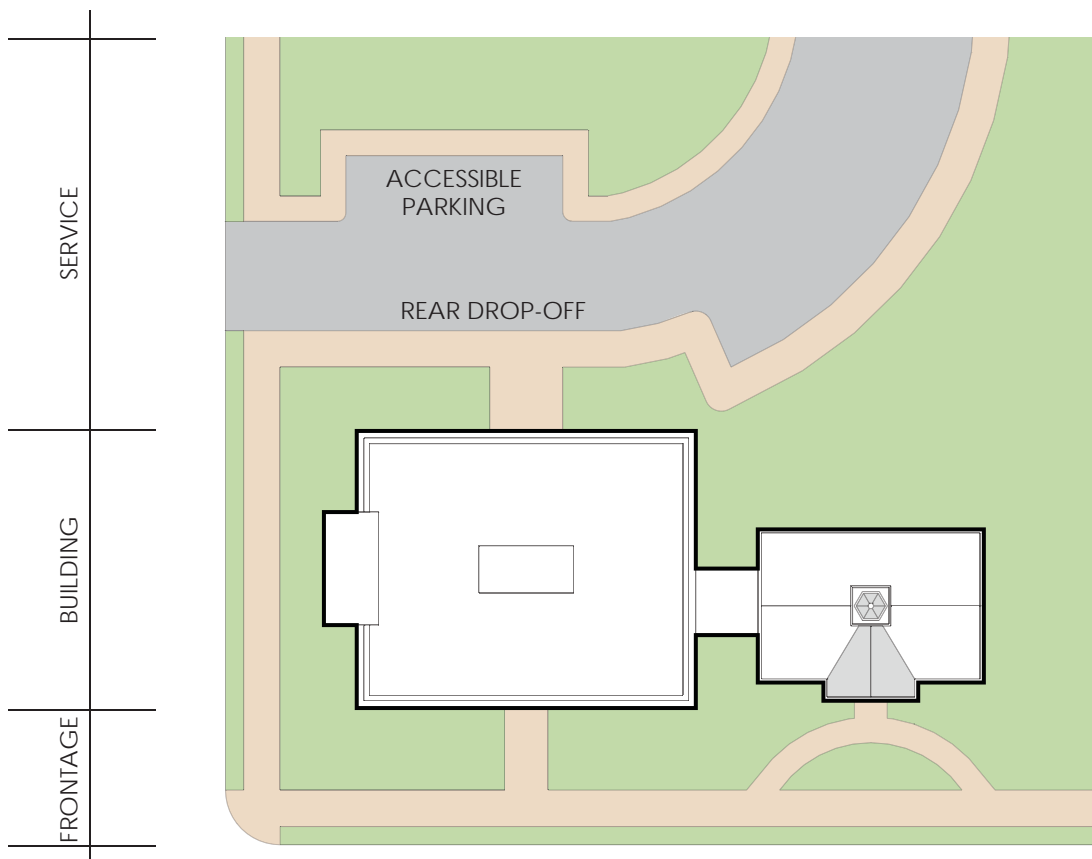
Building Type

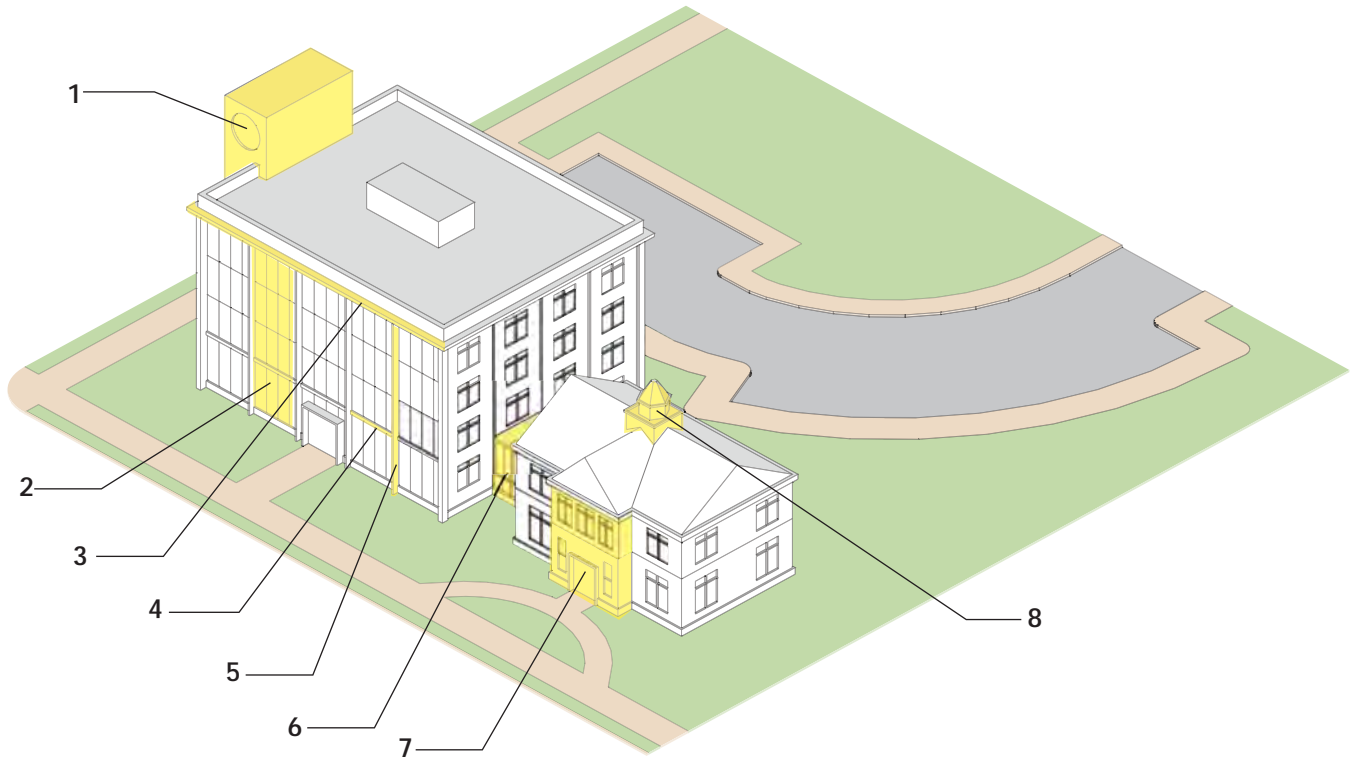
6.13.1 Government/Civic/Institutional

A. Purpose/Definition

A building that contains governmental, civic, institutional or religious uses. They are typically significant buildings that may have large setbacks, monumental and unique design characteristics, public art installations and/or public space areas. Examples include courthouses, government administration offices, schools, libraries, museums and places of worship. They do not contain commercial, residential or industrial uses and are typically fully detached. Main entrances must face the primary street frontage. In no instance are they to face side or rear yard parking lots.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: horizontal window mullions (4), horizontal bands (3), tower elements (1), awnings and overhangs

Horizontal Breaks: vertical window mullions (5), vertical window band (2), tower elements (1), cupola (8), building offsets (7), building connectors (6)

Pedestrian Accommodations: street entrance, parking/drop off entrance, covered entryways

Open Space: front setback, side setback

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|----------|
| A | Min Lot Size | 1 acre |
| B | Min Width | 100 feet |
| C | Max Width | 200 feet |
| D | Min Depth | 70 feet |
| E | Max Depth | - |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials

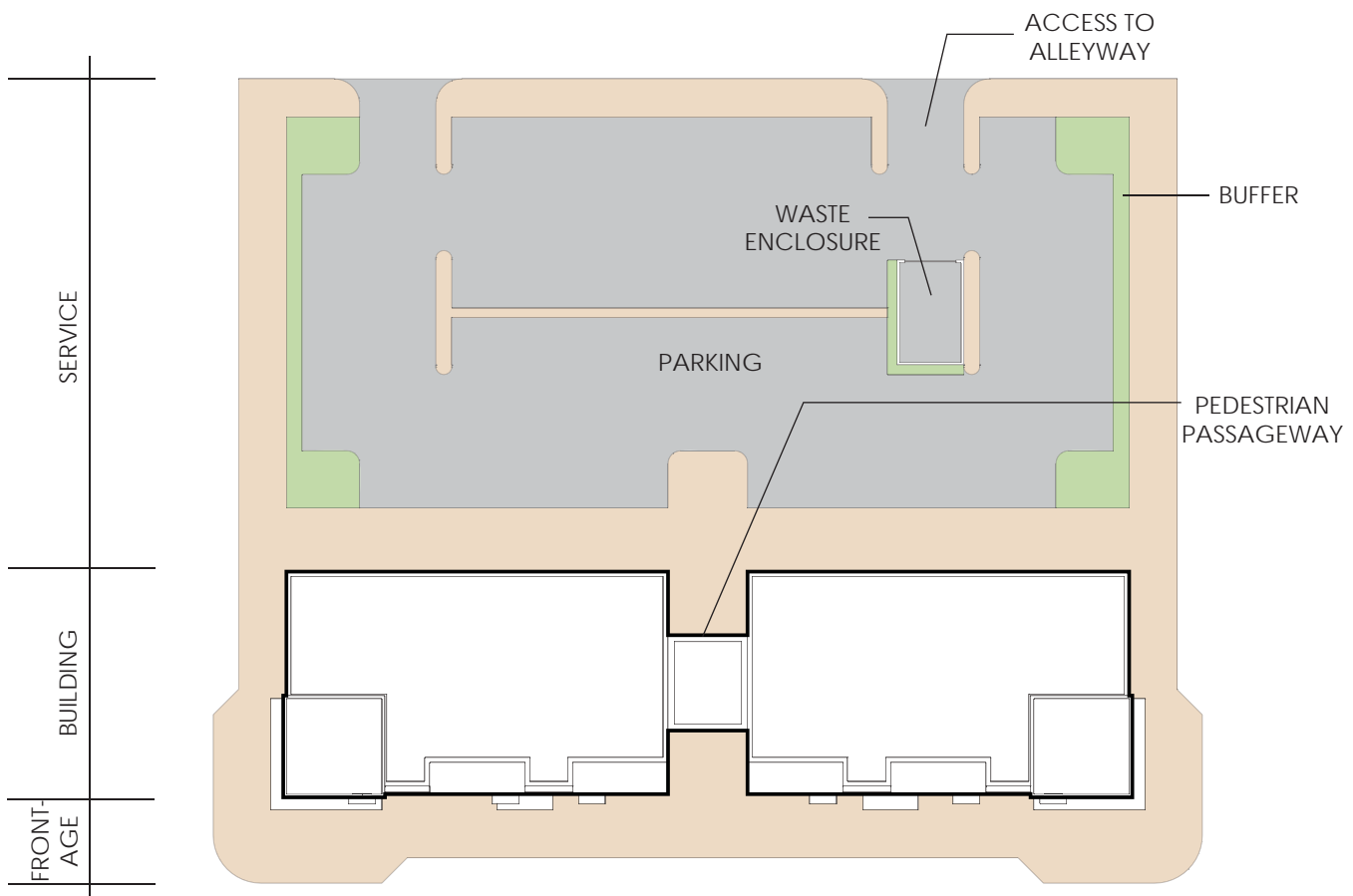
Building Type

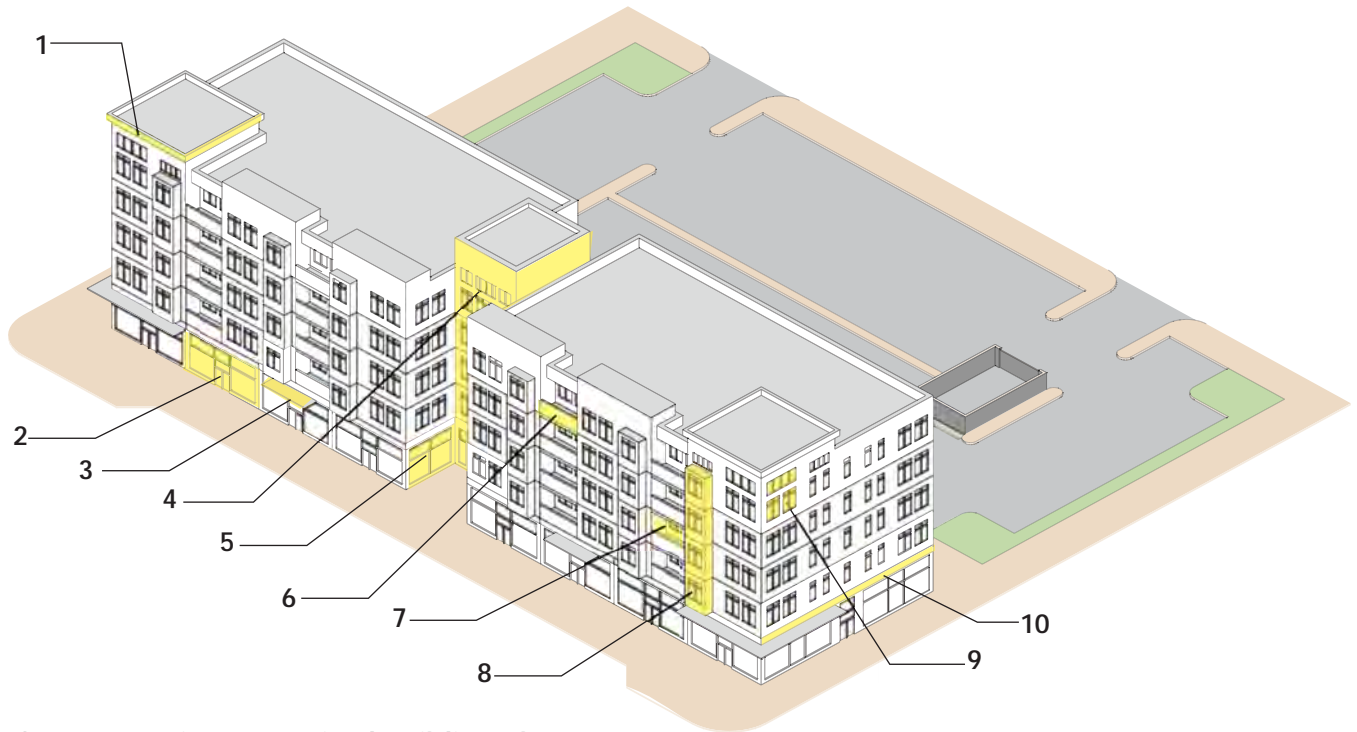
6.13.2 Flex Mixed Use

A. Purpose/Definition

A multi story building that contains ground floor retail uses, with upper floor residential, retail, office or other commercial uses. These buildings may be attached, semi-attached or detached. They are served by elevators and intended for the highest density areas of the City. Common entrances and other spaces may be shared. They shall be designed as elevator buildings. Residential access may be provided from internal courtyards or corridors. Access to retail must be provided from the primary street frontage as well as parking areas in the rear. Parking is permitted inside the ground floor of the building but may not abut the primary public frontage.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: awnings or overhangs (3), horizontal bands or entablature (10), balconies or balconettes (6), offset roof line (1)

Horizontal Breaks: bay windows (8), vertical windows (9), building offsets (4), building connectors (4)

Ground Floor: commercial storefronts (2), awnings or overhangs (3), windows facing pedestrian passageways (5), vertically-oriented window openings for private spaces

Upper Floor: vertically-oriented window openings (reflective of function) (9), private or common outdoor spaces (7)

Pedestrian Accommodations: mid-building passages (4), windows facing passageways (5), awnings or overhangs (3), retail entrances from parking lot

Open Space: private balconies (7), common terraces or roof gardens, mid-building passages (4)

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|-----------|
| A | Min Lot Size | 10,000 sf |
| B | Min Width | 100 feet |
| C | Max Width | 200 feet |
| D | Min Depth | 70 feet |
| E | Max Depth | 90 feet |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials



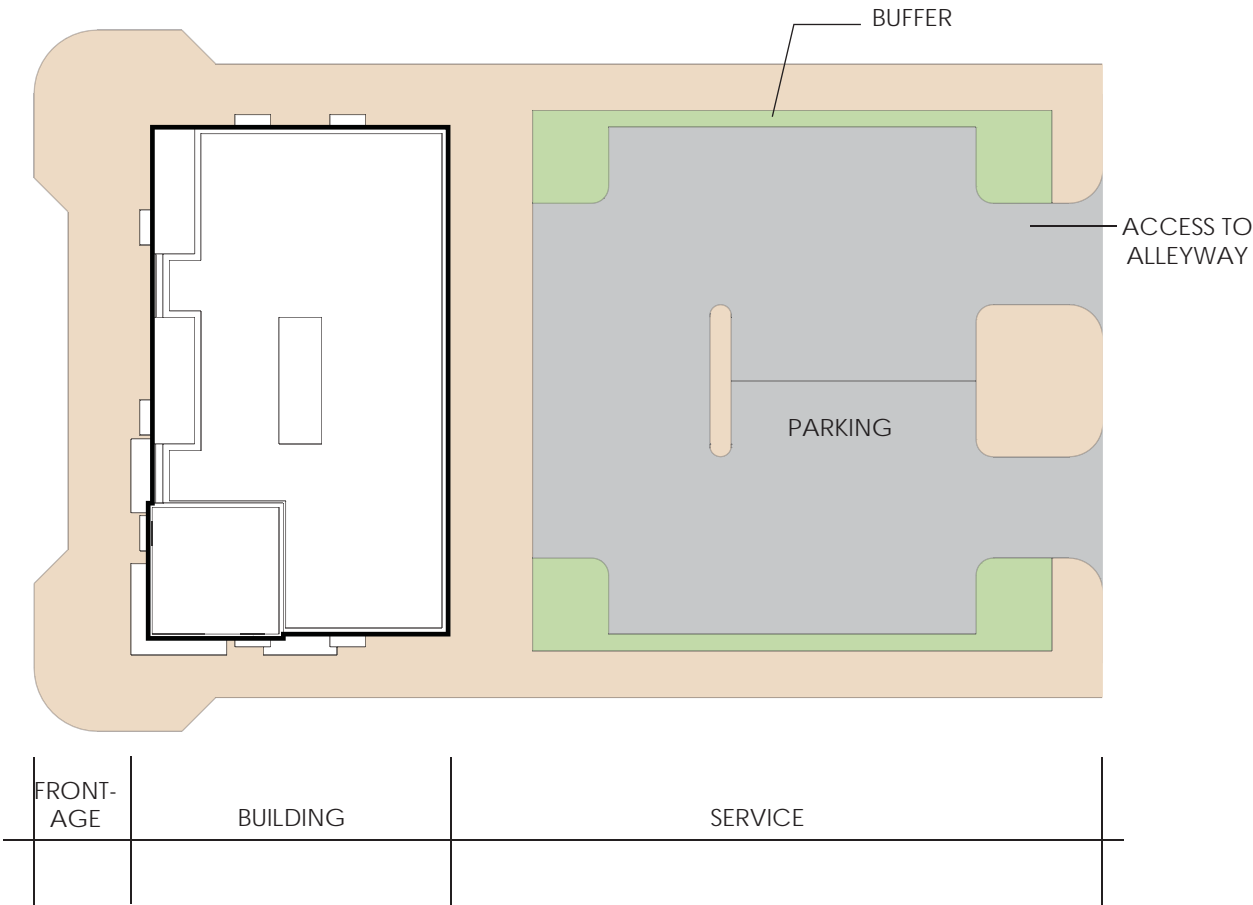
Building Type

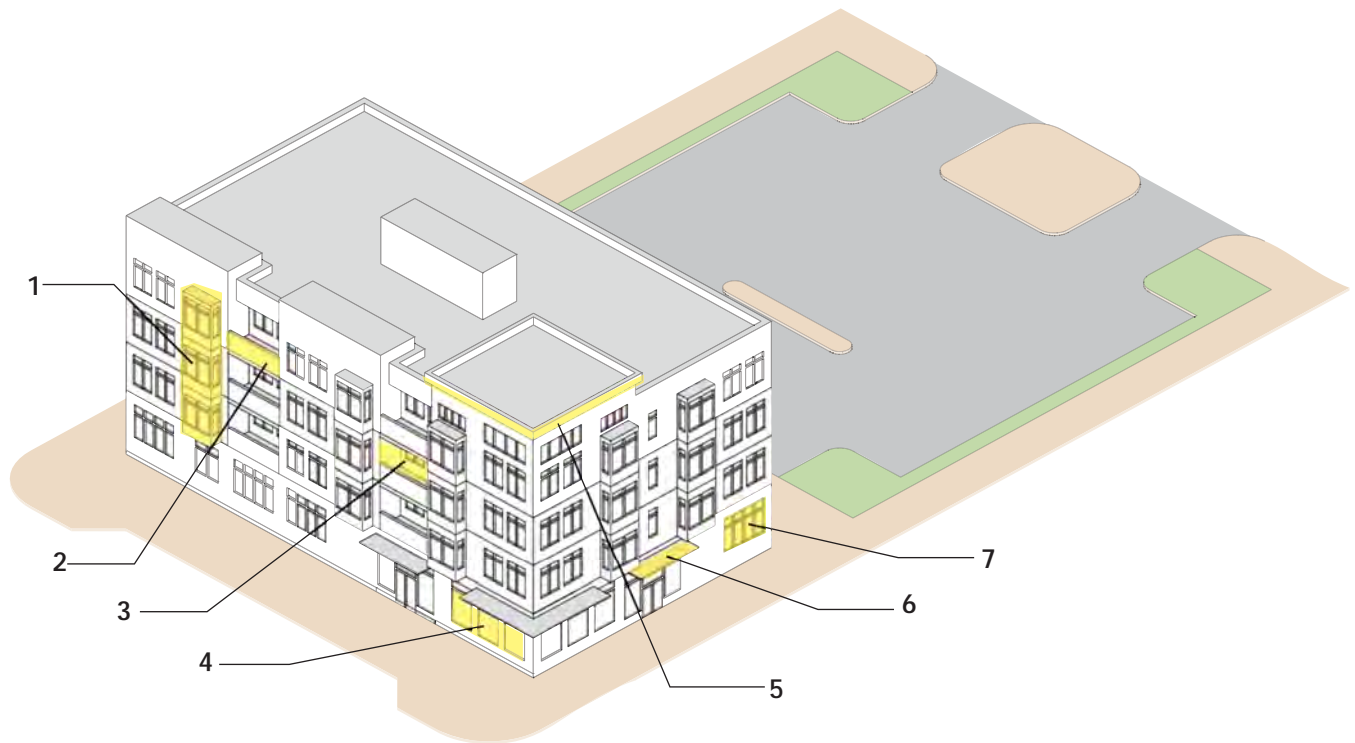
6.13.3 Elevator Flats

A. Purpose/Definition

A multistory building devoted solely to multifamily dwelling units in which households live independently but may share common entrances and other spaces. They shall be served by elevators. The ownership structure may be condominium or rental. Individual unit access may be provided from the primary street frontage, courtyards, side yard areas or internal corridors. Parking is permitted inside the ground floor of the building but may not abut the primary street frontage.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: awnings or overhangs (6), horizontal bands, balconies or balconettes (2), offset roof line (5)

Horizontal Breaks: bay windows (1), vertical windows, building offsets

Ground Floor: lobby entrances facing street, awnings or overhangs (6), storefront windows for semi-public spaces (4), vertically-oriented window openings for private spaces (7)

Upper Floor: vertically-oriented window openings (reflective of function), private or common outdoor spaces (3)

Pedestrian Accommodations: building entrances facing street, mid-building passages, windows facing passageways, awnings or overhangs (6)

Open Space: private balconies (3), common terraces or roof gardens, mid-building passages

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|-----------|
| A | Min Lot Size | 10,000 sf |
| B | Min Width | 100 feet |
| C | Max Width | 200 feet |
| D | Min Depth | 70 feet |
| E | Max Depth | 90 feet |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

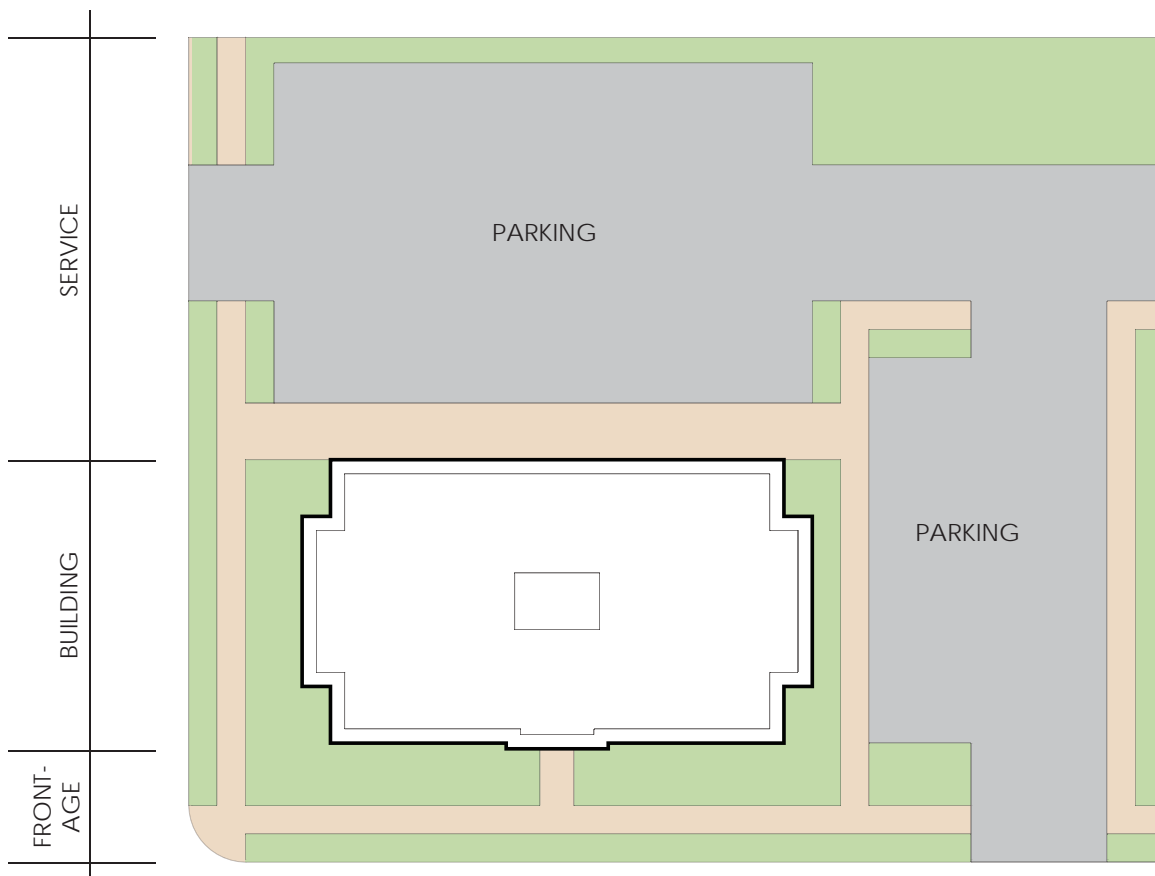
See Section 6.20 Definitions for further clarification of materials

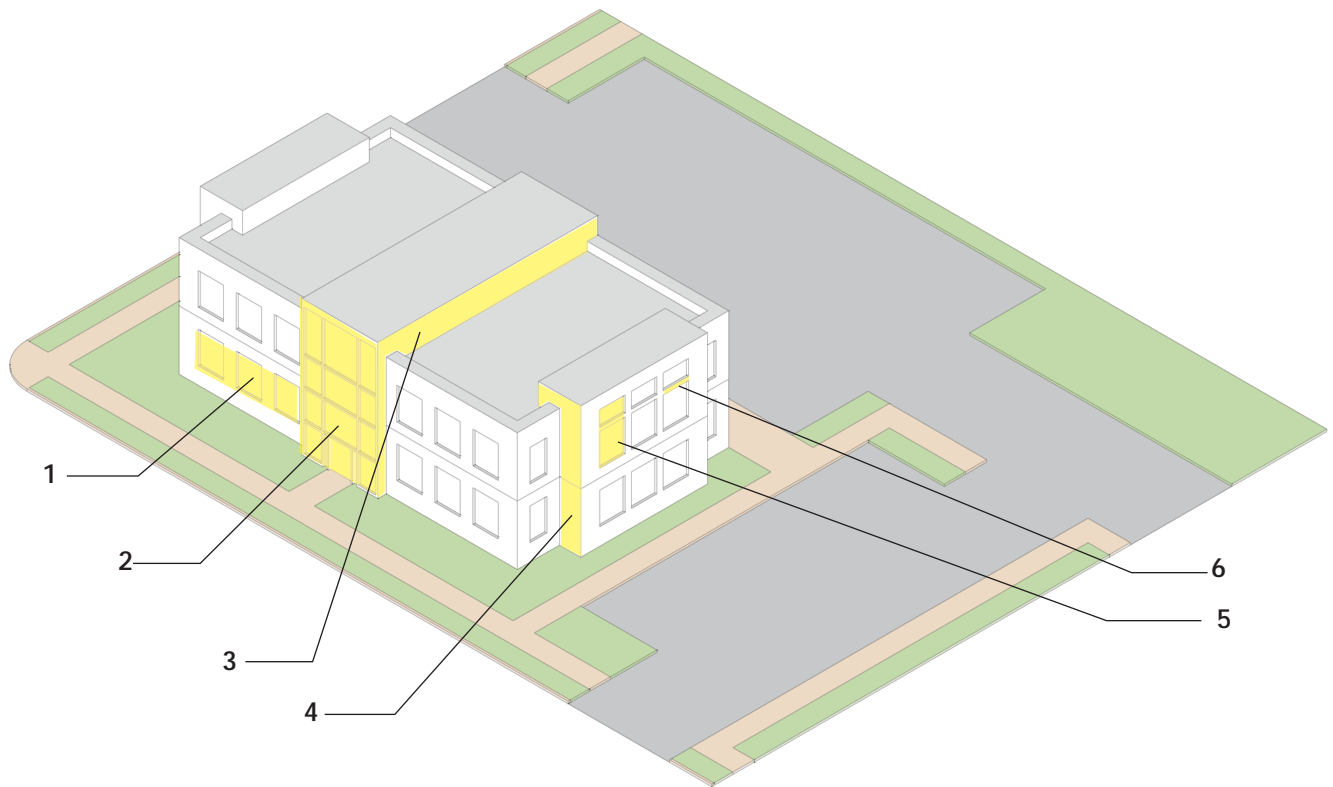
Building Type

6.13.4 Commercial

A. Purpose/Definition

A single or multi story building devoted solely to commercial uses. They may be designed as walkups or elevator buildings depending on the number of floors. The ownership structure may be condominium or rental. Common entrances and other spaces may be shared. Primary access is to be provided from the primary street frontage. Additional access is permitted from courtyards or internal corridors. Parking is permitted inside the ground floor of the building but may not abut the primary street frontage. Side yard parking equal to one row of parking plus drive aisle is permitted to a maximum of 45 feet.

B. Plan: Required Site Elements



C. *Massing: Required Building Elements*

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: horizontal window band (1), window mullions, offset roof line (3)

Horizontal Breaks: window mullions (6), vertical windows (2, 5), building offsets (4)

Ground Floor: storefronts, building entrances (2)

Upper Floor: window openings reflective of function

Pedestrian Accommodations: building entrances facing street (2), mid-building passages, windows facing passageways, awnings or overhangs

Open Space: front setback, side setback

D. *Permitted Building Dimensions*

| | DIMENSION | STANDARD |
|---|--------------|-----------|
| A | Min Lot Size | 10,000 sf |
| B | Min Width | 100 feet |
| C | Max Width | 200 feet |
| D | Min Depth | 70 feet |
| E | Max Depth | - |

E. *Permitted Façade Materials*

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials



Building Type

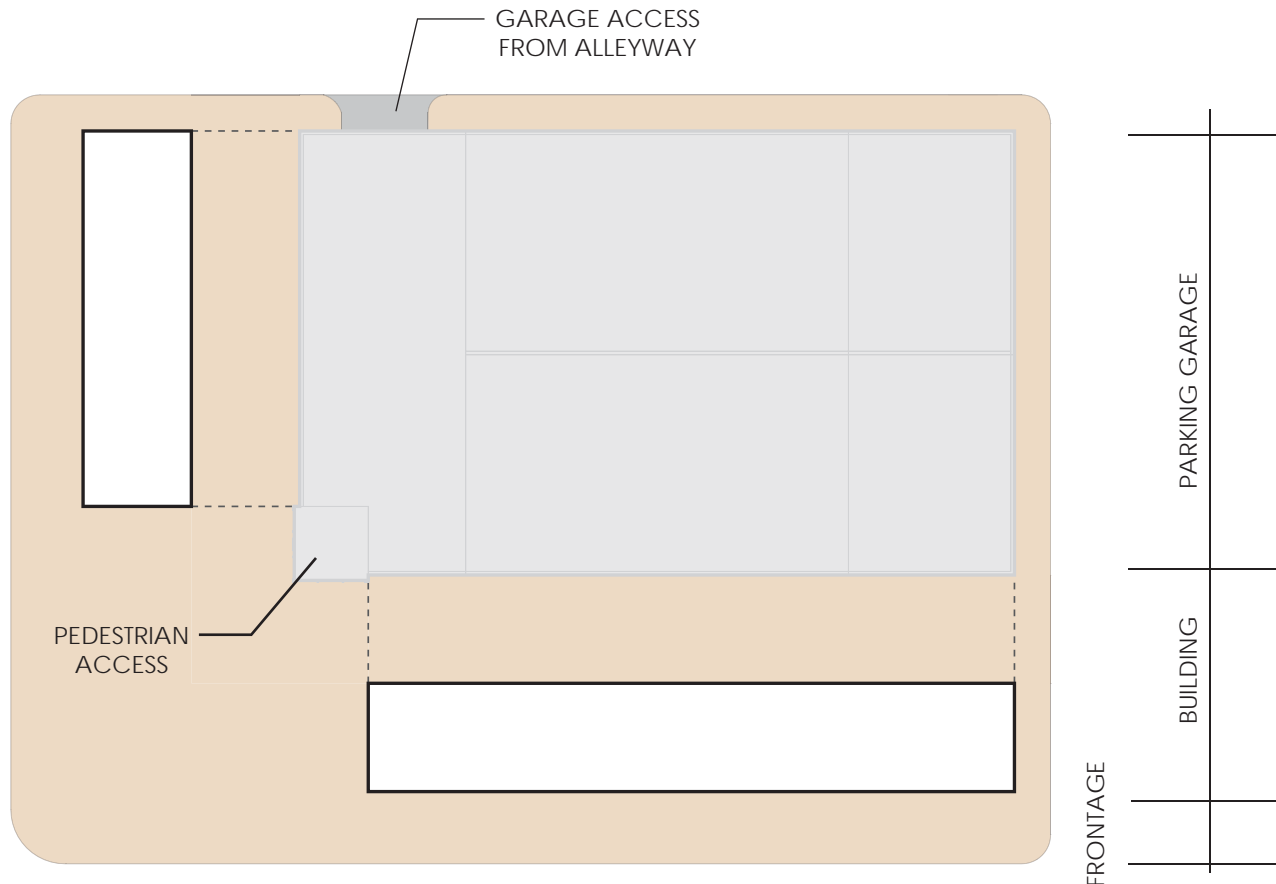
6.13.5 Liner

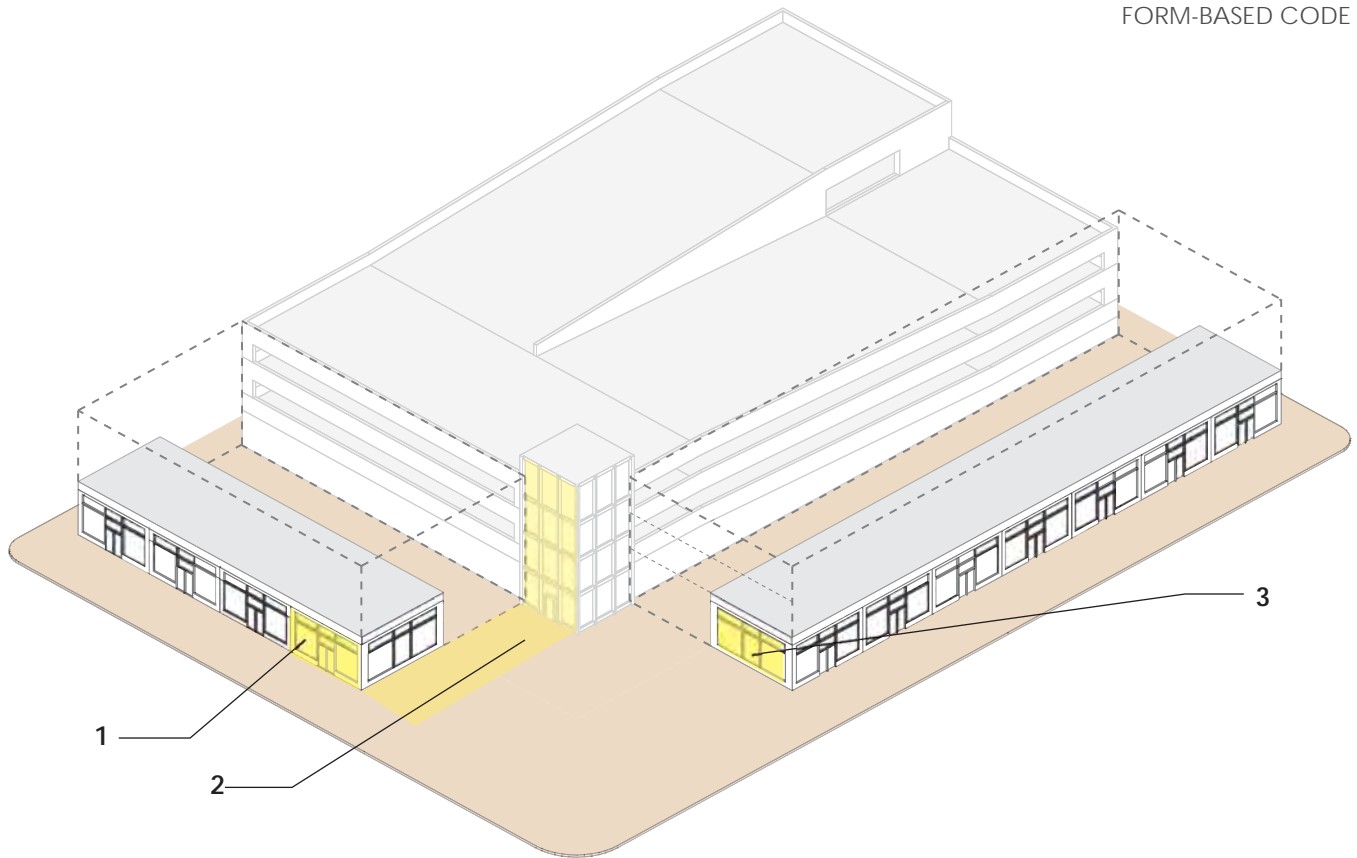
A. Purpose/Definition

A single or multi story building devoted to commercial or residential uses. Their purpose is to “line” the exteriors of large structures such as parking garages, movie theaters or supermarkets. This allows large, blank façades to be screened by pedestrian scaled uses. The building may be attached or integrated into the larger structure, or sit in front of it, with separations dictated by building codes. Individual unit access is to be provided from the primary street frontage. Secondary access may be provided from the building that is being “lined”.

B. Plan: Required Site Elements

C. Massing: Required Building Elements





Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: awnings or overhangs, horizontal bands, balconies or balconettes, offset roof line

Horizontal Breaks: bay windows, vertical windows, building offsets, building connectors

Ground Floor: commercial storefronts (1), awnings or overhangs, windows facing pedestrian passageways (3), vertically-oriented window openings for private spaces

Upper Floor: vertically-oriented window openings (reflective of function), private or common outdoor spaces

Pedestrian Accommodations: mid-building passages to rear structure, (2), windows facing passageways (3), awnings or overhangs

Open Space: private balconies, common terraces or roof gardens, mid-building passages (2)

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|----------|
| A | Min Lot Size | 3500 sf |
| B | Min Width | 120 feet |
| C | Max Width | - |
| D | Min Depth | 30 feet |
| E | Max Depth | - |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials



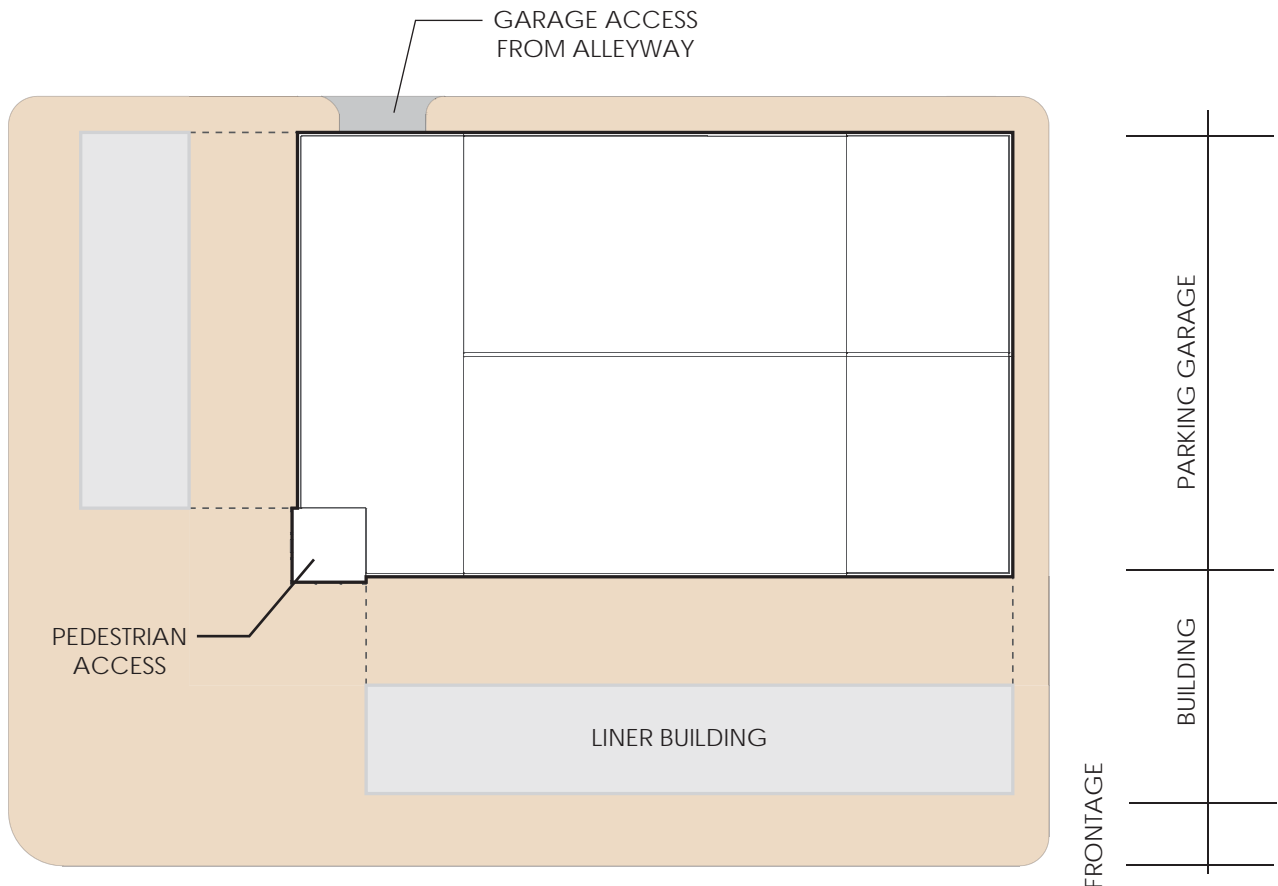
Building Type

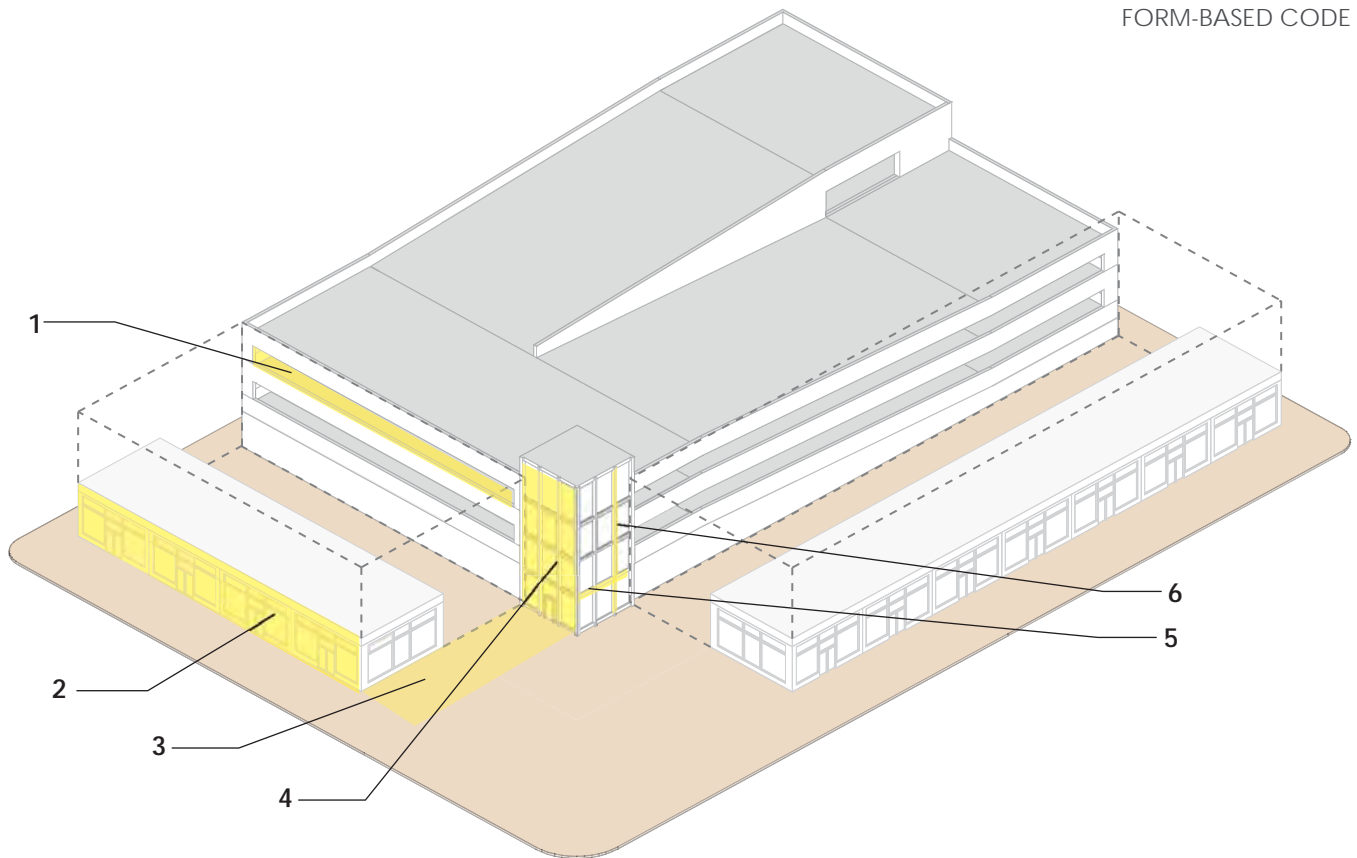
6.13.6 Parking Structure

A. Purpose/Definition

A building or part thereof which is designed specifically for automobile parking and where there are a number of floors or levels on which parking takes place. Parking structures are to be located internal to a block and not permitted on any street frontages (with the exception of a Small Frontage Type) without liner buildings (see Section 6.13.5).

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: awnings or overhangs, horizontal bands, offset roof line, wall openings (1), window mullions (5)

Horizontal Breaks: circulation/access elements (4), building offsets, building connectors, window mullions (6)

Ground Floor: car and pedestrian building entrances (4), liner building (2)

Upper Floor: liner building (2), wall openings (screened on public frontages) for ventilation (1)

Pedestrian Accommodations: mid-liner-building passages to rear structure, (3), windows facing passageways, awnings or overhangs

Open Space: mid-liner-building passages to rear structure, possible service space between liner building and parking structure

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|-----------|
| A | Min Lot Size | 15,000 sf |
| B | Min Width | 120 feet |
| C | Max Width | - |
| D | Min Depth | 120 feet |
| E | Max Depth | - |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | NP |
| H | Clapboard | NP |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials



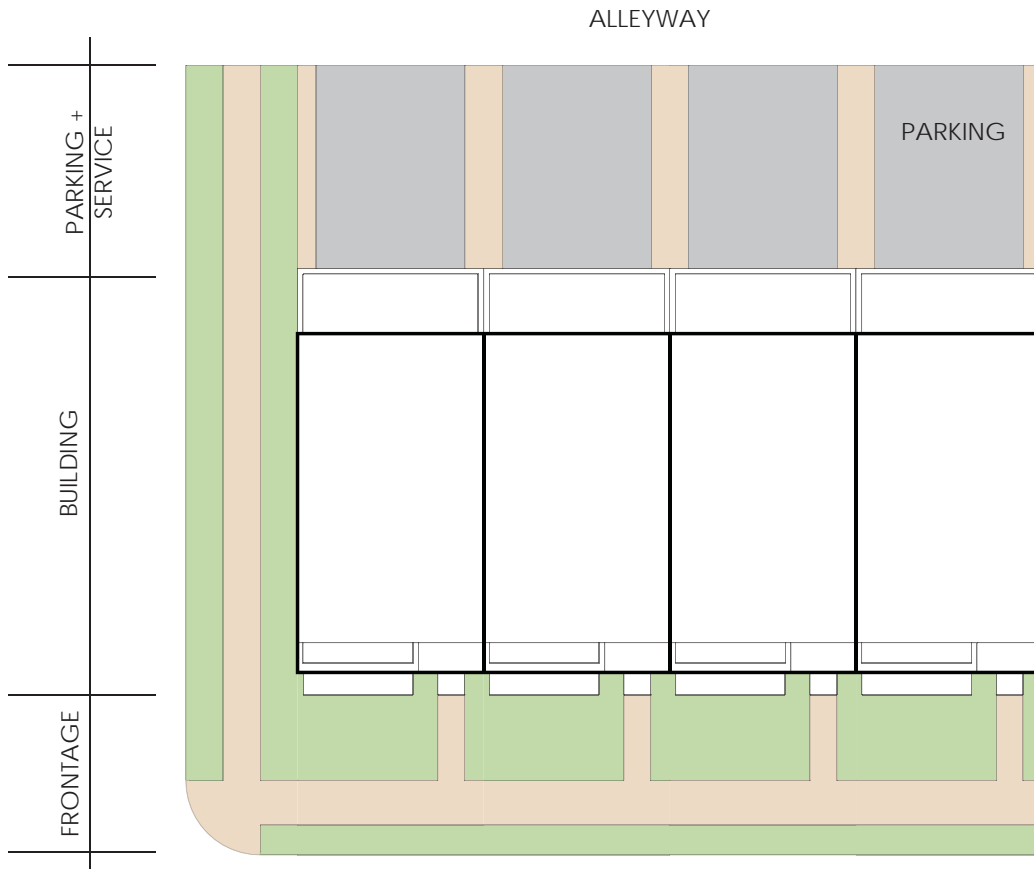
Building Type

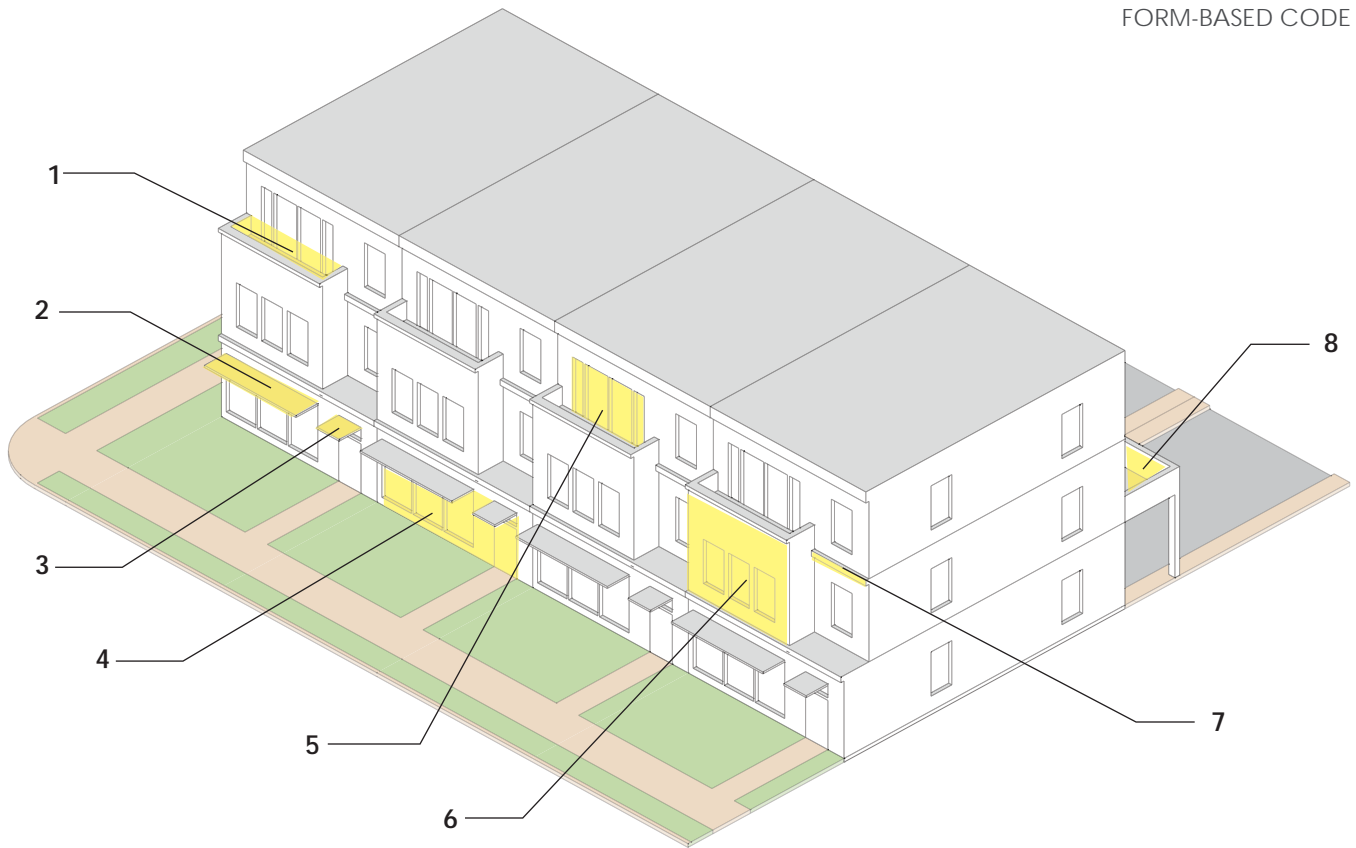
6.13.7 Live-Work Unit

A. Purpose/Definition

A hybrid commercial-residence building that may be new construction or a conversion of an existing home into a combination ground-floor business and upper-floor single-family dwelling unit.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: porches, awnings or overhangs (2, 3), horizontal bands (7), offset roof line, balconies or balconettes (1)

Horizontal Breaks: vertical windows (5), building offsets (6), offset roof line

Ground Floor: storefronts (4), awnings or overhangs (2, 3)

Upper Floor: vertically-oriented window openings (reflective of function), private outdoor spaces (1, 8)

Pedestrian Accommodations: at-grade street entrances, awnings or overhangs (2, 3), rear-yard parking

Open Space: front setback, balconies or balconettes (1), porches, decks (8)

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|------------------|----------|
| A | Min Lot Size | 2000 sf |
| B | Min Width | 25 feet |
| C | Max Width | 35 feet |
| D | Min Depth | 40 feet |
| E | Max Depth | - |
| F | Max Units in Row | 8 |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Glass | P |
| F | Stucco | P, Rear Façade |
| G | Siding | P, Rear Façade |
| H | Clapboard | P, Rear Façade |
| I | Concrete Block | NP |
| J | Concrete Panel | NP |
| K | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials



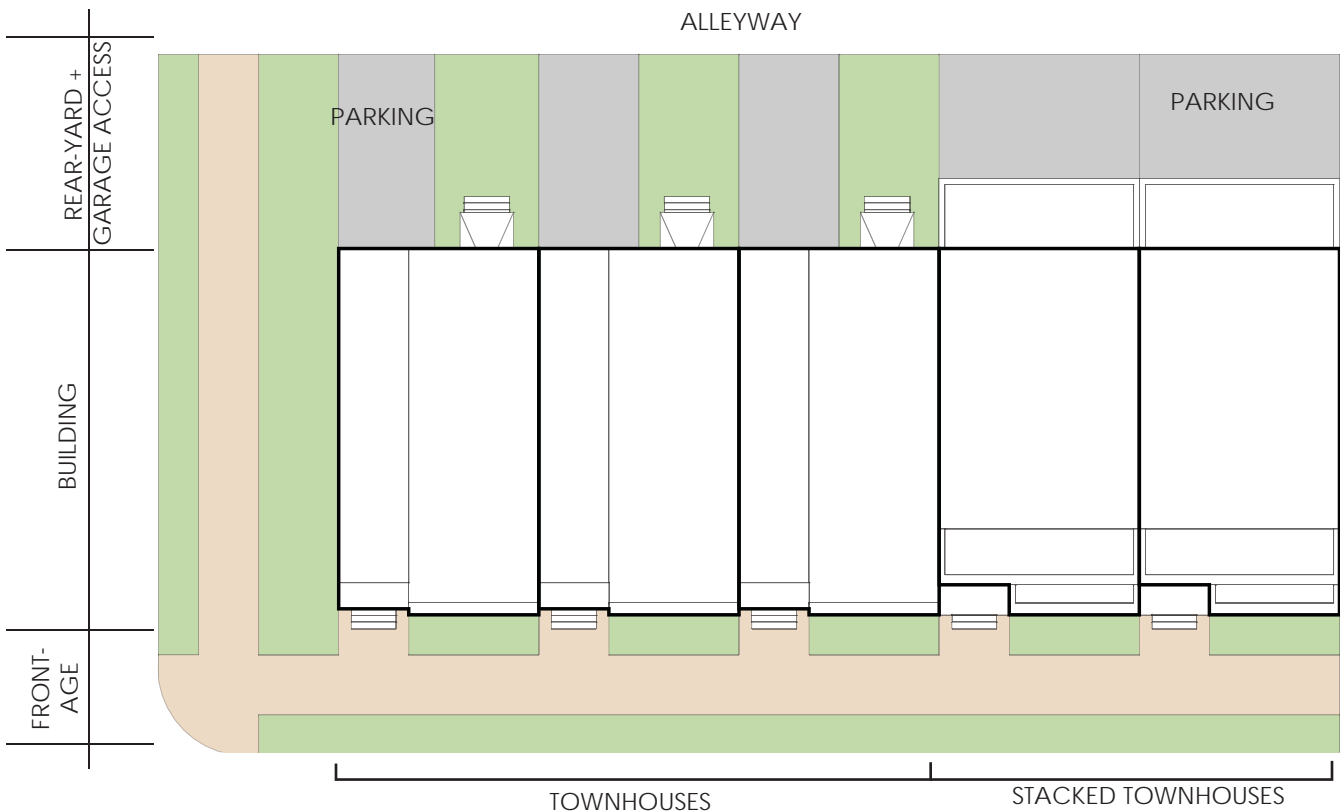
Building Type

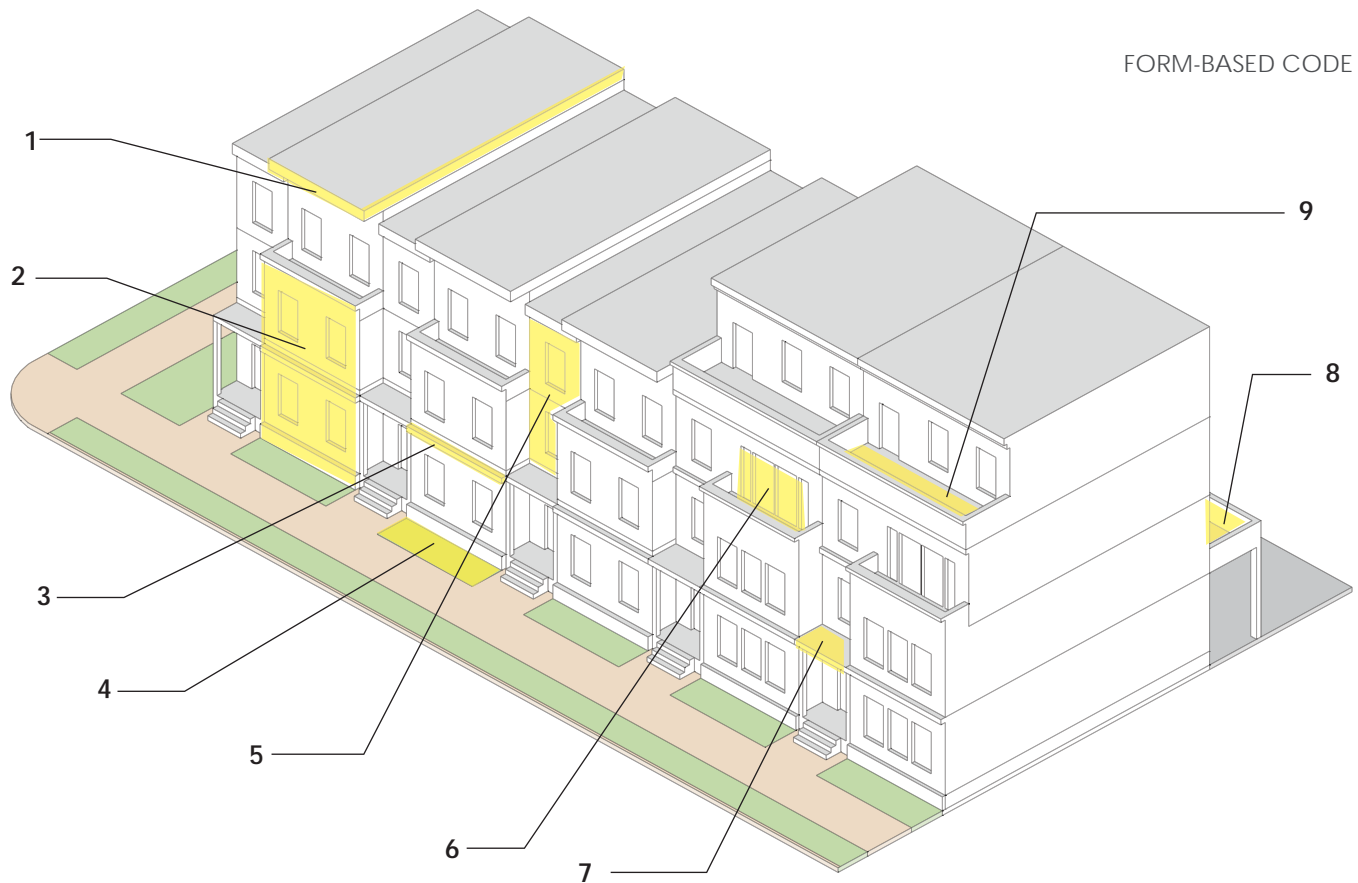
6.13.7 Townhouse

A. Purpose/Definition

A one-family dwelling unit with a private entrance, attached horizontally with other dwelling units in a linear arrangement. They are located on individual lots and are separated from other dwelling units by an approved wall, extending from the foundation through the roof that is structurally independent of the corresponding wall of the adjoining unit. They have a totally exposed front and rear wall to be used for access, light and ventilation. A variation of the Townhouse is the Stacked Townhouse, with two, similarly sized one-family dwelling units integrated into the footprint of a Townhouse.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: porches, awnings or overhangs (7), horizontal bands (3), offset roof line (1), balconies or balconettes (9)

Horizontal Breaks: vertical windows (6), building offsets (2, 5), offset roof line (1)

Ground Floor: unit entrances, awnings or overhangs (7), porches

Upper Floor: vertically-oriented window openings (reflective of function), private outdoor spaces (9, 10)

Pedestrian Accommodations: awnings or overhangs (7), rear-yard parking

Open Space: front setback (4), balconies or balconettes (9), porches, decks (8)

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|------------------|----------|
| A | Min Lot Size | 1500 sf |
| B | Min Width | 20 feet |
| C | Max Width | 35 feet |
| D | Min Depth | 35 feet |
| E | Max Depth | - |
| F | Max Units in Row | 8 |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Stucco | P, Rear Façade |
| F | Siding | P, Rear Façade |
| G | Clapboard | P, Rear Façade |
| H | Concrete Block | NP |
| I | Concrete Panel | NP |
| J | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials

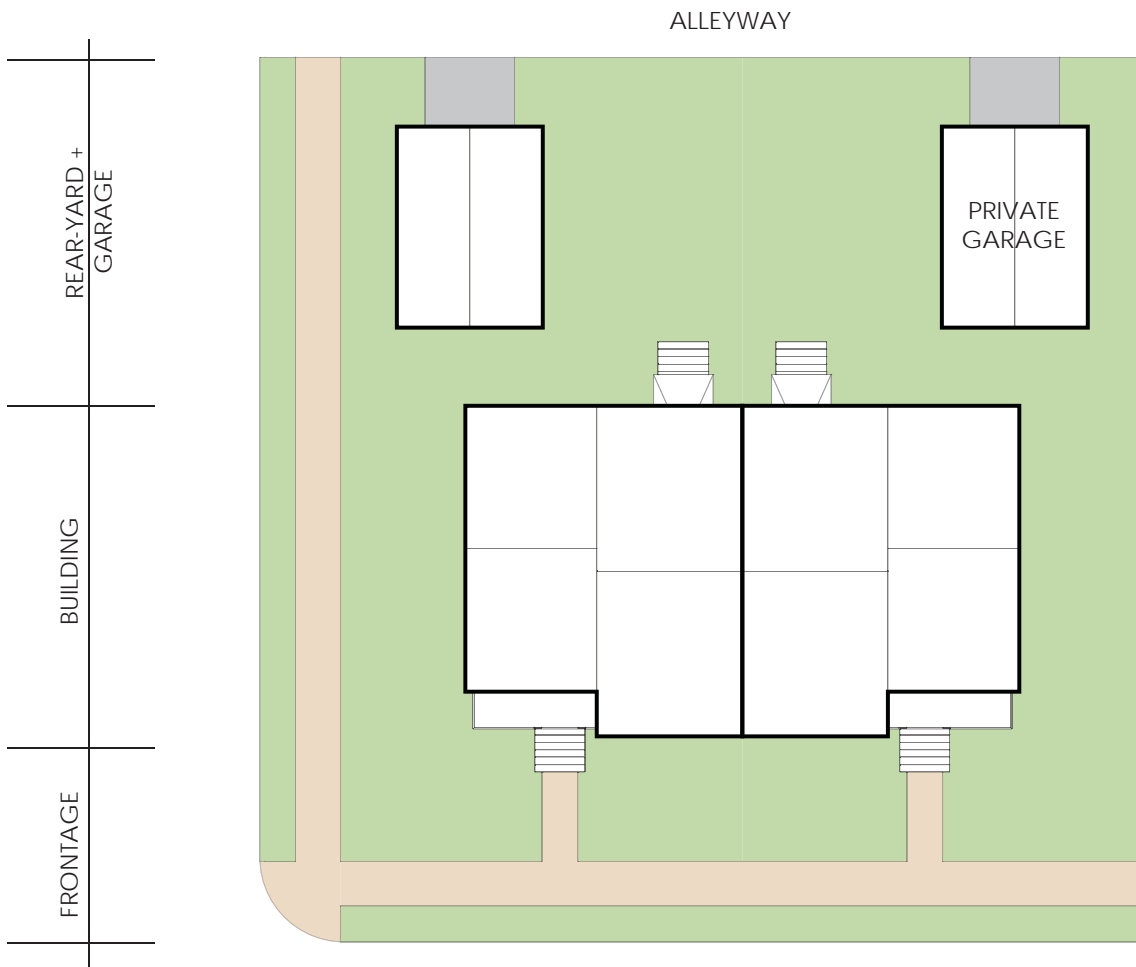
Building Type

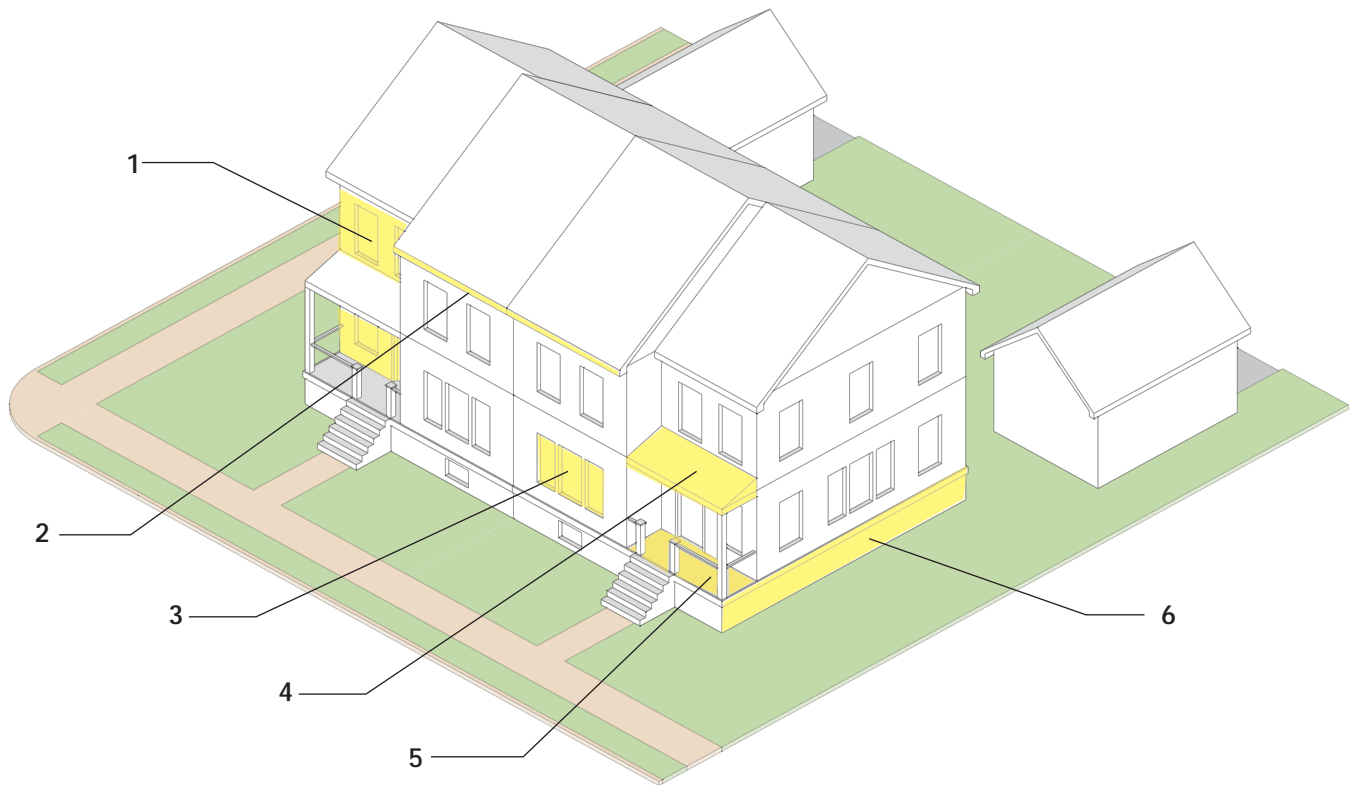
6.13.8 Twin

A. Purpose/Definition

A one-family dwelling unit with a private entrance, attached horizontally with one other dwelling unit in a linear arrangement. They are located on individual lots and are separated from the other dwelling unit by an approved wall, extending from the foundation through the roof that is structurally independent of the corresponding wall of the adjoining unit. They have a totally exposed front, rear and side wall to be used for access, light and ventilation.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: porches (5), awnings or overhangs (4), horizontal bands (6), offset roof line (2), balconies or balconettes, window grouping (3)

Horizontal Breaks: vertical windows, building offsets (1), offset roof line (2)

Ground Floor: unit entrances, awnings or overhangs (4), porches (5)

Upper Floor: vertically-oriented window openings (reflective of function), private outdoor spaces

Pedestrian Accommodations: awnings or overhangs (4), rear-yard garage accessed from alley

Open Space: front setback, balconies or balconettes, porches (5), decks, rear setback, side setback

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|----------|
| A | Min Lot Size | 4000 sf |
| B | Min Width | 25 feet |
| C | Max Width | 35 feet |
| D | Min Depth | 35 feet |
| E | Max Depth | - |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Stucco | P, Rear Façade |
| F | Siding | P |
| G | Clapboard | P |
| H | Concrete Block | NP |
| I | Concrete Panel | NP |
| J | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials

SFD

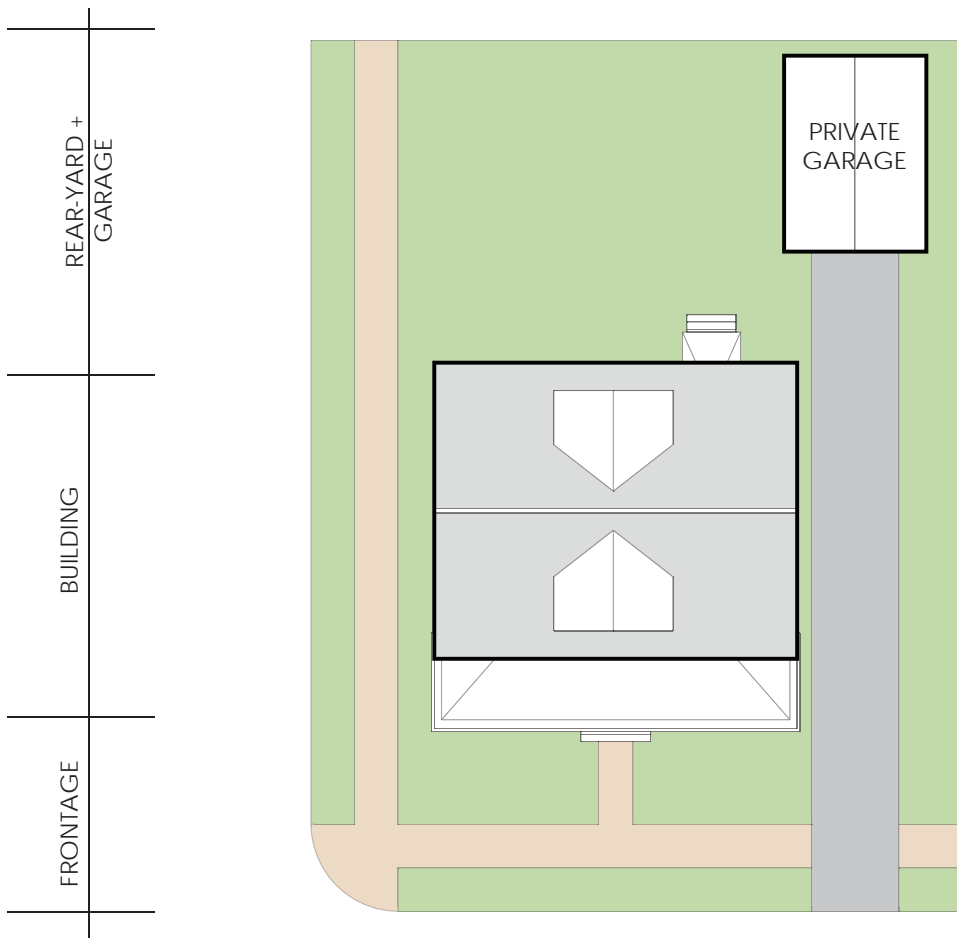
Building Type

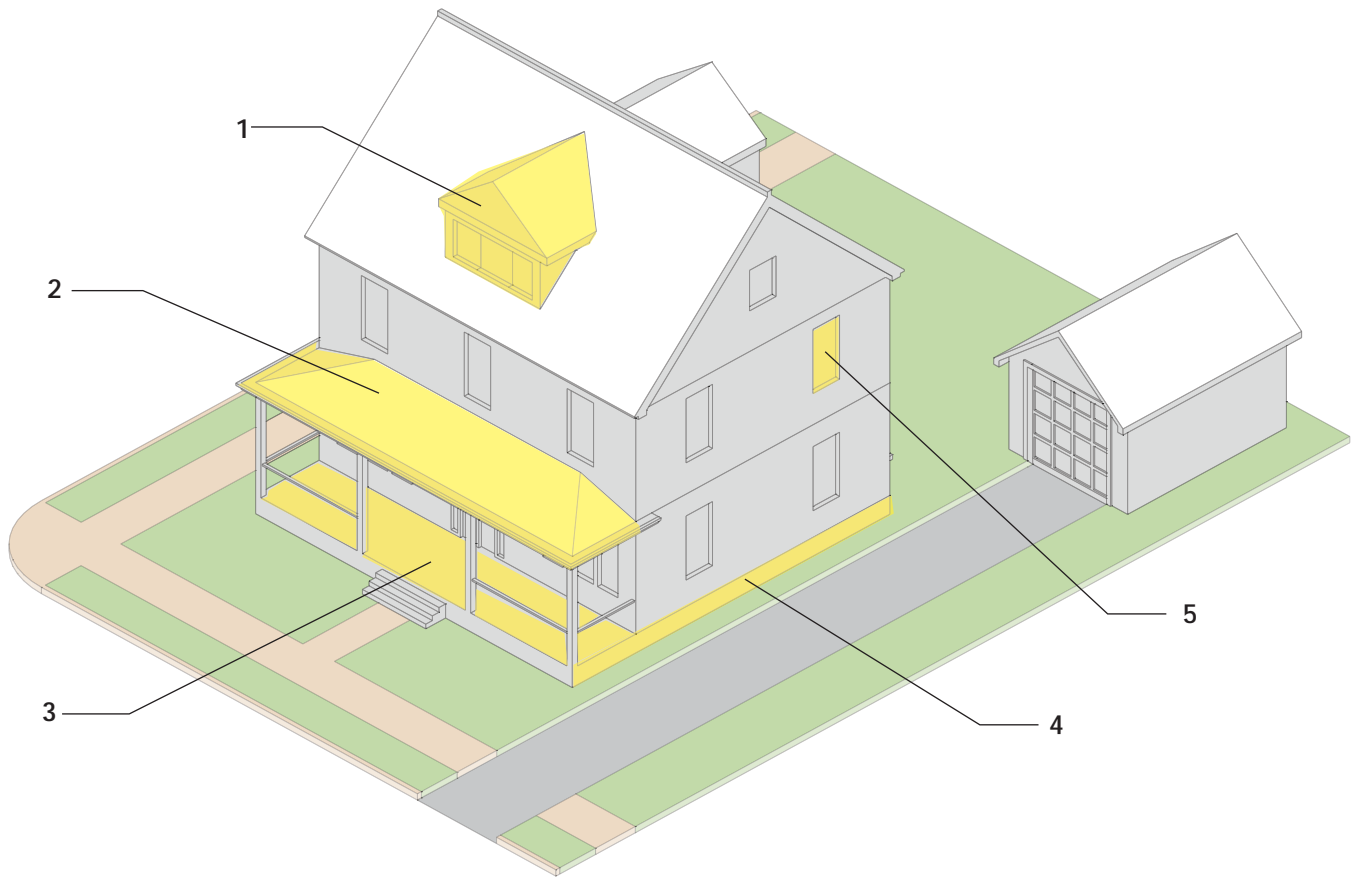
6.13.9 Single Family Detached

A. Purpose/Definition

A detached single family unit on its own individual lot designed for or used exclusively by one family.

B. Plan: Required Site Elements





C. Massing: Required Building Elements

Required Building Elements are regulated in Transect Charts and General Design Standards to ensure that buildings respect the pedestrian scale of the City. To meet the regulated standards, the following elements are suggested for incorporation into the building design:

Vertical Breaks: dormers (1), porches (3), awnings or overhangs (2), horizontal bands (4), offset roof line, balconies or balconettes, window grouping

Horizontal Breaks: vertical windows (5), building offsets, offset roof line, dormers (1)

Ground Floor: unit entrances, awnings or overhangs (2), porches (3)

Upper Floor: vertically-oriented window openings (reflective of function), private outdoor spaces

Pedestrian Accommodations: awnings or overhangs (2), rear-yard garage accessed from alley

Open Space: front setback, balconies or balconettes, porches (3), decks, rear setback, side setback

D. Permitted Building Dimensions

| | DIMENSION | STANDARD |
|---|--------------|----------|
| A | Min Lot Size | 5000 sf |
| B | Min Width | 30 feet |
| C | Max Width | 45 feet |
| D | Min Depth | 30 feet |
| E | Max Depth | - |

E. Permitted Façade Materials

| | MATERIAL | STANDARD |
|---|---------------------|----------------|
| A | Brick | P |
| B | Stone | P |
| C | Architectural Panel | P |
| D | Extruded Metal | P |
| E | Stucco | P, Rear Façade |
| F | Siding | P |
| G | Clapboard | P |
| H | Concrete Block | NP |
| I | Concrete Panel | NP |
| J | Prefab. Steel/Metal | NP |

P – Permitted NP – Not Permitted

See Section 6.20 Definitions for further clarification of materials

6.14 Public Frontage Types

DTB

Frontage Type

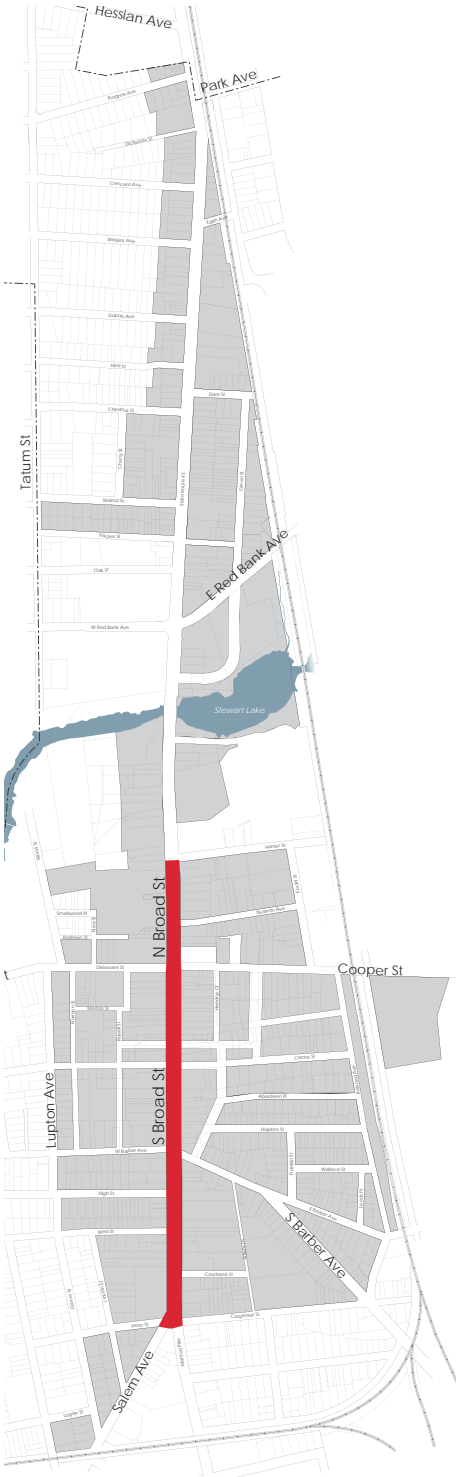
6.14.1 Downtown Broad Street

A. Purpose/Definition

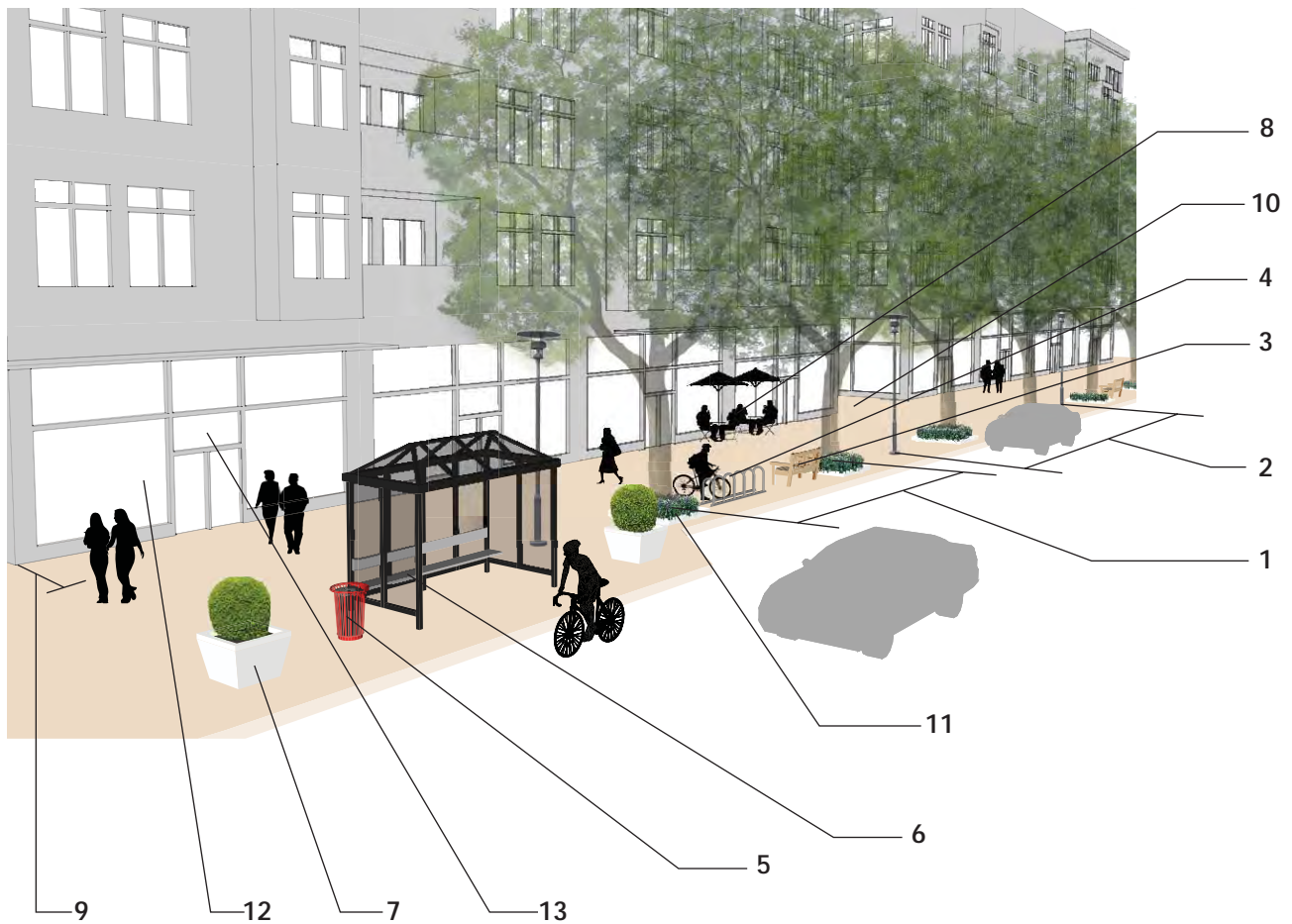
Intended to serve as the primary spine for Woodbury's downtown commercial and governmental area. As such, the streetscape must provide capacity for a high amount of pedestrian activity. This necessity, along with the grand scale of the street, warrants wide sidewalks with high quality materials and design, generous building heights, high quality façade materials and design, maximum first floor transparency, minimal to zero front and side yard setbacks, zero curb cuts and no front yard parking lots. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|------------------------|
| 1 | Street Tree Spacing | Avg. 40' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 100 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Stop |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Permitted |
| 9 | Private Setback | Paved |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree Pits |
| 12 | Storefronts | Required |
| 13 | Signs | Permitted |



C. Typical Pedestrian Realm and Associated Elements



D. Permitted Street Trees

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak. In general, tree types may be alternated except between Carpenter and Barber Ave where Green Ash trees have been selected and between Barber and Delaware/Cooper where Willow Oak trees have been selected, both for specific intent (per Street Tree Plan in Section 4.4.1).



Frontage Type

6.14.2 Uptown Broad Street

A. Purpose/Definition

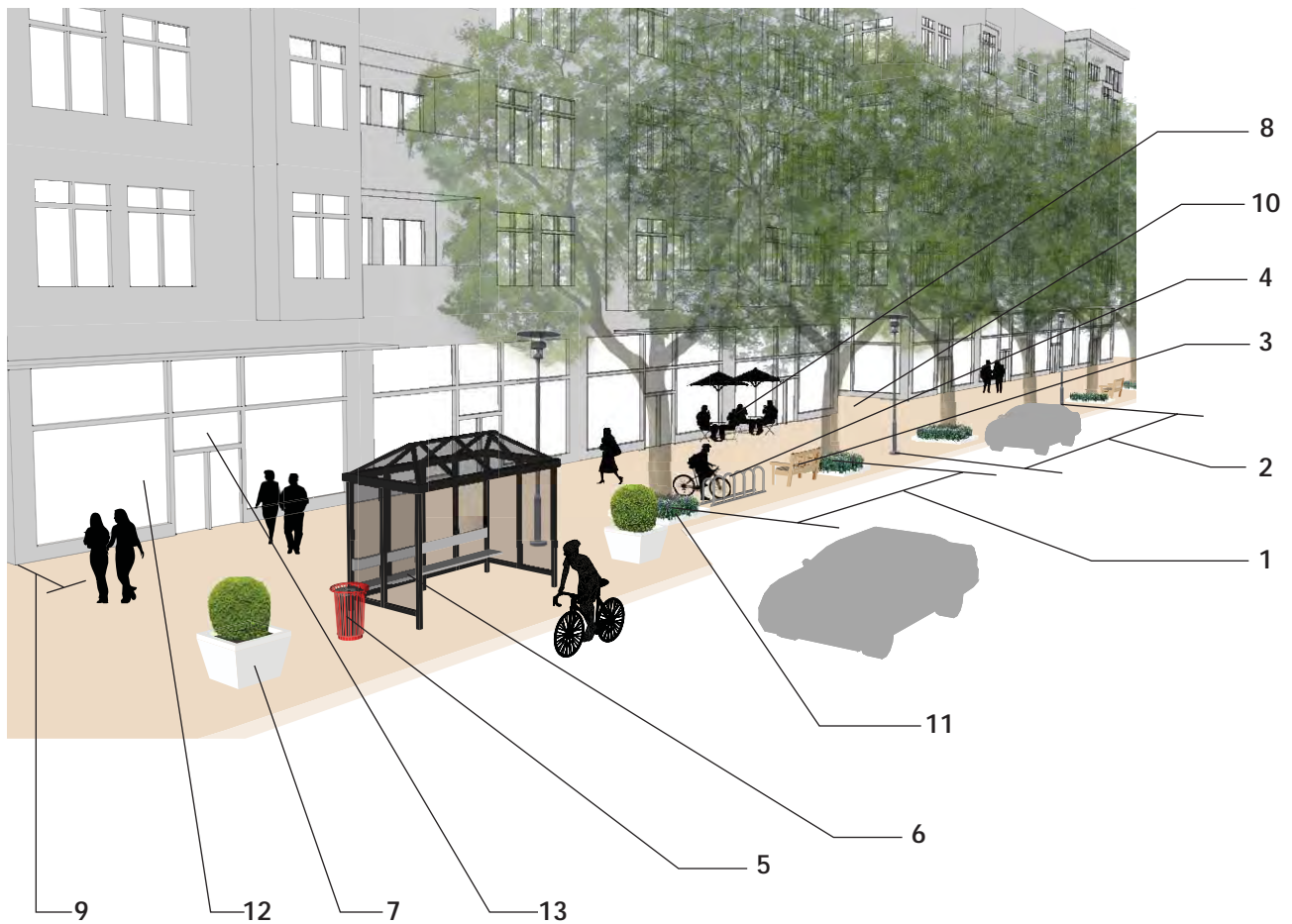
Intended to serve as the primary spine of Woodbury's uptown commercial and medical area. As such, the streetscape must provide capacity for a high amount of pedestrian activity. This necessity, along with the grand scale of the street, warrants wide sidewalks high quality materials and design, generous building heights, high quality façade materials and design, maximum first floor transparency, minimal to zero front and side yard setbacks, curb cuts and front yard parking lots. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|------------------------|
| 1 | Street Tree Spacing | Avg. 40' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | 1 per 100 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Stop |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Permitted |
| 9 | Private Setback | Paved or Landscape |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree Pits |
| 12 | Storefronts | Permitted |
| 13 | Signs | Permitted |



C. *Typical Pedestrian Realm and Associated Elements*



D. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak. In general, tree types may be alternated except between the City boundary and Edith Street where Green Ash trees have been selected and between Red Bank Avenue and the bridge where Willow Oak trees have been selected, both for specific intent (per Street Tree Plan in Section 4.4.1).



Frontage Type

6.14.3 Collector

A. Purpose/Definition

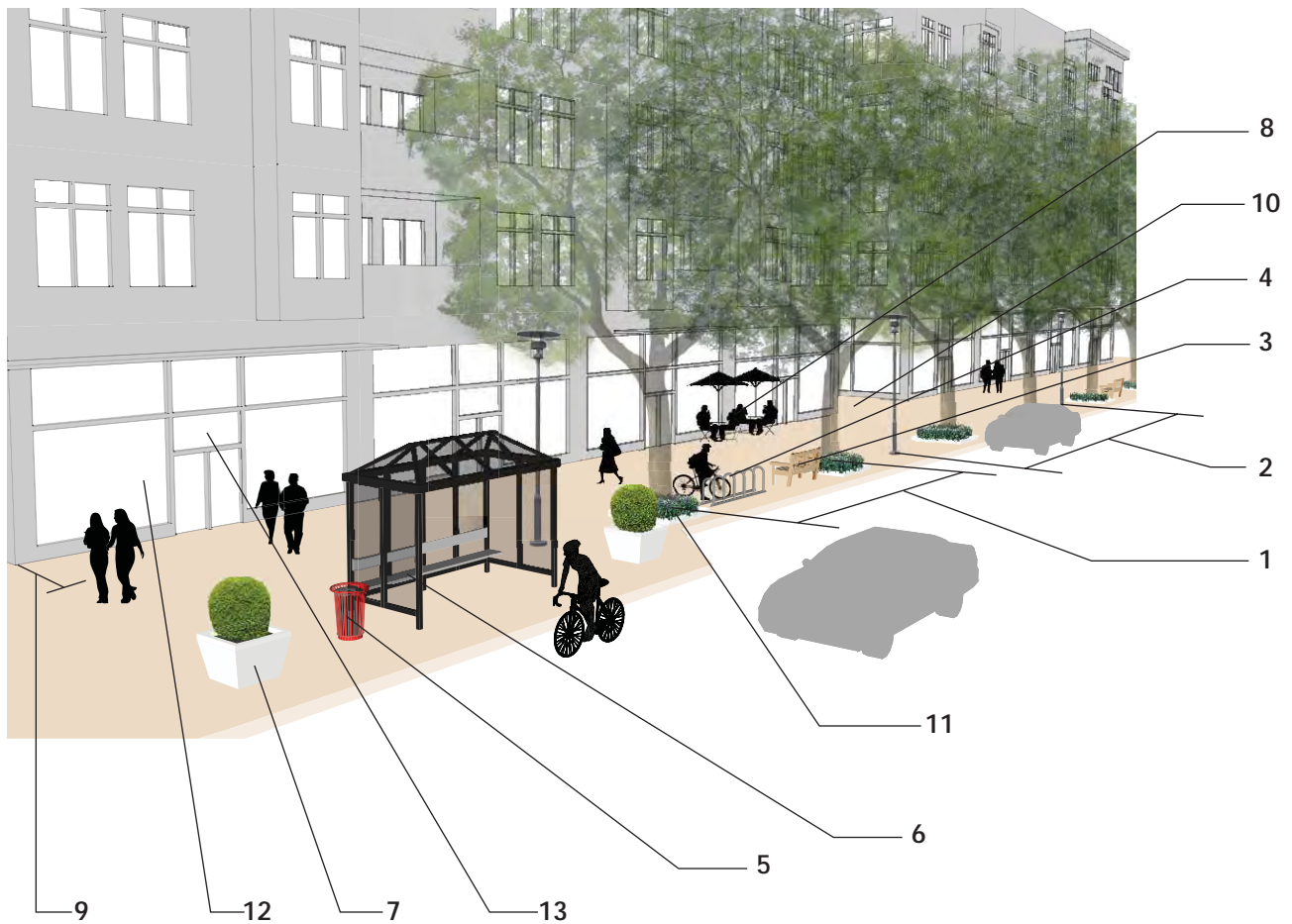
Intended to serve as the primary feeders to Broad Street. While existing right-of-way constraints limit the size of the pedestrian realm, the streetscape must provide capacity for a moderate to high amount of pedestrian activity where possible. As such, new development may provide the opportunity for additional pedestrian space. New development is to provide moderate to generous building heights, high quality façade materials and design, moderate to maximum first floor transparency, minimal front and side yard setbacks, and minimal curb cuts and no front yard parking. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|------------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 85' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Required at every Stop |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Permitted |
| 9 | Private Setback | Paved or Landscape |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree Pits |
| 12 | Storefronts | Permitted |
| 13 | Signs | Permitted |



C. Typical Pedestrian Realm and Associated Elements



D. Permitted Street Trees

Permitted trees for this frontage were selected for their ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash, Tulip Poplar, Shademaster Honeylocust, Skyline Honeylocust, Sawtooth Oak, Willow Oak, and Little Leaf Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.



Frontage Type

6.14.4 Special

A. Purpose/Definition

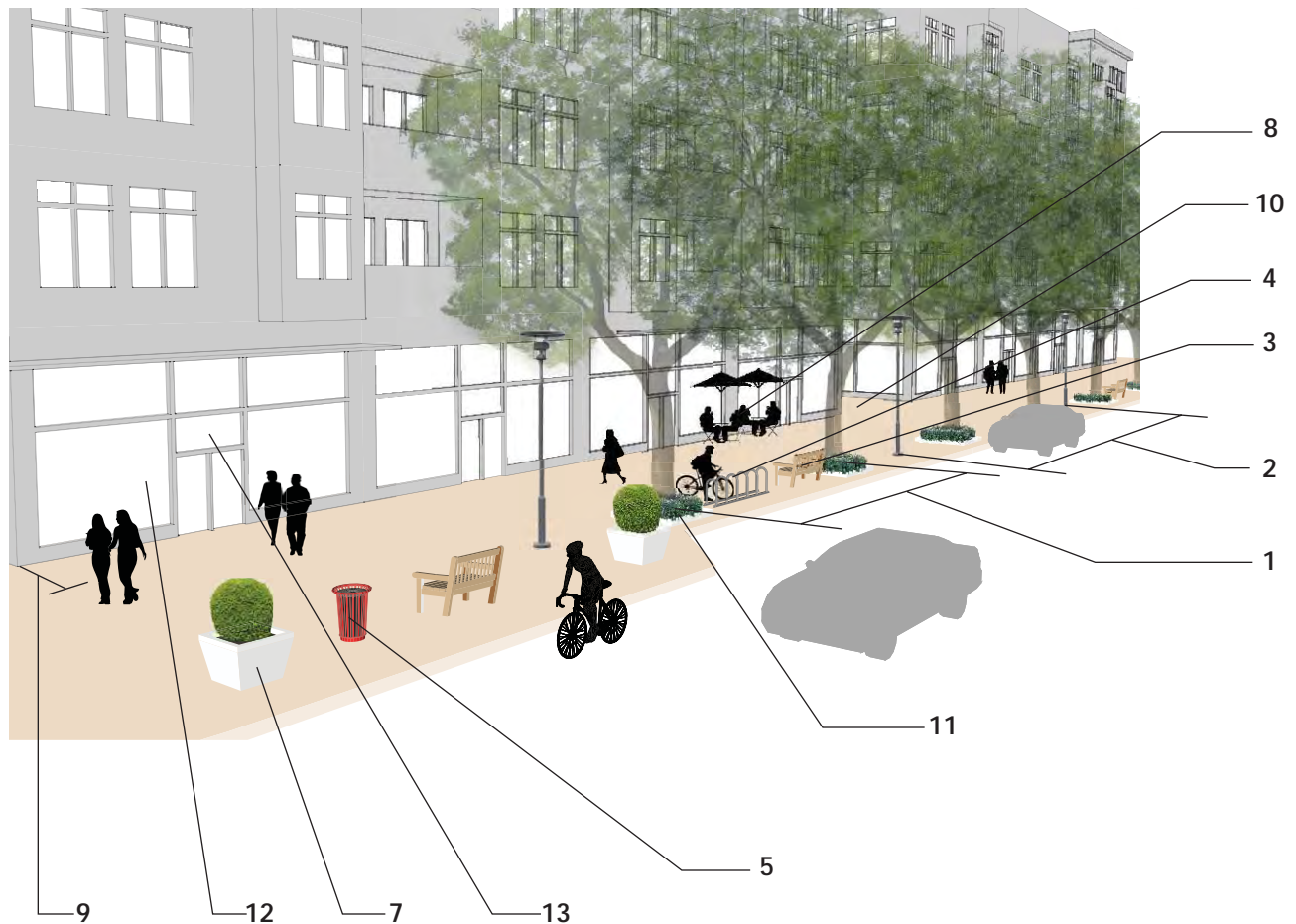
Intended to serve the pedestrian-oriented streets to be added to the street grid and existing streets that will be substantially modified as a result of redevelopment. These master planned streets are to have streetscapes that provide capacity for a moderate to high amount of pedestrian activity. New streets provide the opportunity for wide sidewalks that shall have high quality materials and design. They should be accompanied by development with generous building heights, high quality façade materials and design, moderate first floor transparency, minimal front and side yard setbacks, and minimal curb cuts and no front or side yard parking. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 85' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Permitted |
| 9 | Private Setback | Paved |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree Pits |
| 12 | Storefronts | Permitted |
| 13 | Signs | Permitted |



C. *Typical Pedestrian Realm and Associated Elements*



D. *Permitted Street Trees*

Permitted trees for this frontage were selected for their large size and ability to provide shade to wide sidewalks while still allowing storefronts, signs, and pedestrian-level lighting to be visible under the canopy. They are as follows: Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except along Centre Street and the new parallel connector road where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent. The London Plane tree is designated for the split sections of Centre Street, while the male Ginkgo tree is planned for the sections between Lupton and Russell and the new street and Railroad Avenue. The Fastigate English Oak tree is proposed for the new street, part of the parallel system.



Frontage Type

6.14.5 Service

A. Purpose/Definition

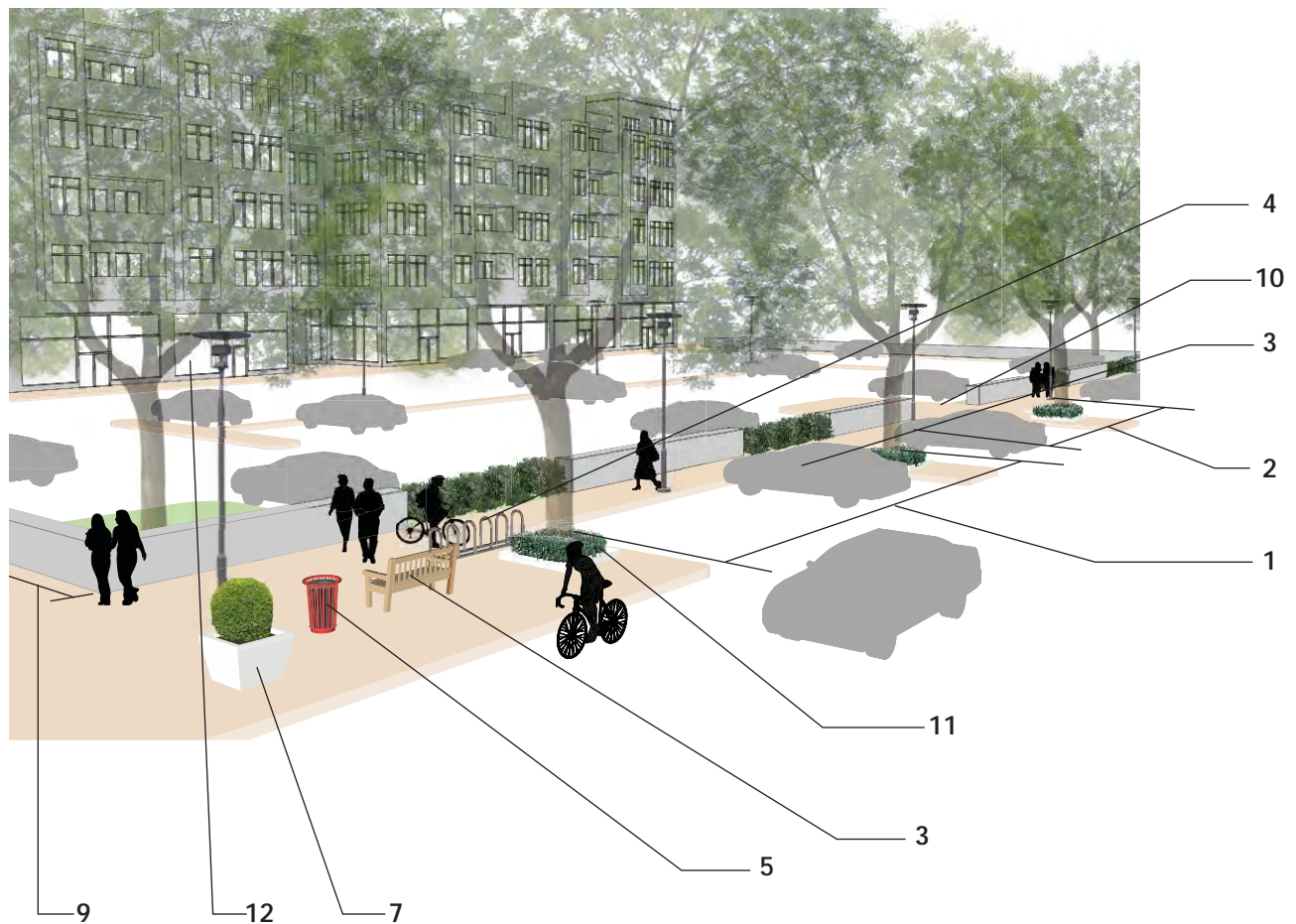
Intended to serve the parking-oriented streets to be added to the street grid. These master planned streets are designed to maximize on-street parking through 90-degree head-in parking stalls. While the pedestrian realm is scaled down to accommodate parking, it is to be enhanced by moderate to generous building heights, moderate quality façade materials and design, moderate first floor transparency, minimal front and side yard setbacks, minimal curb cuts and screened front yard parking. No tax lots or buildings are intended to front directly on to the service street as the primary street; if this should occur, frontage type “Local” should be used to determine the building standards in the appropriate Transect chart. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements



| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 60' On Center |
| 2 | Street Lighting Spacing | Avg. 60' On Center |
| 3 | Bench | 1 per 150 Linear Feet |
| 4 | Bicycle Rack | 1 per 200 Linear Feet |
| 5 | Trash Receptacle | 1 per 150 Linear Feet |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Not Permitted |
| 9 | Private Setback | Paved or Landscape |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree Pits |
| 12 | Storefronts | Not Permitted |
| 13 | Signs | Permitted |

C. *Typical Pedestrian Realm and Associated Elements*



D. *Permitted Street Trees*

Permitted trees for this frontage were selected for their large size and ability to provide shade to sidewalks while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: Sugar Maple, Green Ash 'Newport', Green Ash 'Summit', Shademaster Honeylocust, Skyline Honeylocust, London Plane, Heritage Oak. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

LOC

Frontage Type

6.14.6 Local

A. Purpose/Definition

Intended to serve existing residential streets that have minimal to moderate through traffic and will be largely unchanged through redevelopment. As such, the streetscape must provide capacity for minimal to moderate pedestrian activity. New development is to provide varied building heights, high quality façade materials and design, low to moderate first floor transparency, varied front and side yard setbacks and minimal curb cuts and front yard parking. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|--------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 85' On Center |
| 3 | Bench | Permitted |
| 4 | Bicycle Rack | Permitted |
| 5 | Trash Receptacle | Permitted |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Not Permitted |
| 8 | Outdoor Cafes | Not Permitted |
| 9 | Private Setback | Landscape |
| 10 | Mid-block Ped Connection | Not Applicable |
| 11 | Planting Strip | Grass Verge |
| 12 | Storefronts | Not Permitted |
| 13 | Signs | Not Permitted |



C. *Typical Pedestrian Realm and Associated Elements*



D. *Permitted Street Trees*

Permitted trees for this frontage were selected for their ability to provide shade to sidewalks and grow in a confined space while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: Tulip Poplar, Little Leaf Linden 'Chancellor', and Silver Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.



Frontage Type

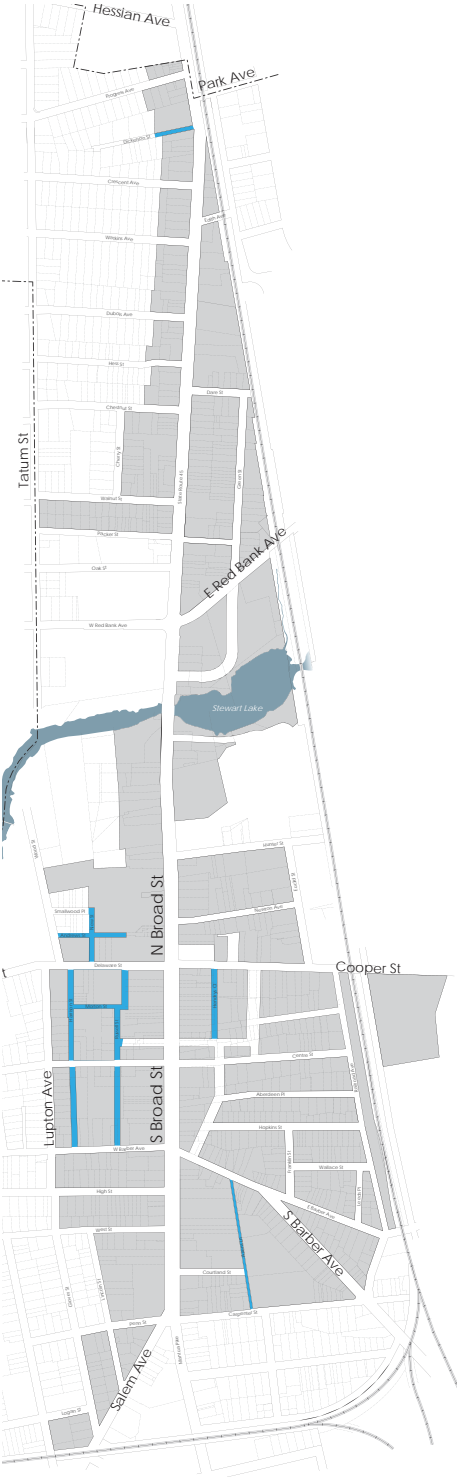
6.14.7 Small

A. Purpose/Definition

Intended to serve existing alleys and residential streets with extremely narrow rights of way and new alleys that will be provided to service the rear of new buildings. As such, the streetscape must provide capacity for minimal to no pedestrian activity. New development is to provide varied building heights, lower to moderate quality façade materials and design, low first floor transparency, moderate front and side yard setbacks and opportunities for curb cuts and parking. Street right-of-way and cross sections shall adhere to the Street Regulating Plan (Section 4.3).

B. Pedestrian Realm Amenity Requirements

| | AMENITY TYPE | REQUIREMENT |
|----|--------------------------|-----------------------|
| 1 | Street Tree Spacing | Avg. 30' On Center |
| 2 | Street Lighting Spacing | Avg. 70' On Center |
| 3 | Bench | Permitted |
| 4 | Bicycle Rack | Permitted |
| 5 | Trash Receptacle | Permitted |
| 6 | Bus Shelter | Not Applicable |
| 7 | Planter | Permitted |
| 8 | Outdoor Cafes | Not Permitted |
| 9 | Private Setback | Paved or Landscape |
| 10 | Mid-block Ped Connection | 1 per 200 Linear Feet |
| 11 | Planting Strip | Tree pits |
| 12 | Storefronts | Not Permitted |
| 13 | Signs | Permitted |



C. *Typical Pedestrian Realm and Associated Elements*

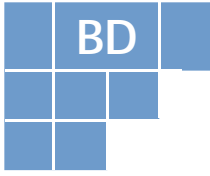


D. *Permitted Street Trees*

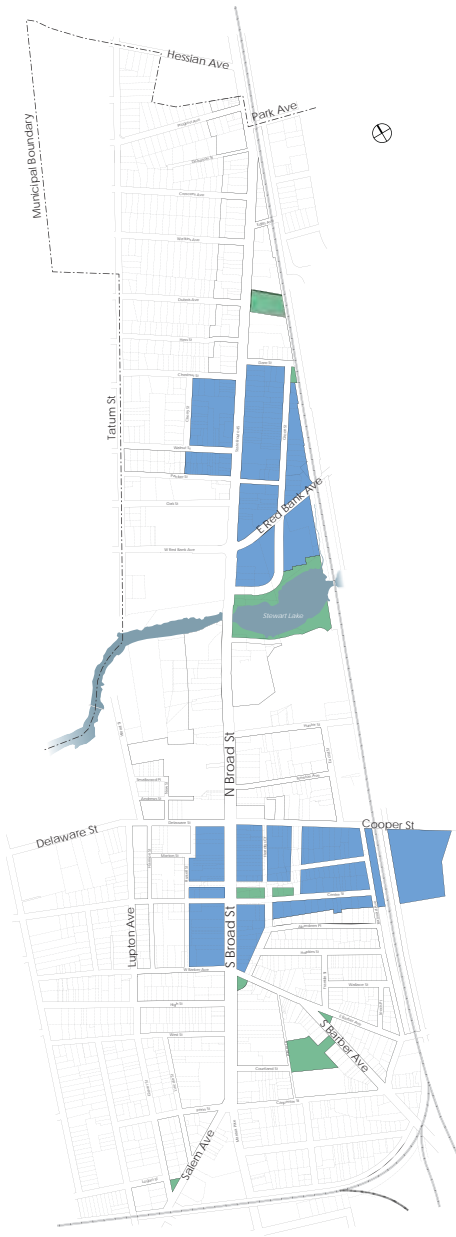
Permitted trees for this frontage were selected for their ability to shade sidewalks and grow in a confined space, while still allowing porches, entryways, and pedestrian-level lighting to be visible under the canopy. They are as follows: Ginkgo, Pin Oak, Sargent Cherry, and Silver Linden. In general, tree types may be alternated except where, per the Street Tree Plan (Section 4.4.1), trees have been selected for specific intent.

6.15 Development Standards

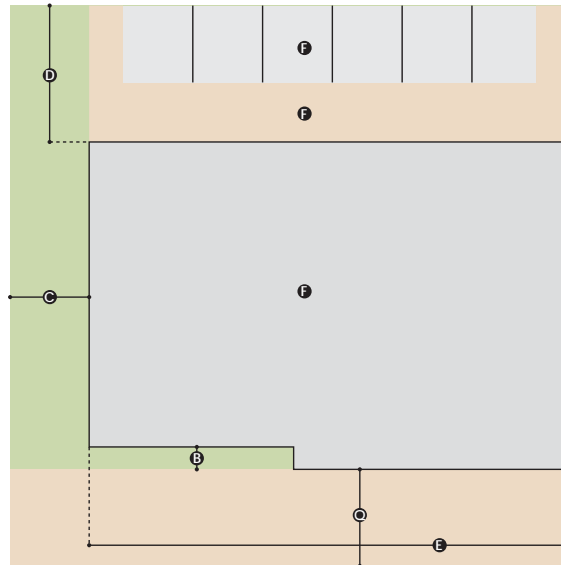
6.15.1 Downtown Transect



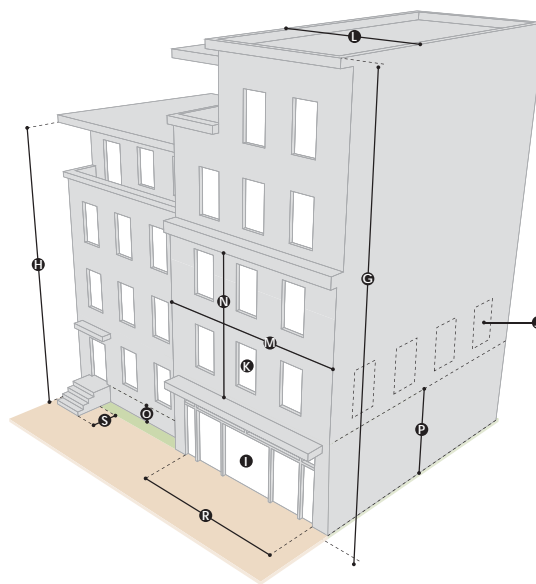
A. Location



B. Plan View



C. Massing View

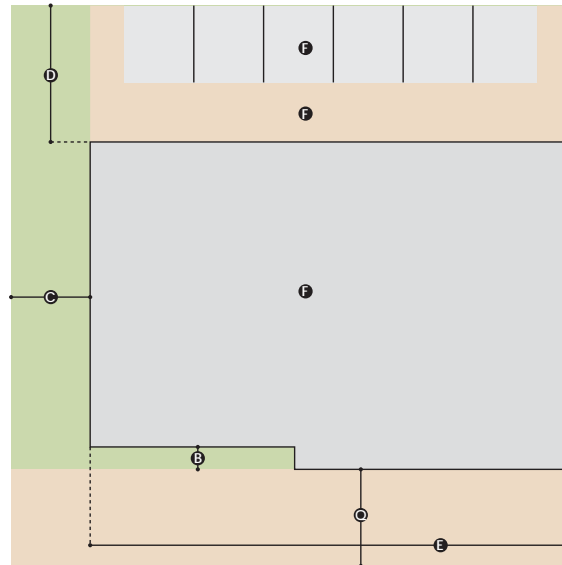


D. Development Standards Table

| | | | PUBLIC FRONTAGE | | | | | | | |
|---|---|--|-----------------|--------------------|-------|-------|-------|--------------------|-------|-------|
| | | | DTB | UTB | COL | SPC | SRV | LOC | SML | |
| DESIGN STANDARDS | A | Lot Size | Min | SEE BUILDING TYPES | | | | | | |
| | | Max | | | | | | | | |
| | B | Front Yard Setback | Min | 0 ft | 0 ft | 0 ft | 0 ft | -- | 10 ft | 10 ft |
| | | | Max | 10 ft | 10 ft | 10 ft | 10 ft | -- | 15 ft | 15 ft |
| | C | Side Yard Setback | Min | -- | -- | -- | -- | -- | -- | -- |
| | | | Max | 0 ft | 0 ft | 10 ft | 0 ft | -- | 10 ft | 10 ft |
| | D | Rear Yard Setback* | Min | 50 ft | 50 ft | 50 ft | 50 ft | -- | 50 ft | 50 ft |
| | | | Max | -- | -- | -- | -- | -- | -- | -- |
| | E | Frontage Percentage | Min | 90% | 90% | 90% | 90% | -- | 50% | 50% |
| | | | Max | -- | -- | -- | -- | -- | -- | -- |
| | F | Lot Coverage | Min | -- | -- | -- | -- | -- | -- | -- |
| | | | Max | 100% | 100% | 90% | 100% | -- | 80% | 80% |
| | G | Building Height | Min | 50 ft | 50 ft | 50 ft | 50 ft | -- | 50 ft | 24 ft |
| | | | Max | 70 ft | 70 ft | 70 ft | 70 ft | -- | 70 ft | 50 ft |
| | H | Eave Height | Min | 40 ft | 40 ft | 40 ft | 40 ft | -- | 40 ft | 24 ft |
| | | | Max | 60 ft | 60 ft | 60 ft | 60 ft | -- | 60 ft | 50 ft |
| | I | Ground Floor Front Façade Fenestration | Min | 70% | 70% | 70% | 70% | -- | 30% | 30% |
| | | | Max | -- | -- | -- | -- | -- | -- | -- |
| | J | Ground Floor Side/Rear Façade Fenestration | Min | 50% | 50% | 50% | 50% | -- | 30% | 30% |
| | | | Max | -- | -- | -- | -- | -- | -- | -- |
| | K | Upper Floor Façade Fenestration | Min | 30% | 30% | 30% | 30% | -- | 20% | 20% |
| | | | Max | -- | -- | -- | -- | -- | -- | -- |
| L | Distance between Roofline Offsets | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 40 ft | 40 ft | 40 ft | 40 ft | -- | 40 ft | 40 ft | |
| M | Distance between Horizontal Façade Breaks | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 40 ft | 40 ft | 40 ft | 40 ft | -- | 40 ft | 40 ft | |
| N | Distance between Vertical Façade Breaks | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 24 ft | 24 ft | 24 ft | 24 ft | -- | 24 ft | 24 ft | |
| O | First Floor Elevation | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 0 ft | 0 ft | 3 ft | 3 ft | -- | 3 ft | 3 ft | |
| P | First Story Clear Height | Min | 16 ft | 16 ft | 16 ft | 16 ft | -- | 12 ft | 12 ft | |
| | | Max | -- | -- | -- | -- | -- | -- | -- | |
| Q | Pedestrian Realm Width | Min | 18 ft | 15 ft | 15 ft | 20 ft | -- | 9 ft | 5 ft | |
| | | Max | -- | -- | -- | -- | -- | -- | -- | |
| R | Storefront Width | Min | 20 ft | 20 ft | 20 ft | 20 ft | -- | 20 ft | -- | |
| | | Max | 40 ft | 40 ft | 40 ft | 40 ft | -- | 40 ft | -- | |
| S | Front Façade Encroachments | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 3 ft | 3 ft | 3 ft | 3 ft | -- | 3 ft | 3 ft | |
| T | Accessory Building Setback* Front | Min | 30 ft | 30 ft | 30 ft | 30 ft | -- | 30 ft | 30 ft | |
| | | Max | -- | -- | -- | -- | -- | -- | -- | |
| U | Accessory Building Setback* Side/Rear | Min | 20 ft | 20 ft | 5 ft | 5 ft | -- | 5 ft | 5 ft | |
| | | Max | -- | -- | -- | -- | -- | -- | -- | |
| V | Accessory Building Height | Min | -- | -- | -- | -- | -- | -- | -- | |
| | | Max | 20 ft | 20 ft | 20 ft | 20 ft | -- | 20 ft | 20 ft | |
| W | Front Yard Parking | | NP | NP | NP | NP | -- | NP | NP | |
| X | Side Yard Parking | | NP | NP | NP | NP | -- | NP | NP | |
| Y | Rear Yard Parking | | P | P | P | P | -- | P | P | |
| AA | GCI | | P | P | P | P | -- | P | NP | |
| BB | FLX | | P | P | P | P | -- | P | NP | |
| CC | FLT | | P | P | P | P | -- | P | P | |
| DD | COM | | P | P | P | P | -- | P | P | |
| EE | LNR | | P | P | P | P | -- | P | P | |
| FF | PK** | | P | P | P | P | -- | P | P | |
| GG | LW | | NP | NP | P | P | -- | P | P | |
| HH | TWH | | NP | NP | P | P | -- | P | P | |
| II | TWI | | NP | NP | NP | NP | -- | NP | NP | |
| JJ | SFD | | NP | NP | NP | NP | -- | NP | NP | |
| * Min setback of rear façade from rear parking lot shall be 10 feet | | | | | | | | P - Permitted | | |
| ** Parking structures must be located in rear yards or internal to blocks | | | | | | | | NP - Not Permitted | | |



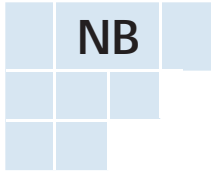
B. Plan View



A 3D perspective diagram of a building facade with various dimension lines labeled with letters. The building has a central section with a flat roof and two side sections. The central section has a flat roof of width 'L'. The total height of the building is 'H'. The height of the central section is 'G'. The height of the side section is 'P'. The width of the central section is 'N'. The width of the side section is 'M'. The width of the building at the base is 'R'. The width of the central section at the base is 'I'. The width of the side section at the base is 'O'. The width of the building at the top is 'S'. The width of the central section at the top is 'J'. The width of the side section at the top is 'K'. The width of the building at the bottom is 'Q'. The width of the central section at the bottom is 'L'. The width of the side section at the bottom is 'P'. The width of the building at the top is 'S'. The width of the central section at the top is 'J'. The width of the side section at the top is 'K'. The width of the building at the bottom is 'Q'. The width of the central section at the bottom is 'L'. The width of the side section at the bottom is 'P'.

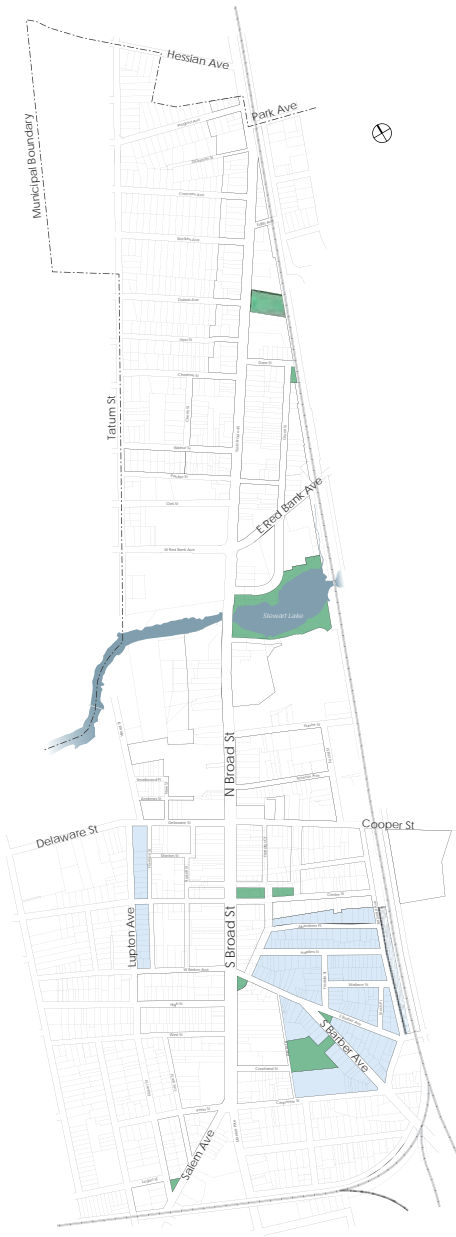
D. Development Standards Table

| | | | PUBLIC FRONTAGE | | | | | | |
|------------------|---|--|-----------------|--------------------|-------|-------|-----|--------------------|-------|
| | | | DTB | UTB | COL | SPC | SRV | LOC | SML |
| DESIGN STANDARDS | A | Lot Size | Min | SEE BUILDING TYPES | | | | | |
| | | Max | | | | | | | |
| | B | Front Yard Setback | Min | 10 ft | 10 ft | 10 ft | -- | -- | 10 ft |
| | | Max | 20 ft | 20 ft | 20 ft | -- | -- | 20 ft | -- |
| | C | Side Yard Setback (unattached side of bldg) | Min | 5 ft | 5 ft | 5 ft | -- | -- | 5 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | D | Rear Yard Setback* | Min | 20 ft | 20 ft | 20 ft | -- | -- | 20 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | E | Frontage Percentage | Min | 65% | 65% | 65% | -- | -- | 65% |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | F | Lot Coverage | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 80% | 80% | 80% | -- | -- | 70% | -- |
| | G | Building Height | Min | 30 ft | 30 ft | 30 ft | -- | -- | 25 ft |
| | | Max | 55 ft | 55 ft | 55 ft | -- | -- | 55 ft | -- |
| | H | Eave Height | Min | 25 ft | 25 ft | 25 ft | -- | -- | 25 ft |
| | | Max | 55 ft | 55 ft | 55 ft | -- | -- | 55 ft | -- |
| | I | Ground Floor Front Façade Fenestration | Min | 70% | 70% | 50% | -- | -- | 30% |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | J | Ground Floor Side/Rear Façade Fenestration | Min | 30% | 30% | 25% | -- | -- | 25% |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | K | Upper Floor Façade Fenestration | Min | 30% | 30% | 20% | -- | -- | 20% |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | L | Distance between Roofline Offsets | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 40 ft | 40 ft | 40 ft | -- | -- | 40 ft | -- |
| | M | Distance between Horizontal Façade Breaks | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 40 ft | 40 ft | 40 ft | -- | -- | 40 ft | -- |
| | N | Distance between Vertical Façade Breaks | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 24 ft | 24 ft | 24 ft | -- | -- | 24 ft | -- |
| | O | First Floor Elevation | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 0 ft | 0 ft | 3 ft | -- | -- | 3 ft | -- |
| | P | First Story Clear Height | Min | 12 ft | 12 ft | 12 ft | -- | -- | 12 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | Q | Pedestrian Realm Width | Min | 18 ft | 15 ft | 15 ft | -- | -- | 9 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | R | Storefront Width | Min | 20 ft | 20 ft | 20 ft | -- | -- | 20 ft |
| | | Max | 40 ft | 40 ft | 40 ft | -- | -- | 40 ft | -- |
| | S | Front Façade Encroachments | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 3 ft | 3 ft | 3 ft | -- | -- | 3 ft | -- |
| | T | Accessory Building Setback* Front | Min | 30 ft | 30 ft | 10 ft | -- | -- | 10 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | U | Accessory Building Setback* Side/Rear | Min | 20 ft | 20 ft | 5 ft | -- | -- | 5 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | V | Accessory Building Height | Min | -- | -- | -- | -- | -- | -- |
| | | Max | 20 ft | 20 ft | 20 ft | -- | -- | 24 ft | -- |
| BUILDING TYPES | W | Front Yard Parking | NP | NP | NP | -- | -- | NP | -- |
| | X | Side Yard Parking | P | P | P | -- | -- | P | -- |
| | Y | Rear Yard Parking | P | P | P | -- | -- | P | -- |
| | AA | GCI | P | P | P | -- | -- | NP | -- |
| | BB | FLX | P | P | P | -- | -- | NP | -- |
| | CC | FLT | P | P | P | -- | -- | P | -- |
| | DD | COM | P | P | P | -- | -- | P | -- |
| | EE | LNR | P | P | P | -- | -- | P | -- |
| | FF | PK** | P | P | P | -- | -- | P | -- |
| | GG | LW | P | P | P | -- | -- | P | -- |
| | HH | TWH | NP | NP | NP | -- | -- | P | -- |
| | II | TWI | NP | NP | NP | -- | -- | P | -- |
| | JJ | SFD | NP | NP | NP | -- | -- | P | -- |
| | * Min setback of rear façade from rear parking lot shall be 10 feet | | | | | | | P - Permitted | |
| | ** Parking structures must be located in rear yards or internal to blocks | | | | | | | NP - Not Permitted | |

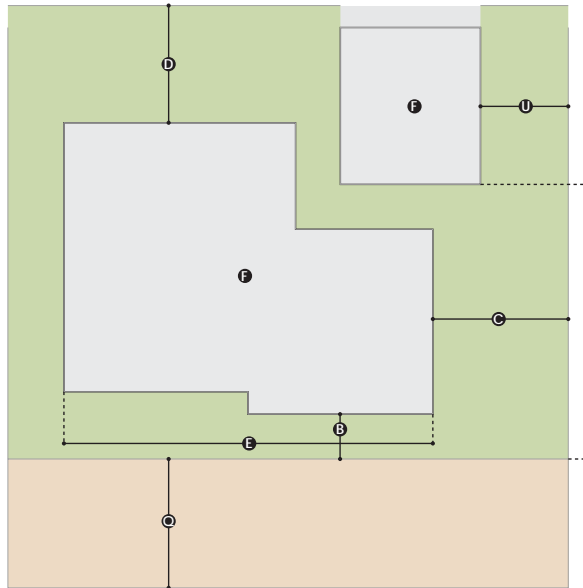


6.15.3 Neighborhood Transect

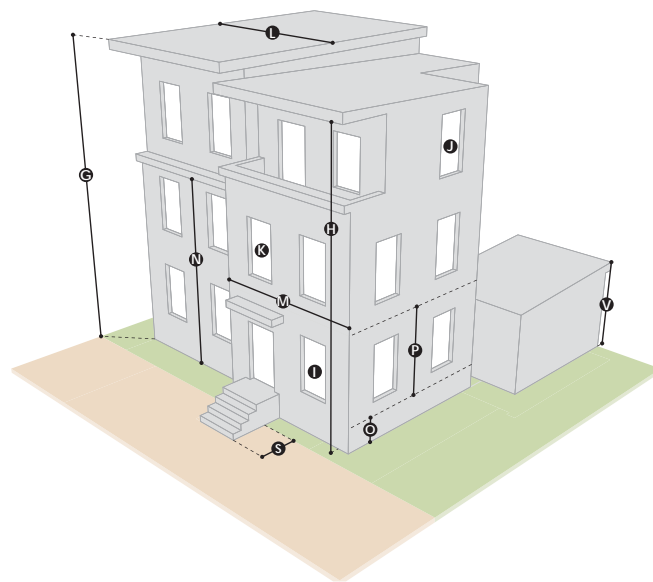
A. Location



B. Plan View



C. Massing View



D. Development Standards Table

| | | | PUBLIC FRONTAGE | | | | | | |
|---|----|-----------------------------|-----------------|--------------------|-------|-------|-----|--------------------|-------|
| | | | DTB | UTB | COL | SPC | SRV | LOC | SML |
| DESIGN STANDARDS | A | Lot Size | Min | SEE BUILDING TYPES | | | | | |
| | | Max | | | | | | | |
| | B | Front Yard Setback | Min | -- | -- | 10 ft | -- | -- | 10 ft |
| | | Max | -- | -- | 20 ft | -- | -- | 20 ft | -- |
| | C | Side Yard Setback | Min | -- | -- | 5 ft | -- | -- | 5 ft |
| | | (unattached side of bldg) | Max | -- | -- | 13 ft | -- | -- | 13 ft |
| | D | Rear Yard Setback* | Min | -- | -- | -- | -- | -- | -- |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | E | Frontage Percentage | Min | -- | -- | 50% | -- | -- | 50% |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | F | Lot Coverage | Min | -- | -- | -- | -- | -- | -- |
| | | Max | -- | -- | 70% | -- | -- | 70% | -- |
| | G | Building Height | Min | -- | -- | -- | -- | -- | -- |
| | | Max | -- | -- | 45 ft | -- | -- | 35 ft | -- |
| | H | Eave Height | Min | -- | -- | 45 ft | -- | -- | 24 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | I | Ground Floor Front Façade | Min | -- | -- | 25% | -- | -- | 25% |
| | | Fenestration | Max | -- | -- | -- | -- | -- | -- |
| | J | Ground Floor Side/Rear | Min | -- | -- | 20% | -- | -- | 20% |
| | | Façade Fenestration | Max | -- | -- | -- | -- | -- | -- |
| | K | Upper Floor Façade | Min | -- | -- | 20% | -- | -- | 20% |
| | | Fenestration | Max | -- | -- | -- | -- | -- | -- |
| | L | Distance between Roofline | Min | -- | -- | -- | -- | -- | -- |
| | | Offsets | Max | -- | -- | 30 ft | -- | -- | 30 ft |
| | M | Distance between Horizontal | Min | -- | -- | -- | -- | -- | -- |
| | | Façade Breaks | Max | -- | -- | 30 ft | -- | -- | 30 ft |
| | N | Distance between Vertical | Min | -- | -- | -- | -- | -- | -- |
| | | Façade Breaks | Max | -- | -- | 24 ft | -- | -- | 24 ft |
| | O | First Floor Elevation | Min | -- | -- | 1 ft | -- | -- | 1 ft |
| | | Max | -- | -- | 3 ft | -- | -- | 3 ft | -- |
| | P | First Story Clear Height | Min | -- | -- | 12 ft | -- | -- | 10 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| | Q | Pedestrian Realm Width | Min | -- | -- | 15 ft | -- | -- | 9 ft |
| | | Max | -- | -- | -- | -- | -- | -- | -- |
| BUILDING TYPES | R | Storefront Width | Min | -- | -- | 20 ft | -- | -- | -- |
| | | Max | -- | -- | 40 ft | -- | -- | -- | -- |
| | S | Front Façade Encroachments | Min | -- | -- | -- | -- | -- | -- |
| | | Max | -- | -- | 3 ft | -- | -- | 3 ft | -- |
| | T | Accessory Building Setback* | Min | -- | -- | 30 ft | -- | -- | 30 ft |
| | | Front | Max | -- | -- | -- | -- | -- | -- |
| | U | Accessory Building Setback* | Min | -- | -- | 5 ft | -- | -- | 5 ft |
| | | Side/Rear | Max | -- | -- | -- | -- | -- | -- |
| | V | Accessory Building Height | Min | -- | -- | -- | -- | -- | -- |
| | | Max | -- | -- | 24 ft | -- | -- | 24 ft | -- |
| | W | Front Yard Parking | -- | -- | NP | -- | -- | NP | -- |
| | X | Side Yard Parking | -- | -- | P | -- | -- | P | -- |
| | Y | Rear Yard Parking | -- | -- | P | -- | -- | P | -- |
| | AA | GCI | -- | -- | NP | -- | -- | NP | -- |
| | BB | FLX | -- | -- | NP | -- | -- | NP | -- |
| | CC | FLT | -- | -- | NP | -- | -- | NP | -- |
| | DD | COM | -- | -- | NP | -- | -- | NP | -- |
| | EE | LNR | -- | -- | NP | -- | -- | NP | -- |
| | FF | PK** | -- | -- | NP | -- | -- | NP | -- |
| | GG | LW | -- | -- | P | -- | -- | NP | -- |
| | HH | TWH | -- | -- | P | -- | -- | P | -- |
| | II | TWI | -- | -- | P | -- | -- | P | -- |
| | JJ | SFD | -- | -- | P | -- | -- | P | -- |
| * Min setback of rear façade from rear parking lot shall be 10 feet | | | | | | | | P - Permitted | |
| ** Parking structures must be located in rear yards or internal to blocks | | | | | | | | NP - Not Permitted | |

6.16 Design Standards

The following DESIGN STANDARDS are applicable across all transect zones, building types and public frontages unless otherwise noted. They are intended to ensure that redevelopment of the Downtown Business District is of the highest quality and adheres to the vision articulated in the *Redevelopment Plan*.

6.16.1 Residential Building Architecture

The following shall be required of Single Family Detached, Twin, Townhouse and Live/Work buildings:

- A. Building fronts shall be oriented to the primary street upon which the structure is sited. All building fronts shall have a primary pedestrian access (i.e. a front door) store fronts shall be open and inviting to pedestrians.
- B. There are to be no blank facades. All facades must provide windows and façade offsets and breaks as prescribed in Section 6.15.
- C. Awnings, balconies, bay windows and porches are permitted on front, side and rear facades. Porches must respect setback requirements. Awnings, upper floor balconies and bay windows may encroach into setbacks as prescribed in Section 6.15.
- D. Decks are permitted in rear yards. They may encroach into rear setbacks up to 10 feet; however, side yard setbacks must be respected.
- E. All HVAC and telecommunication systems must be screened or minimized from public view and from view of adjacent properties. Screens must be designed to blend with the architecture of the principle building.
- F. Windows shall be un-tinted. Mirror or reflective glass is not permitted in any commercial or residential location.
- G. Dwelling units shall have a minimum of 200 square feet of private outdoor space which may include a lawn, deck, patio or terrace. Such outdoor space shall be incorporated into the architecture or otherwise enclosed for privacy, as appropriate by a decorative

fence or wall, evergreen hedge, trellis or arbor or combination thereof.

- H. Free standing private garages, decks, fences, garden walls and garden sheds are permitted accessory uses in rear yards only. Structures must follow setback and coverage rules established in Section 6.15.
- I. Home offices and in-law suites are permitted above private garages associated with single family detached houses and twins. There is a limit of one home office or one in-law suite per principal dwelling unit. Structures must follow setback, coverage and building height rules in Section 6.15.
- J. Permitted parking locations are prescribed in Section 6.15 and 6.18.
- K. Parking inventory requirements are prescribed in Section 6.18.
- L. Signs are permitted as accessory to Live/Work units. Requirements are prescribed in Section 6.17.
- M. Sidewalk and street tree requirements are prescribed in Section 6.14, 6.16.4 and 6.16.11.
- N. All utilities and related appurtenances shall be located underground or be in the main building or structure.

6.16.2 Non-Residential, Multifamily and Mixed Use Building Architecture

The following shall be required of Elevator Flats, Commercial, Government/Civic/ institutional, Flex Mixed Use, Liner buildings and Parking Structures:

- A. Building fronts shall be oriented to the primary street upon which the structure is sited. Mixed use buildings, buildings with ground floor store fronts and corner buildings shall have multiple entrances at least one entrance addressing each street frontage.
- B. Building with ground floor retail shall be modulated with at least one pedestrian (patron) entrance every 40' along the street frontage.

- C. All buildings shall have pedestrian access on both the street frontage side of the building (Front) and the off street parking lot side of the building (Rear)
- D. Ground floor retail establishments shall have patron access from both the front and the rear off street parking lot side of the building.
- E. The vertical plane of storefronts shall be setback or recessed a minimum of 16 inches from the primary vertical plane of the façade.
- F. Storefronts shall be shaded by either an awning or solar screen.
- G. Entablatures shall define the transition of ground floor storefronts and the second floor of all mixed-use buildings.
- H. There are to be no blank facades. All facades must provide windows and façade offsets and breaks as prescribed in Section 6.15. Display windows for ground floor store fronts shall not be blocked with merchandise or interior merchandise displays.
- I. Awnings, balconies, bay windows and upper floor terraces are permitted. Awnings, upper floor balconies and bay windows may encroach into setbacks up to 3 feet. For front yards with 0' setback requirements, awnings, and upper floor balconies and bay windows may encroach up to 3' into the right of way providing a minimum clearance of 10 feet.
- J. All HVAC and telecommunication systems must be screened or minimized from public view and from adjacent properties. Screens must be designed to blend with the architecture of the principle building.
- K. Windows shall be un-tinted. Mirrored or reflective glass is not permitted.
- L. Buildings containing residential units shall provide a minimum average of 50 square feet per dwelling unit of outdoor space. Space may be private or communal for the sole use of the building residents. Outdoor space may take the form of balconies, roof decks or terraces and shall be incorporated into the architecture of the building.

- M. Flex Buildings, Flats, and Multi-Story Commercial Buildings shall provide a minimum ground floor lobby of 400 square feet.
- N. Ground floor eating establishments shall be permitted to operate outdoor cafes provided that a minimum of five (5) feet of pedestrian clearance is provided for sidewalks and entrances remain unimpaired. Pop-out café windows are encouraged.
- O. No commercial statements of the occupant's products or services shall be permitted as part of the building façade or elevation with the exception of signage.
- P. Signs are permitted as prescribed in Section 6.17.
- Q. Building roofs are to be uncluttered. Vertical roof projections such as vents, stacks, elevator mechanical rooms, or roof mounted equipment shall be integrated into the architecture. All penetrations through the roof (i.e. mechanical equipment or skylights) shall be organized in a manner that is integral to the architectural form of the building, completely screened from view by parapet walls or an approved enclosure. Screen shall reflect and complement the architecture of the building.
- R. Buildings shall be arranged and clustered to maximize opportunities for share circulation, parking, loading, pedestrian walks, walls, plaza connections, and transit related facilities. See Section 6.12 for typical block structure.
- S. Telecommunication equipment is permitted provided they are (1) located on the roofs of buildings, (2) not to exceed in height 10 feet above a prescribed building height and (3) incorporated into the architecture and screened from public view.
- T. Permitted parking locations are prescribed in Section 6.15 and 6.18.
- U. Parking inventory requirements are prescribed in Section 6.18.
- V. Sidewalk and street tree requirements are prescribed in Section 6.14, 6.16.4 and 6.16.11.

- W. Loading and trash enclosure requirements are prescribed in Section 6.16.8.
- X. Buffer requirements are prescribed in Section 6.16.6.
- Y. All utility and related appurtenances shall be located underground or be in the main building or structure.
- Z. A Drive-Through Facility shall only be permitted to locate in the rear yard or otherwise behind a building in the Downtown Transect whereas a drive-through facility located in the Live-Work Transect shall only be permitted to located in either the side or rear-yard areas; and, in both the Downtown and Live-Work Transects, Drive-Through Facilities shall be limited to 2 service lanes.

6.16.3 General Landscaping

- A. Landscape plans shall be signed and sealed by New Jersey certified Landscape Architects and shall be subject to Planning/Zoning Board review and approval.
- B. Landscape shall be required in those areas that are designated as required setback areas, areas within parking lots, and areas not used for ingress, egress, parking, or storage, and areas subject to grading and re-contouring. Although each site could have a different building configuration and use, and in some cases individual owners, an overall landscape theme dealing with major design elements shall be established. These elements shall include:
 - 1. Setback and buffer areas along streets as well as adjacent residentially zoned areas and properties;
 - 2. Parking lots and areas around buildings.
- C. Landscape is permitted to be integrated with other functional ornamental site design elements, where appropriate, such as recreational facilities, paths and walkways, foundations, water features, trellises, pergolas, gazebos, fences, walls, street furniture and public art.

- D. Landscaping schemes are permitted to include seasonal flowers in planters, planting beds and hanging baskets.
- E. High-maintenance lawn areas that do not support active recreation shall be prohibited in order to reduce the need for irrigation and mowing. Drought tolerant, noninvasive, native groundcovers and grasses shall be used as a substitute for lawn areas.
- F. Plant suitability, maintenance and compatibility with site and construction features are critical factors which shall be considered when preparing a landscape plan.
- G. Plantings shall be drought tolerant, noninvasive and native species.
- H. Only nursery grown plant materials shall be acceptable and all trees, shrubs and groundcovers shall be planted according to accepted horticultural standards.
- I. Trees and shrubs shall be planted according to the following minimum caliper, height and spread requirements:
 - 1. Street trees: 3 ½-inch caliper
 - 2. Ornamental deciduous trees: 8-foot height
 - 3. Evergreen trees: 6-foot height
 - 4. Shrubs: 18-inch spread
- J. Within two (2) years from the time of planting, all dead or dying plants, whether installed new, transplanted or existing, shall be replaced by the developer. The developer shall be responsible for the required maintenance and watering for the initial two (2) years. Trees or other vegetation which die after the second year shall be replaced and maintained by the property owner or their agents.
- K. All landscaping shall be maintained in excellent condition by the property owners or development association by cutting, trimming, feeding, watering and weeding as necessary.

- L. Landscaping shall be installed upon the substantial completion of the building, weather and season permitting, and an underground irrigation system may be required by the Planning/Zoning Board in some landscaped areas.

6.16.4 *Street Trees*

- A. Street trees shall be required along all existing or proposed public streets within the public frontage of a subdivision or site plan, and are in addition to other required plantings.
- B. Requirements for specific type specie and tree spacing per street frontage are prescribed in Section 6.14. The spacing and species of street trees is based on the hierarchy of streets and their right-of-way widths and thus shall be determined by frontage types.
- C. In order to determine the required number of street trees, the linear footage of public frontage shall be divided by the required planting interval, without deducting areas for driveway curb cuts or crosswalks. Trees need not be evenly spaced. If the linear footage of Public Frontage is less than the required planting interval, one tree shall be provided at the discretion of the Planning/Zoning Board.
- D. Street tree plantings shall be substantially uniform in size and shape and shall have straight trunks.
- E. Tree pits shall be at least one-third (1/3) larger in width and in depth than the existing root ball of the particular tree to be planted. The pit in which the tree is to be planted shall contain proper amounts of topsoil and peat moss, but no chemical fertilizer shall be added until the tree has been planted for one year.
- F. Subsequent or replacement trees shall conform to the type of existing tree in a given area.
- G. Within sight triangles, a tree may be permitted only with the site-specific approval of the City Engineer.
- H. Root barriers shall be placed along the tree side of a sidewalk for a distance of 12 feet, centered on the trunk.

- I. Street tree requirements may be waived by the Planning/Zoning Board where existing preserved vegetation is considered sufficient to meet these requirements and is reasonably assured of continued survival.
- J. Street trees shall be planted so as not to interfere with the installation, safe use and maintenance of sidewalks, roadways and utilities. The choice for each frontage type has taken into consideration such encumbrances.
- K. All trees shall be nursery-grown stock and shall have a root ball wrapped in burlap, with a replacement guarantee by the developer of two (2) years.

6.16.5 *Parking Lot Landscaping*

- A. A screen planting, fence, wall or combination thereof, not less than four (4) feet in height, measured from the centerline of the adjacent street, shall be provided between off-street parking areas and any public right-of-way other than "Small" streets and railroads, except where a building intervenes. This screen planting design shall achieve 50% opacity after five years' growth.
- B. Planting islands shall conform to the following standards:
 - 1. One (1) planting island, a minimum of nine feet wide by 18 feet long, spaced not more than 180 feet apart, or every 20 parking stalls, in single or double bays; or one (1) diamond tree pit, a minimum of nine (9) square feet, spaced not more than 45 feet apart, or every five (5) parking stalls in double bays, which shall be placed at a 45 degree angle to the stalls, at the conjunction of four (4) adjacent stalls.
 - 2. Islands shall be placed opposite each other in adjacent rows of parking in order to reduce the number of raised islands and increase the area available for tree roots.
 - 3. The last parking stall in a row shall be separated from drive aisles by a planting island with a minimum width of nine (9) feet.

4. Each planting island shall contain one (1) shade tree and three (3) shrubs, along with groundcover, grasses and/or perennials, so as to cover the entire area when installed. Shrubs located at the end of an island shall be maintained at a maximum height of three (3) feet for sight line and safety purposes.
5. Each diamond tree pit shall contain one (1) tree along with groundcover.
6. All shrubs shall be set back a minimum of two (2) feet from the edge of the island.

6.16.6 Buffer/Landscaping

- A. The primary purpose of buffers is to eliminate views and reduce noise perception beyond the lot line.
- B. Buffers are required in the following locations:
 1. Along all lot lines separating Single Family Dwelling, Twin and Townhouse buildings from all other building types and lots used exclusively for parking.
 2. Along all lot lines separating any building from utility or telecommunication equipment.
 3. Along edges where parking lots share property line with public streets.
 4. Along all railroad right-of-ways.
- C. Buffers shall have a minimum width of five (5) feet and consist of a living fence that is comprised of a combination of plantings, decorative fencing or masonry wall. Chain link fences are prohibited. Fences shall have a minimum height of six (6) feet and a maximum height of eight (8) feet. Buffers that incorporate vegetation shall be designed to achieve 80% opacity within a height of six (6) feet after five (5) years growth.
- D. The location and design of buffers shall consider the building being screened, its distance from the lot line and differences in elevations grade.

- E. Buffer widths shall be in addition to any required building setbacks.
- F. No structure, activity, storage of materials or parking of vehicles shall be permitted within a buffer.
- G. Buffers shall be identified on site and subdivision plans.

6.16.7 *Lighting*

- A. Pedestrian light fixtures along sidewalks and walkways shall have a maximum mounting height of 15 feet.
- B. Parking lot, service area and street lighting shall be provided by freestanding light fixtures designed to minimize glare to the street and adjacent lots. Such fixtures shall have a maximum mounting height of 25 feet.
- C. The type and color of the light fixture and pole shall be evaluated for compatibility with existing street lighting in the vicinity of the site.
- D. City blocks shall have uniform street lighting themes.
- E. All lighting shall be serviced by underground wiring.
- F. Spotlight-type fixtures attached to buildings are prohibited.
- G. Where lights along lot lines will be visible from the interior of adjacent buildings, the lights shall be properly shielded and/or mounting heights reduced.
- H. All lighting designs and installations are subject to Planning/Zoning Board review and approval.
- I. All lighting plans shall be accompanied by a point-by-point plan indicating numerical illumination levels. The plan shall indicate the average, minimum, maximum and minimum to maximum illumination levels for maintained foot-candles.
- J. The use of string lighting in trees, up-lighting in trees or other special effect lighting and after-hours lighting is prohibited except when part of a City-sponsored streetscape decoration initiative.

- K. All lighting shall be designed and installed to avoid off-site spillage halo effect to the greatest extent reasonably possible and consistent with public safety. Light sources should not be visible from outside the boundaries of the site.

6.16.8 Loading, Refuse Collection and Other Service and Utility Areas.

- A. All loading, refuse collection and service and utility areas must be sufficient to serve the business being conducted on the parcel without using adjacent streets. No such areas shall be visible from any neighboring property or adjacent street. Provision must be made for handling all freight on those sides of the buildings which do not face a street. The recommended method of screening shall consist of walls and gates compatible in color and texture with the building material, buffered by a landscape strip. The strip shall have a minimum width of three (3) feet and shall be located on all sides that do not include an entry access or abut a windowless façade. Buffers shall be planted so as to sufficiently obscure the view of the facilities from public view throughout the year. The screen shall not be less than five (5) feet in height at the time of planting and eight (8) feet in height at the time of maturity.
- B. All outdoor containers shall be visually screened within a durable, noncombustible enclosure, so as not to be visible from adjacent lots or sites, neighboring properties or streets. No collection areas shall be permitted between a street and the front of a building.
- C. Collection areas shall be effectively designed to contain all material generated on site and deposited between collections. Deposited materials should not be visible from outside the enclosure.
- D. Collection enclosures shall be designed of durable materials with finishes and colors which are unified and harmonious with the overall architectural theme.
- E. Collection areas shall be located upon the site so as to provide clear and convenient access to collection vehicles. Refuse collection and recycling areas shall not be located within required landscaped yards and buffers.

- F. An option to reduce the visual impact of the collection containers is to store and compact material inside the building, thus eliminating the need to screen outside containers.
- G. Delivery, loading, trash removal or compaction, or other such operations may be limited by the Planning/Zoning Board between certain hours where noise impacts at the lot line of any adjoining residential property or district shall be required to meet City and State requirements. Also, the applicant shall provide an effective litter management plan, subject to Planning/Zoning Board approval. Such management plan shall be submitted with an application for final site plan approval.

6.16.9 *Fence and Walls*

- A. Fences and walls are to be used solely for the screening and buffering purposes provided in other sections of the *FBC*, in addition to their use on the perimeter of private patios and courts, recreational areas and other sensitive areas deemed appropriate by the Planning/Zoning Board. All approved fences and walls shall be designed as integrated parts of the overall architectural and site designs.
- B. Chain link fences are prohibited. Permitted fence styles include wrought iron, board-on-board and other decorative styles approved by the Planning/Zoning Board.
- C. Concrete masonry unit/cinder block wall veneers are prohibited. Permitted wall veneer materials include brick, polished or unfinished stone, architectural paneling and other materials approved by the Planning/Zoning Board.
- D. Fences and walls shall not be located where they impede pedestrian or bicycle circulation through or between site areas.
- E. No hedge, wall or fence of any type shall be erected or maintained if it is deemed a safety hazard by obstructing the view of motorists.
- F. Fences and walls not used for buffering purposes shall have a maximum height of six (6) feet.

6.16.10 Maintenance

All site improvements including, but not limited to, streets, drives, parking lots, drainage areas, culverts, curbing, buildings, and lighting must be maintained in good condition and repair by either the owner or other designated entity. Such maintenance includes, but is not limited to, the following:

- A.** Prompt removal of all litter, trash, refuse, and wastes.
- B.** Lawn mowing.
- C.** Tree and shrub pruning.
- D.** Landscape watering.
- E.** Keeping exterior lighting and mechanical facilities in working order.
- F.** Keeping lawn and garden areas alive, free of weeds, and attractive.
- G.** Keeping parking areas, driveways, and roads in good repair.
- H.** Complying with all government health and police requirements.
- I.** Striping of parking areas and repainting of improvements.
- J.** Repair of exterior damages to improvements.

6.16.11 Sidewalks and/or Pathways

- A.** Sidewalks and/or pathways shall be installed by the developer within perimeter landscape areas along streets and drives.
- B.** On-site pedestrian circulation systems shall be provided to meet the circulation needs of on-site users. Such systems shall provide safe, all-weather efficient and aesthetically pleasing means of on-site movement and shall be an integrated part of the overall architectural and site design.

- C. Sidewalks shall be provided along the full length of the building, along any façade featuring a visitor or customer entrance, and along any façade abutting parking areas. Sidewalks shall provide weather protection features such as awnings or arcades when located close to customer entrances.
- D. Where appropriate, connections shall be made between on-site and perimeter sidewalk and/or pathway circulation systems.
- E. Pedestrian crosswalks shall be clearly delineated by a material different from the surrounding road surface through the use of durable, low-maintenance surface materials. Sidewalks shall be scored concrete to enhance pedestrian safety and comfort, as well as the attractiveness of the sidewalk and/or pathway. Other materials such as London pavers, brick pavers or stone pavers are permitted on a case-by-case basis.

6.16.12 Environmental Design

The following environmentally sustainable features are permitted to be incorporated into the site and building design of new development:

- A. Solar panels (electric and hot water).
- B. Skylighting that allows buildings to take advantage of natural light during the day and natural heat during the cold months.
- C. Solar screening that allows for natural cooling during the warm months.
- D. Wind turbines.
- E. Green or vegetative roofs.
- F. Highly reflective and highly emissive roofing material.
- G. Greywater systems such as cisterns and rain barrels for use in landscape irrigation and toilets.
- H. Building materials with a high percentage of recycled content.

- I. Bio-based and rapidly renewable building materials.
- J. Building materials with lower VOC (Volatile Organic Compound) values.
- K. Drought tolerant, noninvasive, native landscape species.
- L. Porous pavement.
- M. LED (light emitting diode) lighting.
- N. Bioswales and rain gardens.

6.17 Sign Standards

The following sign standards are applicable across all transect zones and building types. The determination of where signs are permitted is regulated by the Frontage Type (see Section 6.14) upon which a building is sited. These regulation are intended to ensure that redevelopment in the Downtown Business District is of the highest quality and adheres to the vision articulated in the *Redevelopment Plan*.

6.17.1 Intent and Purpose of Standards

- A. To implement and enforce the sign regulations set forth below.
- B. To encourage signage compatible with the character of buildings and integrated with the building's architectural design and other signs on and in the vicinity of the building.
- C. To promote creativity in signage design and to develop a distinctive image, utilizing high quality materials and craftsmanship.
- D. To encourage efficient use of signage in order to avoid visual clutter.
- E. To protect the public health, safety and welfare by promoting signage that effectively identifies establishments and is readily visible and identifiable to both pedestrians and motorists.

6.17.2 *Sign Approval & Enforcement Procedures*

- A. Any person wishing to erect, alter, modify or expand any sign shall first apply to the Lessor (property owner) or its property management agent for review and approval of such sign improvement. Upon receipt of such approval in writing, such person shall then apply to the City Planner, City Zoning Officer, and City Construction Official for all necessary permits. In order to determine compliance with sign standards, the applicant shall provide all information regarding the sign, including but not limited to, materials, colors, dimensions, text, graphics, fonts, lighting, fabrication and installation details.
- B. Once the City Planner determines conformance with the sign standards, the City Zoning Officer shall issue zoning approval for such sign. Notwithstanding the following requirements, the City Planner shall have some limited discretion in the administration of these standards. In the exercise of that discretion, the City Planner may approve a sign that deviates in a minor way from such standards if it is determined that the sign substantially conforms to requirements set forth.
- C. The Zoning Officer, in consultation with the City Planner, shall have the final authority to approve or deny a proposed sign request, to direct the removal of any sign installed without such approval and to enforce all provisions contained in this plan.
- D. Failure to comply with sign provisions may result in enforcement action by the Zoning Officer, in consultation with the City Planner, which may lead to the imposition of penalties, including fines, as provided by the City Code.
- E. Any applicant aggrieved by a compliance decision rendered by the Zoning Officer and/or City Planner may appeal such decision to the municipal entity having authority over such matters as determined by the City Zoning Officer and/or City Planner in consultation with the Board or City Attorney.

6.17.3 *Sign Area Measurement*

- A.** The areas of any sign or sign face shall be computed as the product of the largest horizontal width and the largest vertical height of the lettering, illustration, display or background. Where there is no geometric frame or edge to the sign, the area shall be defined by a projected, enclosed, four-sided (straight sides) geometric shape which most closely outlines the sign. This shall not be construed to include the supporting members of any sign which are used solely for such purpose.
- B.** The sign area measurement shall not include any framing, trim or molding. If lettering, illustration, display or background is attached directly to the face of a building, the height or width of the sign shall be the height or width of the largest letter, illustration, display or background, whichever is greater.
- C.** For the purpose of computing the allowable area of a double-faced sign, one sign shall be considered.
- D.** If sign faces are not parallel (e.g. V-shaped sign), both faces shall be considered in computing the sign area.

6.17.4 Number of Signs

Each store or establishment is permitted the following number of signs:

| SIGN TYPE | | PERMITTED NUMBER OF SIGNS |
|-----------|--|--|
| A | Awning/Canopy Façade/Wall Projecting/Hanging | Choose combination of 2 per establishment face* with a principal store frontage, plus 1 additional per establishment face without a principal frontage |
| B | Directional | 1 per ingress/egress, plus 1 freestanding directory sign |
| C | Menu Board | 1 per establishment |
| D | Portable | 1 per establishment |
| E | Tenant Identification | One (1) per group of establishments |
| F | Window | One (1) per establishment |

* The front, side and rear façade each qualify as an establishment face. Shops with entrances on parking lots are required to provide at least one (1) sign.

6.17.5 Sign Bulk Standards

All sign shall adhere to the following bulk standards:

| | SIGN TYPE | MAX AREA | MAX LENGTH | MAX LETTER SIZE | MIN HEIGHT | MAX HEIGHT |
|---|--------------------------|---------------------------------|------------|-----------------|------------|------------|
| A | Awning/ Canopy | 3 sq feet | 6 feet | 8 inches | 8 feet | 14 feet |
| B | Directional | 4 sq feet | - | - | - | 6 feet |
| C | Façade/ Wall | 1 sq ft per lin ft of façade | 12 feet | 16 inches | 8 feet | 14 feet |
| D | Menu Board/ Portable | 8 sq feet | - | - | - | - |
| E | Projecting/ Hanging | 8 sq feet | 4 feet | 16 inches | 8 feet | 14 feet |
| F | Tenant Identification | 6 sq feet | 3 feet | 2 inches | - | 6 feet |
| G | Window | 6 sq feet | 6 feet | 8 inches | - | - |

6.17.6 *General Standards*

- A. The sign regulations set forth in the City Code Article XV shall control sign standards not otherwise enumerated in the FBC.
- B. Flags and emblems of a government organization or of a political, civil, philanthropic, educational, medical or religious organization are exempt from all provisions of the FBC and these requirements.
- C. Exposed raceways or ballast boxes are not permitted.
- D. Sign company names or stamps shall be concealed.
- E. Public safety decals in minimum sizes may be used subject to compliance with all applicable codes.
- F. Tenants shall not use or affix to a building or window, temporarily or permanently, any stickers or paper, cardboard, plastic or other similar low quality signs; nor shall banners, pennants, and/or flag type signs of various materials be so used.
- G. No signs shall be placed in front of building windows so as to obscure views into and out of the building.
- H. Special mounting standards shall be used for traffic directional, handicapped parking and other similar on-site signage. Signs shall comply with Subtitle 1 of Title 39 (Standard Traffic Signs) and be mounted utilizing the special design details for such signs approved for the development.
- I. After a tenant has vacated the premises, the Lessor shall be responsible for the removal of all prior tenant signage and restoration of areas where such signage was located to the original conditions, including painting and patching, if necessary.
- J. All signs shall be securely attached to the building by means of metal anchors, bolts or expansion screws, subject to any required review or approval by the City Construction Office.
- K. No sign shall be placed, posted or affixed to any roof, tree, fence and fence post, utility pole or similar structure.

- L. A truck, trailer or other vehicle, moving or non-moving, not used for normal day-to-day operations of the business, or not regularly moved for business related purposes shall be prohibited from use as a sign.
- M. Animated, moving, fluttering (including business related banner, flags and pennants), blinking, flashing, sparkling or glittering signs are prohibited. Signs that produce glare or use bare bulbs, such as exposed neon and other exposed tube signs, as well as signs that use bright day-glo or fluorescent colors are prohibited.
- N. All changeable copy signs are prohibited, except for permitted menu board type signs and permitted office tenant directory/identification signs.
- O. Window painting or decorations for holidays such as Halloween, Thanksgiving or Christmas shall be permitted and exempt from window sign standards. However, such painting and decorations shall be strictly limited to reference to the applicable holiday and shall not include reference to any sales, promotions, merchandise, goods or services. In addition, all such decorations shall be removed within ten days of the end of the applicable holiday.
- P. Permanent signs that advertise continuous sales, special prices, etc. are prohibited.
- Q. To the maximum extent possible, all wiring associated with signs shall be installed and maintained so as not to be in view of the public. Wiring shall under no circumstances be installed within conduit attached to face of any building.
- R. Signs shall not protrude above the first floor clear height prescribed in Section 6.15.

6.17.7 *Maintenance*

- A. Signs and supporting hardware shall be structurally safe, clean, free of visual defects, and functioning properly at all times. The responsibility to satisfy this requirement shall rest with the Lessor and shall be subject to the enforcement of the City Zoning Officer.

- B. Repairs to signs shall use materials and craftsmanship that are comparable or better than were used for the original sign, and shall be subject to the review and approval of the City Planner.

6.17.8 Sign Illumination

- A. Illumination by indirect source of light is encouraged. Individual channel letter signs with translucent faces and reverse channel letter type signs are permitted. The use of LED for such signs is encouraged.
- B. Care shall be taken to prevent light from spilling over into residential areas.
- C. Any illuminated sign adjacent to a residential use shall not be illuminated between the hours of 11:00 pm and 7:00 am.
- D. External light sources shall be directed and shielded to limit direct illumination of any object other than the sign.
- E. Externally illuminated signs shall be lit only by downward pointing shielded fixtures where 90 percent of the light emitted from such fixtures is shone directly on the face of the sign.
- F. Lights shall be lit only to the minimum level required for nighttime readability.
- G. No exposed sign illumination and no flood lighting of signs or storefronts shall be permitted.
- H. Any light fixture used to illuminate a sign shall be compatible in design with architecture of the principal building to which it is attached.
- I. Exposed bulbs or tubing, including neon are prohibited.
- J. Light sources shall utilize energy efficient fixtures to the greatest extent possible.
- K. Burned out lamps, failing ballasts or other necessary electrical components shall be replaced within two weeks.

6.18 Parking and Circulation Standards

6.18.1 Off-Street Parking

- A. Each building site shall include adequate off-street parking as per the ratios established in the FBC. The Urban Land Institute's (ULI) *Shared Parking* and the Institute of Transportation Engineer's (ITE) *Shared Parking Planning Guidelines* publications *may* be used by the applicant when it is deemed appropriate by the Planning/Zoning Board.
- B. Each applicant for non-residential development greater than 5,000 square feet shall be required to conduct a parking study to determine the number of off-street parking spaces required to accommodate peak parking demand. In calculating peak parking demand, the applicant shall:
 - 1. Project the peak number of occupied parking spaces using formulas and procedures presented in the most recent edition of *Parking Generation*, and information published by the ITE.
 - 2. Calculate the number of parking spaces needed by expanding the number of occupied parking spaces by 10% to assure a reasonable number of vacant parking spaces to permit turnover of the parking supply.
 - 3. Identify how these parking spaces will be assured to future occupants and customers of the proposed land uses.
- C. Parking is prohibited in front and side yards unless otherwise noted in the FBC.
- D. All off-street parking spaces shall be provided within 250 feet of an entrance to the building they are intended to serve and be located on the same block, provided that such spaces shall be in the same ownership as or under lease by the owner of the building to which they relate; shall be subject to restrictions adequate to ensure that the number of spaces will be available throughout the life of such use; and shall be located in a transect in which a parking lot or structure is permitted.

- E. The Planning/Zoning Board may require the establishment of cross easements for adjacent parcels with existing or proposed interconnected parking lots.
- F. All residential off-street parking for Townhouse, Twin, Single Family Dwelling and Live/Work buildings shall be dedicated to the user 24 hours per day.
- G. All residential off-street parking for units located in Flex Mixed Use, Flat and Liner buildings shall be dedicated to the user between the hours of 5pm and 9am. A 24-hour per day dedication may be permitted at the discretion of the Planning/Zoning Board.
- H. Non-residential off-street parking shall be shared parking.
- I. Residential parking shall be provided at a minimum of 1 off-street parking space per dwelling unit. *Residential Site Improvement Standards (RSIS)* are also applicable.
- J. Commercial/Retail use parking shall be provided at a maximum of 4 off-street parking spaces per 1,000 square feet of gross leasable space.
- K. Parking shall be provided for civic and cultural institutions on a case by case basis subject to a parking analysis.
- L. All other parking shall be provided at a maximum of 3 off-street parking spaces per 1,000 square feet of gross leasable space.
- M. Off-street parking for Flat, Townhouse, Twin, Single Family Dwelling and Live/Work buildings shall adhere to the following standards:
 - 1. With the exception of lots that do not back up to alleys as depicted on the *Street Regulating Plan*, driveways and driveway access shall be prohibited in any front yard area.
 - 2. Parking for Townhouse buildings shall be provided as driveways or garages accessed from a rear alley.

3. If access of the garage is provided from a street, the front entrance shall be prohibited from being located forward of the rear wall of the principal building.
 4. Driveways serving Twin, Single Family Dwelling and Live/Work buildings that are accessed through the front yard area shall be no wider than 10 feet.
 5. Driveways serving Flat buildings shall be no wider than 24 feet.
 6. Driveways providing access to Townhouse, Twin, Single Family Dwelling and Live/Work buildings shall be set back a minimum of three (3) feet from the side lot line of dwelling units, unless such a driveway is shared by buildings on two (2) adjacent lots on the common side lot line and is subject to a cross easement.
 7. Parking for Flat buildings may be provided in common parking lots located on a lot other than that containing the building. Such a parking lot must be within 300 feet of the entrance of the building being served.
 8. Corner buildings without alley frontage shall provide parking access from the lesser of its two (2) frontages.
- N. Landscaping and screening standards are provided in the *General Landscape* and *Parking Lot Landscape* sections of this plan.
- O. Parking areas shall be located to maximize the potential for shared parking between uses. Parking areas shall be designed and located so as to facilitate transit, bicycle and pedestrian access. Parking spaces closest to building entrances, shall be reserved for:
- P. Parking facilities shall adhere to the Americans with Disabilities Act.
- Q. Employee vanpool vehicles; and
- R. Bicycle racks shall be provided as prescribed in Section 6.14.

- S. Textured crosswalks shall be used where pedestrians come in contact with vehicular traffic. All walks must be well lighted. On-site pedestrian linkages shall connect buildings to external perimeter pedestrian systems.

6.18.2 *On-Street Parking*

- A. On-street parking credit may be counted for spaces which do not actually front along the property for which they are to be considered as serving provided they are located within 1,000 feet of the property for customer or client parking and 2,000 feet of the property for employee parking. The Planning/Zoning Board may, at its discretion, elect to give such spaces on-street parking credit; deny any credit for such spaces; or give partial credit for such spaces depending on the use of the property upon which they front and the uses of adjacent properties located between the property seeking to apply the spaces to their parking needs and the spaces themselves.
- B. The following guidelines shall be used to determine which on-street parking spaces an applicant may assume are available:
 - 1. Parking spaces located along a Local or Small frontage type as identified in the *FBC Regulating Plan* shall not be included in the parking inventory for Government/Civic/Institutional, Flex Mixed Use, Elevator Flat, Commercial or Liner buildings, unless the buildings themselves have frontage on such a street type.
 - 2. Spaces directly abutting an applicant's lot are 100% available to the applicant, unless parking regulations restrict parking during the time period when the spaces are needed to satisfy the applicant's peak parking demand.
 - 3. Spaces abutting Government/Civic/Institutional, Flex Mixed Use, Commercial or Liner building shall not be counted unless that use has an off-setting peak parking demand or unless the owner of the use certifies that it has no need for the available parking spaces.

4. Parking that abuts open space, wetlands or detention basins shall be considered available but must be shared with other nearby users. The applicant shall propose to the Planning/Zoning Board the percentage of the parking for which credit shall be taken and document the methodology used in determining that percentage.

6.18.3 *Shared Parking*

- A. Shared parking is defined as joint utilization of a parking area for more than one use, either on-site or between nearby properties through a dedicated arrangement, in order to fulfill their individual parking requirements because their prime operational hours do not overlap.
- B. Shared parking shall be encouraged for all parking lots that serve Government/Civic/Institutional, Flex Mixed Use, Commercial and Linear buildings. Where necessary the Planning/Zoning Board may, at its discretion, permit limited amounts of dedicated parking and may set the hours for such dedication. The dedication of parking spaces and hourly restrictions shall be done with the intent of encouraging shared parking.
- C. An applicant seeking to satisfy its parking requirement using a shared parking approach shall prepare a parking report that documents how an adequate supply of parking spaces will be provided to satisfy projected parking demand. The report shall be prepared using shared parking procedures and methodology presented in the most recent versions of the ULI *Shared Parking* and the ITE's *Shared Parking Planning Guidelines* publications.

- D. A shared parking study and report shall contain the following:
1. Calculation of the projected peak parking for each land use that will be sharing the available parking supply.
 2. Calculation of the extent to which parking demand will be mitigated on the site as a result of trips captured from adjoining land uses and therefore occurring without the use of a vehicle.
 3. Calculation of the peak parking accumulation for the development.
 4. Expansion of the peak parking accumulation by 10% to determine the needed supply of parking spaces.
 5. Determination of the number of off-street parking spaces to be supplied in lots controlled by the applicant and lots controlled by a third party.
 6. Determination of the number of on-street parking spaces that are available to the development.
 7. Determination of any additional parking solutions necessary to reduce parking demand or mitigate an insufficient supply of parking. Other methods may include the use of fringe parking lots, satellite lots with shuttle service, employee transit use in lieu of parking and/or valet parking service that makes use of identified off-street parking lot.
- E. The minimum parking requirement for each use shall be multiplied by an "occupancy rate" as determined by a study of local conditions (or as found in the following "Occupancy Rate" table, which is followed by step-by-step directions), for each use for the Weekday night, daytime and evening periods, and weekend night, daytime and evening periods, respectively.

| 1. Occupancy Rate Table | | | | | | |
|-------------------------|-----------------|----------------|----------------|-------------------|----------------|----------------|
| USES | MONDAY – FRIDAY | | | SATURDAY & SUNDAY | | |
| | 8am - 6pm | 6pm - Midnight | Midnight - 8am | 8am - 6pm | 6pm - Midnight | Midnight - 8am |
| Residential | 60% | 100% | 100% | 80% | 100% | 100% |
| Office | 100% | 20% | 5% | 5% | 5% | 5% |
| Retail/Commercial | 90% | 80% | 5% | 100% | 70% | 5% |
| Hotel | 70% | 100% | 100% | 70% | 100% | 100% |
| Restaurant | 70% | 100% | 10% | 70% | 100% | 20% |
| Movie Theater | 40% | 80% | 10% | 80% | 100% | 10% |
| Entertainment | 40% | 100% | 10% | 80% | 100% | 50% |
| Institutional | 100% | 40% | 5% | 10% | 10% | 5% |
| Religious | 20% | 40% | 5% | 100% | 50% | 5% |

* This table indicates the percent adjustment of the minimum parking requirement during each time period for shared parking.

** Percentages set forth in the Occupancy Rate table are set to include a small "safety margin" of parking beyond that minimally needed to serve an average peak demand. Therefore, a local study of parking demand may yield a greater reduction in parking required.

*** "Other" occupancy rates as demonstrated by applicant via parking study or other credible evidence.

1. Sum up the adjusted minimum parking requirements of each land use for each of the six time periods to determine an overall project minimum parking requirement for each time period.
2. The highest of the six time period totals shall be the minimum parking requirement for the mixed-use project.
3. One hundred percent (100%) of the parking supply shall be provided within 250 feet of an entrance to the building(s) it will serve unless waived via terms of item 4 and/or 5 below.

4. The number of parking spaces may be reduced on a space per space basis if the applicant can demonstrate that suitable alternative parking spaces can be secured through lease or other similar terms and are located within close proximity to the subject property or site. Spaces to be counted towards this shared parking arrangement must be demonstrated to be vacant and available during the peak hours of operation of the affected uses. Access to those spaces must be provided for vehicles and pedestrians in a safe and efficient manner. Such access shall include shared driveways and interconnected walkways.
5. If the parking requirement, or portions of the parking requirement, cannot be met, developers may purchase relief at the 2010 rate of _____ dollars per parking space. This fee will be deposited in the City Parking Improvement Fund to be used exclusively for the development, improvement or maintenance of public shared parking in the Redevelopment Area.
6. The City Zoning Officer may conduct site visits to confirm that the amount of parking provided is being utilized by the development and that the parking remains sufficient to meet the needs of the development. If at any time it is determined that this is no longer the case, the provision of additional banked or reserved parking spaces may be required by the City Zoning Officer and/or the City Planner to satisfy the demand.

6.18.4 Structured Parking

- A. Structured parking shall be faced with Liner buildings.
- B. Structured parking shall utilize the architectural vocabulary of adjacent façades in order to minimize the inherent look of the parking structure and integrate the structure as part of an overall façade.
- C. Parking entrances shall be identified through increased massing and detail, material change or signage and shall be safely and clearly visible from the street.

- D. Structured parking layouts shall take into consideration pedestrian circulation and connections with adjacent building.
- E. Utilitarian appearances of structured parking shall not be permitted. Structures shall have design treatments such as colonnades, arcades, awnings, landscape, street furniture and other public amenities to create the appearance of an occupied building. Blank walls shall not be permitted.
- F. Parked cars shall be visually screened from adjacent buildings and the public right-of-way. Such screening shall be in keeping with the building's architectural style and materials.
- G. Locating structured parking within the interior of a City block, surrounded by buildings, is required.
- H. With the exception of lots that do not back up to alleys as depicted on the *Street Regulating Plan*, vehicular access to structured parking shall be provided from alleys.

6.18.5 *Parking Management*

The proper management of on-street and off-street parking within the Redevelopment Area will contribute to a successful revitalization of downtown Woodbury. The use, construction, and maintenance of parking facilities exist in either private or public ownership. Only when parking is managed comprehensively can spaces be made available economically. As such, it is recommended that the City explore the establishment of a parking authority, parking utility or a parking department, bureau or agency to oversee all aspects of parking in the Redevelopment Area. By way of example, the City of Summit, New Jersey, has established a "Parking Services Agency" which is dedicated to enhancing "... the quality of life for shoppers, residents and employees of Summit by encouraging the efficient movement of people and goods throughout the City". The Township of West Windsor has a parking authority which manages parking spaces at the Princeton Junction Train Station. Their primary mission statement is to "provide safe, reliable, and convenient parking, with a staff dedicated to customer needs and committed to excellence". There are many other statewide examples of existing agencies involved with the comprehensive management of parking in an urban setting.

The City can create by resolution or ordinance a 5 person "Parking Authority" in accordance with the state "Parking Authority Law". According to the state statute the creating resolution or ordinance "may provide that no real property shall be acquired by the power of eminent domain without the consent of the governing body". Some of the powers and purposes of the authority would be the construction, provision or operation of off-street parking and the management and operation of on-street and other parking meters and related facilities. The statute states that: "Every parking authority is hereby authorized to plan, design, construct, reconstruct, enlarge, improve, manage, repair, operate and use such parking project or projects as in the opinion of the authority will provide an effective and satisfactory method in addition to promoting the purposes of the authority".

At the same time the City settles on a management approach to parking it should outline or otherwise identify the responsibilities of the parking authority, utility, department, bureau or agency. Some of the responsibilities could be to address all or some of the following:

- A. Free customer parking for a maximum number of hours.
- B. Time limits for street parking meters.
- C. An employee parking program.
- D. Valet parking.
- E. Handicapped parking location and quantity.
- F. Park & Ride garages/lots for commuters.
- G. One day visitor parking passes.
- H. Overnight parking passes.
- I. A parking validation program.
- J. Wayfinding signage.

The City should designate a parking manager to oversee the day-to-day operation, implementation and oversight of these responsibilities – both short and long-term.

6.19 Validity of Code

If the provision of any article, paragraph, subdivision or clause of this Code shall be judged invalid by any Court of competent jurisdiction, such Order or Judgment shall not effect or invalidate the remainder of any such article, paragraph, subdivision or clause and to this end, the provision of this Code is hereby declared to be severable.

All Ordinances or parts of Ordinances inconsistent with the Code are hereby repealed to the extent of such inconsistency.

This Code shall take effect immediately upon its final passage, publication and as required by law.

6.20 Definitions

ACCESSORY APARTMENT - A secondary dwelling unit established in conjunction with and clearly subordinate to a primary dwelling unit, whether a part of the same structure as the primary dwelling unit or a detached dwelling unit located above a private garage on the same lot. Also known as an "in-law suite" or as a "granny flat." The maximum size of an accessory apartment shall be 400 square feet.

ACCESSORY BUILDING – A building subordinate to the main building of a lot and used for purposes customarily incidental to those of the main building.

ACCESSORY BUILDING HEIGHT – An accessory building's vertical measurement from the mean level of the ground abutting the accessory building to a point midway between the highest and lowest points of the roof.

ACCESSORY BUILDING SETBACK – The required open space between an accessory building and the side and rear lines of a lot.

ACCESSORY USE – A use subordinate to the main use of land or of a building on a lot and customarily incidental thereto.

AMUSEMENT - Completely enclosed recreation activities. Incidental uses may include the preparation and serving of food. Included in this definition shall be bowling, roller skating or ice-skating, and motion picture theaters.

ARBOR – An open framework structure that forms a shelter, gateway framework or bower. Its primary purpose is to be a semi-architectural place for climbing plants to grow, while providing shaded seating, directional form to frame a view or to create a private out-of-doors area. An arbor can be arched or square-topped. It differs from a gazebo in that its roof area is open to the elements, while a gazebo traditionally has a solid roof that protects those seated beneath it from the elements.

ARCHITECTURAL PANELING - Modular, exterior panels applied to a building or structure that are not necessary for the structural integrity of the building or structure. Made of high quality materials such as wood, metal, or stone. Excluding aluminum, vinyl, or steel siding. Used to modulate the façade and create vertical or horizontal breaks.

ARTISANAL WORKSHOP – Shops of special trade including the manufacturing, compounding assembly processing, packaging or similar treatment of such products as: baked goods, candy, ceramics, pottery, china, weaving and other textile arts, painting, cooperage, woodworking and other artistic endeavors and similar trades. Retail sales of products made on the premises are encouraged.

AWNING – An ancillary lightweight structure usually of canvas, cantilevered from a façade providing shade to the fenestration and spatial containment to the pedestrian. Awnings, to be effective adjunct to a shop front, must thoroughly overlap the sidewalk, be no higher than 10 feet at the front edge of the sidewalk and shall extend no farther than possible as a self-supporting structure (i.e. no visible vertical support poles or columns).

AWNING/CANOPY SIGN – A non-illuminated sign that is attached to, affixed to or painted on an awning or canopy located over a window, door or entrance of a building. Signage is to be placed on the front valance portion of the awning/canopy structure. For non-fabric type awnings/canopies, signage may be placed along the upper edge of the front portion of the canopy. Where applicable, awnings/canopies may contain street numbers, but telephone numbers or product advertising and product logos are prohibited. Awning/canopy signs shall not be internally illuminated.

BALCONY – A platform that projects from the wall of a building and is surrounded by a railing or balustrade.

BLOCK – The aggregate of private lots, passages, rear lanes and alleys, circumscribed by public streets.

BOARDING HOUSE – Any private dwelling or dwelling unit where the owner, tenant or operator thereof is engaged in keeping one or more roomers or boarders who are not the husband or wife, son or daughter, mother or father, sister or brother of the owner, tenant or operator or of the spouse of the owner, tenant or operator and engaged in serving food to some or all of such lodgers, for a part of a day or longer period, under expressed contract or rate of payment.

BREEZEWAY – A covered passage one story in height connecting a principal building with another principal building or an accessory building.

BUFFER – A landscaped area designed to functionally separate and visually screen the elements and uses of land which abut it, including streets, and to ease the transition between them. Buffers may also function to reduce noise, glare and other nuisances.

BUILDING – A structure having a roof supported by columns or walls, entirely separated from any other structure by space or by walls in which there are no communicating doors, windows or similar openings.

BUILDING FACE OFFSET – A horizontal measurement between offsets in portions of a front or rear façade, as a means of breaking up the apparent bulk of the continuous exterior walls of a building or group of attached buildings.

BUILDING HEIGHT – A building's vertical measurement from the mean level of the ground abutting the building to a point midway between the highest and lowest points of the roof, provided that chimneys, towers, spires, towers, elevator penthouses, roof decks, tanks, mechanical equipment and similar projections of the building shall not be included in calculating the height.

BUILDING FRONTAGE – A building's horizontal measurement, parallel to the public right-of-way, of the outer wall that faces a public street.

BUSINESS SERVICES – Establishments primarily engaged in rendering services to other business establishments on a fee or contract basis, such as advertising and mailing; building maintenance; personnel and employment services;

management and consulting services; protective services; equipment rental and leasing; photo finishing, copying and printing; travel; office supply; and similar services.

BUILDING TYPE – A structure category determined by function, disposition on the lot, and configuration, including frontage and height.

BUS STOP SHELTERS or TRANSIT SHELTERS – A freestanding structure, located on a bus or train route, which is designated to accommodate embarking and disembarking transit passengers.

CARPORT - A permanent roofed structure permanently open on at least two sides, designed for or occupied by private passenger vehicles.

CISTERN – A reservoir or tank for storing rainwater and greywater for the purpose of using it for irrigation or other interior or exterior greywater uses.

CITY – The City of Woodbury, in the County of Gloucester and State of New Jersey.

CIVIC – The term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit and municipal parking.

CIVIC BUILDING – A building designed specifically for a civic function.

CIVIC SPACE – An outdoor area dedicated for public use. Civic space types are defined by the combination of certain physical constraints including the relationship between their intended use, their size, their landscaping and the building(s) defining the space.

CLUB, LODGE or FRATERNAL ORGANIZATION – Buildings and facilities, owned or operated by a corporation, association, person or persons, for a social, educational, or recreational purpose, to which membership is required for participation and not primarily operated for profit nor to render a service that is customarily carried on as a business.

COLONNADE – A roof or building structure, extending over the sidewalk, open to the street and sidewalk except for supporting columns or piers.

COMMERCIAL BUILDING – A single or multi story building devoted solely to commercial uses. They may be designed as walkups or elevator buildings depending on the number of floors. The ownership structure may be condominium or rental.

COMMERCIAL INSTRUCTIONAL ACTIVITIES – Any educational or recreational programs or activities involving team or individual instruction that is conducted in a commercial structure and related to children or adults, including, but not limited to: dance, musical instruments, singing, cooking, karate or other sports.

COMMUNITY GARDEN – A parcel of land used for the growing of vegetables, flowers, etc. used for human consumption but not for commercial sales. The garden area shall at least include but not limited to a greenhouse, an accessory storage building, benches, a watering system and fencing.

CORNICE – The top most element of a building façade composed of moldings for an entablature in formal architecture orders or used alone at the roof line or ceilings.

CORNICE LINE – A continuous horizontal cornice feature that provides a vertical break within the façade and/or defines the roof line along the top of the building.

COUNTY – The County of Gloucester in the State of New Jersey.

COURT – An open, unoccupied space bonded on more than two sides by the walls of the building. An inner court is a court entirely surrounded by the exterior walls of a building. An outer court having one side open to a street, alley, yard or other permanently open space.

CULTURAL USES – Services offered to the public by public, private or non-profit facilities, such as, but not limited to, museums, art galleries and libraries.

DECORATIVE PAVING – Paving that is made up of solid, precise, modular units, stamped concrete, scored concrete, seeded concrete, colored concrete or any combination of the above.

DIRECTIONAL SIGN – Any on-premises sign limited to directional messages that do not contain a commercial message, principally for pedestrian – or vehicular traffic, such as “one way”, “entrance” and “exit”.

DISPLAY WINDOWS – Windows on the ground floor of a building that allow visibility into and out of a commercial space and are used for display.

DOWNTOWN TRANSECT – Intended to serve as the primary shopping, professional office, medical, cultural and entertainment district of the City. This transect is envisioned to be the highest density district of the City and consist of mixed-use buildings up to five (5) stories high with active ground floor commercial uses that support a full spectrum of community needs and upper floor residential and commercial uses. Such density is essential to support fixed rail transit and day, night and weekend commercial uses. Streetscapes are intended to accommodate heavy pedestrian activity and as such are to be characterized by wide sidewalks, sidewalk cafes and high quality pedestrian amenities.

DRIVE-THROUGH FACILITY – A facility associated with a use that by design, physical facilities, service, or by packaging procedures, encourages or permits customers to receive services and/or obtain goods while remaining in their motor vehicles.

DRUG PARAPHERNALIA SHOP or HEAD SHOP – An establishment that sells equipment, products, and materials of any kind that are used, intended for use, or designed for use, in planting, propagating, cultivating, growing, harvesting, manufacturing, compounding, converting, producing, processing, preparing, testing, analyzing, packaging, repackaging, storing, containing, concealing, injecting, ingesting, inhaling, or otherwise introducing into the human body marijuana and/or a controlled substance as determined by the State of New Jersey.

DRUG REHABILITATION CLINIC or SUBSTANCE ABUSE TREATMENT FACILITY - A facility used for the purposes of temporary or long-term inpatient treatment of victims of alcohol or drug use or addiction.

EAVE HEIGHT – A building's vertical measurement from the mean level of the ground abutting the building to the lowest point of an overhanging eave.

ELDER-CARE CENTER – A building or space in a building and ground used for the daycare of senior citizens, including normal daily health-related care, i.e., medication administration, but not nursing care and services and does not preclude services like flu shots.

ELEVATOR FLATS – see Flats or Lofts, Elevator

EMERGENCY SERVICES – Publicly or privately owned safety and emergency services, such as, but not limited to, fire stations, police stations, and emergency medical and ambulance service.

ENCROACHMENT – Any structural element that breaks the plane of the vertical or horizontal regulatory limit; extending beyond a front yard setback line, rear yard setback line, or into the public right-of-way.

ENTABLATURE – Generally refers to an elaborated horizontal band along the exterior of a building used primarily just beneath the roofline, sometimes used on the façade between floors.

ENTERTAINMENT-the presentation of the performing arts, including indoor motion picture theaters, theaters for live performances, and indoor and outdoor concert halls. Entertainment uses may include restaurants as an incidental use.

EXTENDED CARE FACILITY – One or more residential structures which incorporate health care services for the long term care of the residents, including assisted living, nursing homes and rehabilitation facilities.

FAÇADE/WALL SIGN – A sign attached to the exterior façade or wall of a building in such a manner that façade or wall is the supporting structure for or forms the background surface of the sign. Such signs shall not project above the edge of the roof or beyond the side walls of a structure.

FARMERS MARKET – A permanent enclosed building typically operated year round where the majority of products consist of perishable foods and prepared foodstuffs or a temporary or occasional outdoor retail sale of farm produce from temporary stands.

FINANCIAL SERVICES - Provision of banking services to consumers or clients. Typical uses include banks, savings and loan associations, savings banks, credit unions, lending establishments, and automatic teller machines (ATMs). Drive-thru services are permitted. The location of drive-throughs shall be regulated in the same manner as parking. Financial services shall not include currency exchange or check cashing.

FIRST FLOOR ELEVATION – A building’s vertical measurement from the mean level of the ground abutting the building to the mean level of the first floor of the building.

FIRST STORY CLEAR HEIGHT – A building’s vertical measurement from the mean level of the first floor of the building to the mean level of the second floor of the building.

FLATS or LOFTS, ELEVATOR – Residential building containing two (2) or more dwelling units attached at one or more common roofs, walls or floors. Typically, the unit’s habitable area is provided on a single level with entrances provided from a common corridor. Common lobby elevators are required when more than one story above grade is provided. Flats or lofts may be contained in a residential building or may be residential flats or lofts over first floor commercial uses.

FLEX MIXED USE BUILDING – A multi story building that contains ground floor commercial uses, with upper floor residential, retail, office or other commercial uses. These buildings may be attached, semi-attached or detached. They are served by elevators and intended for the highest density areas of the City.

FORM-BASED CODE – A land use ordinance comprised of a set of cohesive Design Standard Regulations, and Development Standard Regulations incorporating the Land Use/Transect Plan, Street Regulating Plan, Public Spaces Plan, and Gateway Plan and intended to implement the goals and objectives of the Downtown Business Redevelopment Plan.

FRONT FAÇADE ENCROACHMENTS – Front façade building components that encroach into the public right-of-way, including, but not limited to, overhanging eaves, gutters, cornices, steps bay windows, balconies and balconettes.

FRONT YARD SETBACK – The required open space extending along the street line throughout the full width of the lot, exclusive of overhanging eaves, gutters, cornices, steps, bay windows, balconies and balconettes. A front yard setback applies to all yards that have street frontage, except for yards having a Small Frontage Type.

FRONTAGE PERCENTAGE – The percentage of the width of a lot that is required to be occupied by the building’s primary façade.

FRONTAGE TYPE – The type of façade that is permitted on a primary street (front yard).

FUNERAL HOME or MORTUARY – A building or part thereof used for human funeral services. Such building may contain space and facilities for (1) embalming and the performance of other services used in the preparation of the dead for burial; (2) the performance of autopsies and other surgical procedures; (3) the storage of caskets, funeral urns, and other related funeral supplies; (4) the storage of funeral vehicles; and (5) facilities for cremation.

GASOLINE SERVICE STATION or FILLING STATION - Any building, structure, or area of land used for the retail sale of automobile fuels, oils and accessories, and where repair service and sale of propane and/or kerosene, if any, is incidental.

GOVERNMENT/CIVIC/INSTITUTIONAL BUILDING – A building that contains governmental, civic, institutional or religious uses. They are typically significant buildings that may have large setbacks, monumental and unique design characteristics, public art installations and/or public space areas. Examples include courthouses, government administration offices, schools, libraries, museums and places of worship. They do not contain commercial, residential or industrial uses and are typically fully detached.

GREEN, PLAZA, SQUARE or PIAZZA – Public spaces located within the City and its neighborhoods, which may contain one or more civic building(s) or space located within a primarily unpaved, formally configured area which may contain a small public lawn or park. Portions may be linear in shape though a square, rectangle, circle or other usable shape, as appropriate, shall be the primary focus of the area. Spaces shall be surrounded by canopy street trees. These sites shall be situated at prominent locations and are often dedicated to important events and/or citizens. Spaces may contain water features, an amphitheater, farmers market, and in some appropriate instances, such as in conjunction with residential neighborhoods, may include limited play equipment and courts.

GREEN ROOF – An engineered, multi-layered roofing system sustaining the growth of plants on a roof while protecting the integrity of the underlying structure. The components of a green roof consist of a waterproofing membrane, root barrier, drainage layer, retention layer, filter fabric, growing medium and plants.

GREYWATER - Wastewater obtained from domestic sinks and tubs, but excluding that part of the plumbing waste stream that includes human wastes.

GROUND FLOOR FRONT FAÇADE FENESTRATION – The ratio of ground floor front window area to ground floor front façade area. Window area may include windows contained in doors.

HORIZONTAL FAÇADE BANDING – The use of horizontal strips that differ from the principal material, color, texture or setback of a façade, as a means of breaking up the apparent height of a building.

HORIZONTAL FAÇADE BREAK – A horizontal measurement between building face offsets; vertical elements; bay windows; private amenities organized vertically such as terraces, balconies or balconettes; building offsets or recesses; changes in façade materials or colors; or combination thereof.

HOSPITAL/MEDICAL CENTER: An institution specializing in giving clinical, temporary and emergency services of a medical or surgical nature to human patients, licensed by State law to provide facilities and services in surgery, obstetrics and general medical practice. Such institutions include allied and adjunct medical facilities such as facilities for the education and training of hospital personnel, including but not limited to residents, interns, nurses, technicians and orderlies; medical schools, student residence halls or dormitories, laboratories, research facilities, pharmacies, cafeteria, restaurant or snack bar, gift or flower shop; out-patient treatment and medical offices which may be in the same building or separate buildings provided this does not include non-accessory, non-adjacent or independently operated medical offices, group medical practices or laboratories.

HOTEL – A facility offering transient lodging accommodations to the general public and which must include a restaurant, meeting rooms, lobbies and concierge services. They may include entertainment and recreational facilities or banquet halls.

HOUSE OF WORSHIP - A building where persons regularly assemble for religious purposes and related social events and which building is maintained and controlled by a religious body organized to sustain religious ceremonies and purposes. Examples include churches, synagogues, mosques and temples.

JUDICIAL – Of or pertaining to judgment in courts of law or to the administration of justice. Includes courthouses and associated structures and uses.

KIOSK – A structure which may be constructed somewhere other than the lot on which it is placed or which is comprised of parts which are constructed elsewhere and assembled on a lot, and which is designed and intended to be used primarily for retail sale, display and accessory advertising of food and merchandise.

LABORATORY - A facility for scientific laboratory analysis of natural resources, medical and dental resources, and manufactured materials. The scientific analysis is generally performed for an outside customer, to support the work of that customer. This category includes environmental laboratories for the analysis of air, water, and soil; medical or dental laboratories for the analysis of blood, tissue, or other human medical or dental products.

LAW ENFORCEMENT – The activity of entities empowered by the City or County, which are vested with authority to bear arms and make arrests, and whose primary responsibility is the prevention and detection of crime or the enforcement of the criminal or traffic laws of the City and State.

LIBRARY – A public, nonprofit facility in which literary, musical, artistic, or reference materials such as but not limited to books, manuscripts, computers, recordings, or films are kept for use by or loaning to patrons of the facility, but are not normally offered for sale.

LIGHT INDUSTRY - Research and development activities, the manufacturing, compounding, processing, packaging, storage, assembly, and/or treatment of finished or semi-finished products from previously prepared materials, which activities are conducted wholly within an enclosed building. Finished or semi-finished products may be temporarily stored outdoors pending shipment.

LINER BUILDING – A single or multi story building devoted to commercial or residential uses. Their purpose is to “line” the exteriors of large structures such as parking garages, movie theaters or supermarkets. This allows large, blank façades to be screened by pedestrian scaled uses. The building may be attached or integrated into the larger structure, or sit in front

of it, with separations dictated by building codes. Individual unit access is to be provided from the primary street frontage. Secondary access may be provided from the building that is being “lined”.

LIVE/WORK BUILDING – A hybrid commercial-residence building that may be new construction or a conversion of an existing home into a combination ground-floor business and upper-floor single-family dwelling unit.

LIVE-WORK TRANSECT – Intended to serve as a primarily urban residential district with accommodation for some small scale neighborhood commercial functions. The transect is generally located at the periphery of the Downtown Transect and is purposed to act as a transition to lower density residential neighborhoods. It is envisioned to be a mid to high density area of the City and consist primarily of attached residential building types with some ground floor neighborhood commercial functions. Streetscapes are intended to accommodate moderate pedestrian activity.

LIVING FENCE – Hedges, trees, or other living landscaping that forms a border between two spaces.

MARQUEE SIGN – A sign attached to a marquee for the purpose of identifying a theater or movie house. Such signs shall not project above the edge of the roof or beyond the side walls of a structure.

MASSAGE PARLOR – An establishment, business or use which provides the service of massage and body manipulations, including exercise, heat and light treatments of the body and all forms and methods of physical therapy unless operated by a medical practitioner or professional physical therapist or certified massage practitioner.

MASSING – The manner in which a building’s height, depth, and width are proportionate to the width of the street on which it is located.

MENU BOARD SIGN – A permanently mounted wall sign used to inform the public of the list of entrees, dishes, foods and entertainment available in a restaurant, café, luncheonette, delicatessen or similar establishment. Such signs shall be located in a permanently mounted display box on the façade of the building adjacent to the entrance, displayed within a window adjacent to the entrance, or at a podium that will be placed inside the restaurant upon closing.

MIXED-USE – A building that includes more than one use; typically having different uses on the first floor and the upper floors of the building. A common example is ground story commercial with offices or residences on upper floors.

MOTOR VEHICLE SALES - Any business establishment that sells or leases new or used automobiles, trucks, vans, trailers, recreational vehicles, boats, or motorcycles or other similar motorized transportation vehicles. Such an establishment may maintain an inventory of the vehicles for sale or lease either on-site or at a nearby location and may provide on-site facilities for the repair and service of the vehicles sold or leased by the establishment.

MOTOR VEHICLE SERVICE AND REPAIR - The repair and maintenance of automobiles, motorcycles, trucks, trailers, or similar vehicles including but not limited to body, fender, muffler, or upholstery work, oil change and lubrication, painting, tire service and sales, washing, or installation of CB radios, car alarms or stereo equipment.

MUSEUM/GALLERY – A building having public significance by reason of its architecture or former use or occupancy or a building serving as a repository for a collection of natural, scientific, or literary curiosities or objects of interest, or works of art, and arranged, intended, and designed to be used by members of the public for viewing, with or without an admission charge, and which may include as an incidental use the sale of goods to the public as gifts or for their own use.

NEIGHBORHOOD TRANSECT – Intended to serve as a low density residential district with some small scale neighborhood commercial functions and also act as a transitional zone between mid to high density uses and existing neighborhoods on the City's periphery. This transect is envisioned to be a low to mid density district of the City and consist primarily of single family dwellings and semi-detached residential building types with some ground floor and auto-oriented neighborhood commercial functions. Streetscapes are intended to accommodate low to moderate pedestrian activity.

OFFICE – A room, suite of rooms, or building in which a person transacts the affairs of a business, profession, service, industry, or government.

OFFSET – A change in elevation or primary façade plane with a difference of at least 3 feet.

PARK - A noncommercial, not-for profit facility designed to serve the recreation needs of the residents of the community. Such facilities include neighborhood parks, community parks, regional parks, and special use facilities, all as described in the recreation and open space element of the City Master Plan. Such facilities may also include but shall not be limited to school and religious institution ball fields, football fields, and soccer fields, if they meet the above definition. Commercial recreation or amusement facilities shall not be considered parks.

PARKING BAY – Paved area used to accommodate either one (1) or two (2) rows of parking stalls plus one access lane.

PARKING STRUCTURE – A building or part thereof which is designed specifically for automobile parking and where there are a number of floors or levels on which parking takes place. Parking structures are to be located internal to a block and not permitted on any street frontages (with the exception of a Small Frontage Type) without liner buildings

PAWN SHOP - An establishment that engages, in whole or in part, in the business of loaning money on the security of pledges of personal property, or deposits or conditional sales of personal property, or the purchase or sale of personal property.

PEDESTRIAN REALM – The area of the public right-of-way intended for pedestrian movement; the area between the face of the curb and the nearest property line, and may extend beyond the public right-of-way into private property.

PEDESTRIAN REALM WIDTH – A horizontal measurement perpendicular to the street between the street curb and the edge of paved surface, which includes the sidewalk, planting strip and other pedestrian areas, and may extend beyond the public right-of-way into private property.

PEDESTIAN TAKE OUT WINDOW – A window opening in the façade of a commercial building used for the sale of food and/or beverages. Such a window shall be designed for the exclusive use of pedestrians, and shall be located to provide a safe waiting area, and not disrupt or impede pedestrian movement on a sidewalk used by the public.

PHARMACY – An establishment engaged in the retail sale of prescription drugs, nonprescription medicines, cosmetics, and related supplies.

PLANTING STRIP – A portion of public right-of-way located between the curb and sidewalk which is dedicated to the planting of trees and low ground cover.

PORTABLE SIGN – A sign that is not permanently affixed to a structure or the ground. Also known as A-Frame or Sandwich-Board signs. These signs are constructed with two faces which rest at an angle less than 45 degrees to each other. Such signs are only permitted for retail stores, restaurants, cafés, luncheonettes, delicatessens and similar establishments. Such signs must be located upon the lot is services. The sign shall not obstruct pedestrian or vehicular flow.

PORTICO – A covered walk or porch that is supported by columns or pillars; also known as colonnade.

PRINCIPAL STORE FRONTAGE – Any building face that includes a public/customer entrance (not emergency, service or employee entrances) to the store or establishment to which a sign is related.

PRIVATE GARAGE – An accessory building used for the storage of motor vehicles.

PROJECTING/HANGING SIGN – A sign attached to and projecting out from the exterior façade or wall of a building at a 90 degree angle in such a manner that it is located completely in the public right-of-way, partially in the right-of-way or completely on private property. Internal illumination of a projecting sign is prohibited.

PUBLIC FRONTAGE TYPE – same as FRONTAGE TYPE.

PUBLIC SPACE – Property (streets, alleys, civic greens and parks) within the public domain and physically within a city or neighborhood within which citizens may exercise their rights. At its most ideal level, public space and public buildings can be characterized as being of, for and by the People.

PUBLIC SPACES PLAN – Both a master plan element and a land use ordinance that regulates location and general design of public spaces.

PUBLIC WORKS - Any improvement facility or service, together with its associated public site or right-of-way necessary to provide transportation, drainage, public or private utilities, energy, or similar essential services.

RAIL STATION - A facility, either light or heavy rail, for the boarding of passengers and related ticketing sales and offices.

RAIN GARDEN – A planted depression that is designated to take as much as possible of the excess rainwater run-off from a house or other building and its associated landscape.

REAR ALLEY – A vehicular street or driveway located to the rear of lots providing access to service areas and parking, and containing utility easements. Alleys should be paved from building face to building face, with drainage by inverted crown at the center or with curbs at the edges.

REARYARD SETBACK – The required open space extending along the rear line of a lot from side yard to side yard of a building, exclusive of overhanging eaves, gutters, cornices, steps, bay windows, balconies and balconettes. A rear yard setback is not applicable to a yard that fronts on a public street, except for yards having a Small Frontage Type.

REGULATING PLAN – A Zoning Map that shows the location of various Transects.

RESEARCH AND DEVELOPMENT - Investigative activities conducted by a business with the intention of making a discovery that can either lead to the development of new products or procedures, or to the improvement of existing products or procedures.

RECESS – A change in the building façade plane that sits at least 3 feet back from the beginning of the primary building setback.

RECREATION - The refreshment of body and mind through forms of play, amusement, or relaxation. The recreational experience may be active, such as, but not limited to boating, fishing, and swimming, or may be passive, such as, but not limited to, enjoying nature or its wildlife.

RECREATIONAL FACILITY - An indoor facility, with or without seating for spectators, and providing accommodations for a variety of individual or organized, including but not limited to basketball, ice hockey, wrestling, soccer, tennis, volleyball, racquetball, or handball. Such facility may also provide other regular organized events, health and fitness club facilities, swimming pool, snack bar and other support facilities.

REGULATING PLAN – Both a master plan element and a land use ordinance that regulates land use, public streets, and public spaces

RESTAURANT – An establishment where food and drink are prepared, served, and primarily consumed within the principal building or structure. Restaurants may be full service/sit down, carry out or fast food. Drive-thru services are permitted. The location of drive-throughs shall be regulated in the same manner as parking.

RETAIL SALES – Establishments engaged in selling goods or merchandise to the general public for personal or household consumption, which render services incidental to the sale of such goods, and are engaged in activity designed to attract the general public to purchase such goods or merchandise. Pharmacies are permitted to have drive-thru services. The location of drive-throughs shall be regulated in the same manner as parking.

RETAIL SERVICES – Establishments providing services or entertainment, as opposed to products, to the general public, including eating and drinking places, hotels, finance, insurance, real estate, personal services, motion pictures, amusement and recreation establishments, health, education and social services, museums, galleries and similar uses.

ROOFLINE OFFSET – A horizontal measurement between offsets in portions of a roofline, as a means of breaking up the apparent bulk of the continuous roofline of a building or group of attached buildings. Such offsets consist of elements such as dormers and gables.

ROOMING UNIT – A room or group of rooms forming a single habitable unit used or intended to be used for living and sleeping and having a private bathroom or the use of a common bathroom with other rooming units in the same building within the meaning of this chapter, but such rooming units shall not be used for cooking or eating purposes.

SCHOOL - An institution for the teaching of children or adults including primary and secondary schools, colleges, professional schools, dance schools, business schools, trade schools, art schools, and similar facilities.

SELF-STORAGE FACILITY or MINI-WAREHOUSE FACILITY - A building or group of buildings divided into separate compartments used

to meet the temporary storage needs of customers and may include refrigerated facilities.

SHARED PARKING – Joint utilization of a parking area for more than one use, either on-site or between nearby properties through a dedicated arrangement, in order to fulfill their individual parking requirements because their prime operational hours do not overlap.

SIDE FAÇADE FENESTRATION – The ratio of side window area to side façade area. Window area may include windows contained in doors.

SIDE FAÇADE WINDOW SETBACK – A horizontal measurement between all windows located on the side façade of a building to the shared side lot line. This setback is to be measured perpendicularly from the shared side lot line.

SIDEWALK CAFÉ – Any eating establishment where food and other refreshments are served upon the public right-of-way, namely the sidewalks immediately in front of any restaurant, café, cafeteria or place of business where food and/or other refreshments are served, or where permitted on private property.

SIDE YARD SETBACK – The required open space extending along the side line of a lot from the front yard to the rear yard of a building, exclusive of overhanging eaves, gutters, cornices, steps, bay windows, balconies and balconettes. A side yard setback is not applicable to a yard that fronts on a public street, except for yards having a Small Frontage Type.

SINGLE-FAMILY DETACHED BUILDING - A detached single family unit on its own individual lot designed for or used exclusively by one family.

SOLAR SCREEN – A device attached to a building to provide shading for glazed areas thereof.

SPECIAL CARE FACILITY – Includes assisted living facilities, convalescent care, nursing homes, extended care facilities and life care facilities.

STATE – The State of New Jersey.

STORAGE SHED – A subordinate structure or building used primarily for storage purposes.

STOREFRONT – A ground floor façade with a large display window.

STREET FURNITURE – Functional elements of the streetscape, including but not limited to benches, trash receptacles, planters, telephone booths, bus shelters, kiosks, sign posts, street lights and bollards.

STREET REGULATING PLAN – Both a master plan element and a land use ordinance that regulates location and general design of public streets and public spaces

STREET VENDORS – Any person or persons, including corporations, who engage in the activity of vending.

STREETSCAPE – Design elements in the Public Right-of-Way, primarily located in the Pedestrian Realm including street furniture, sidewalks, tree pits, grass verges, landscaping, lighting, and signage.

SURFACE PARKING LOT - An off-street, surfaced, ground level open area, for the temporary storage of motor vehicles.

TATTOO PARLOR - An establishment whose principal business activity, either in terms of operation or as held out to the public, is the practice of one or more of the following: (1) placing of designs, letters, figures, symbols, or other marks upon or under the skin of any person, using ink or other substances that result in the permanent coloration of the skin by means of the use of needles or other instruments designed to contact or puncture the skin; (2) creation of an opening in the body of a person for the purpose of inserting jewelry or other decoration.

TENANT IDENTIFICATION SIGN – An exterior wall sign that serves as a common or collective identification for two or more tenant in the same building, displaying the name of each tenant.

THEATER - A structure used for dramatic, operatic, motion pictures, or other performance, for admission to which entrance money is received. Adult motion picture theaters or establishments featuring burlesque are prohibited.

TOWNHOUSE BUILDING - A one-family dwelling unit with a private entrance, attached horizontally with other dwelling units in a

linear arrangement. They are located on individual lots and are separated from other dwelling units by an approved wall, extending from the foundation through the roof that is structurally independent of the corresponding wall of the adjoining unit. They have a totally exposed front and rear wall to be used for access, light and ventilation. A variation of the Townhouse is the Stacked Townhouse, with two, similarly sized one-family dwelling units integrated into the footprint of a Townhouse.

TRANSECT – One of several areas on the Zoning Map regulated by the Smart Code. Transect Zones are administratively similar to the land use zones in conventional codes, except that in addition to the usual building use, density, height, and setback requirements, other elements of the intended habitat are integrated, including those of the private lot and building and public frontage.

TRANSPARENCY – The degree, measured as a percentage of the overall façade space, to which a façade has clear, transparent windows on each story.

TWIN BUILDING - A one-family dwelling unit with a private entrance, attached horizontally with one other dwelling unit in a linear arrangement. They are located on individual lots and are separating from the other dwelling unit by an approved wall, extending from the foundation through the roof that is structurally independent of the corresponding wall of the adjoining unit. They have a totally exposed front, rear and side wall to be used for access, light and ventilation.

UPPER FLOOR FRONT FAÇADE FENESTRATION – The ratio of upper floor front window area to upper floor front façade area. Window area may include windows contained in doors providing access to terraces, balconies or balconettes.

VERTICAL FAÇADE BREAK – A vertical measurement between horizontal elements such as horizontal façade banding; private amenities oriented horizontally such as terraces, balconies, balconettes and porches; changes in façade materials or colors; overhanging eaves; or combination thereof. Outdoor terraces, balconies and porches shall have a minimum depth of four (4) feet and outdoor balconettes shall have a maximum depth of two (2) feet.

VETERENARIAN SERVICE – An establishment that provides animals with medical and surgical treatment, as well as care during the time of such treatment.

WINDOW SIGN – A sign that is applied or attached to the interior of a window or located in such a manner within a building that it can be seen from the exterior of the building through a window. Such signs shall be limited to individual letters placed on the interior surface of the window and intended to be viewed from outside. Glass-mounted graphic logos may be applied by silk screening or pre-spaced vinyl die-cut forms.

WINDOW-TO-EAVE OFFSET – A vertical measurement from the highest point of a façade window to the lowest point of an overhanging eave or other façade protrusion such as a balcony or balconette.

WIRELESS TELECOMMUNICATION TOWER – A tower, pole, or similar structure that supports a wireless telecommunications antenna operated for commercial purpose above ground in a fixed location, freestanding, guyed, or on a building or other structures.

7.0 | RELATIONSHIP TO EXISTING PLANS & REGULATIONS

7.1 Relationship to Local Objectives

The *Redevelopment Plan* and the public engagement process utilized to prepare the Plan facilitates most if not all of the goals and objectives stated in the Comprehensive Master Plan for the City of Woodbury (2005). Master Plan goals and objectives are indicated below.

A. Master Plan Goals:

1. Preserve and protect the character of established residential neighborhoods.
2. Preserve and protect the natural heritage, both environmental and cultural.
3. Balance new development and ratable with the needs of the community.
4. Continue the dialogue among the citizens and businesses regarding the needs and concern of neighborhoods and the business community.
5. Preserve and improve open space systems.
6. Encourage economic and employment growth specifically in the central business district.
7. Preserve and protect the historic character of the downtown and surrounding areas.

8. Maintain, enhance and further develop a strong “sense of place” in the downtown distinctive to Woodbury.
9. Revise the land use ordinances to insure compatibility with new growth demands for housing, commercial and industrial uses within the community.

B. Land Use Element Goals & Objectives

1. Establish a design criterion and performance standards that enhance and beautify the downtown district.
2. Continue to maintain an open dialogue with and involve residents regarding land use issues in their neighborhood.

C. Circulation Element Goals & Objectives

1. Work with the New Jersey Department of Transportation, the Delaware Valley Regional Planning Commission, and the Gloucester County Planning and Highway Departments to conform to Woodbury’s goals.
2. Reinforce a pedestrian friendly community throughout.
3. Provide adequate parking for downtown.
4. Promote bike friendly environment.
5. Increase capacity at traffic intersections on Broad Street and limit thru traffic to one lane in each direction on Broad Street from Red Bank to Barber.
6. Encourage enforcement of pedestrian safety in signed and marked pedestrian zones and crosswalks and recommend the use of contrasting material and texture for sidewalks and crosswalks within the downtown area.
7. Encourage rail passenger service to come to Woodbury.

D. Parks, Recreation & Open Space Element Goals & Objectives

1. Maintain parks and recreational facilities to meet neighborhood and community needs.
2. Improve and expand cultural, non-athletic, and recreational opportunities for citizens of all ages within the community.
3. Create linkages between existing parks and public recreation areas.
4. Develop a trail system that connects residential neighborhoods to parks, public open space, and commercial centers. Easements through private land should be obtained, when necessary, to provide public access to open space.
5. Preserve and enhance the tree-lined character of residential streets.

E. Housing & Population Element Goals & Objectives

1. Preserve and enhance existing neighborhoods through rehabilitation of any substandard housing units and the encouragement of appropriate infill development where permitted by zoning. Rehabilitation includes both major and minor housing repairs.
2. Continue to design and implement zoning that promotes opportunities for young professionals.
3. Balance housing needs with other land use goals and policies as defined in the master plan.
4. Continue to support home ownership.
5. Encourage new urbanism and neo-traditional developments that provide a variety of housing opportunities.
6. Establish design criteria for residential redevelopment within existing neighborhoods.

F. Community Facilities Element Goals & Objectives

1. Explore the expansion or relocation and expansion of the Police Building.

G. Economic Element Goals & Objectives

1. Expand and diversify the economic profile of the City, reestablishing Woodbury as an important economic center in the region.
2. Provide a business friendly environment for existing businesses, which have invested in the City and provide a support for those wishing to upgrade or renovate.
3. Encourage redevelopment and full occupancy in existing commercial locations.
4. Encourage dialogue with property owners of downtown businesses regarding opportunities within the community.
5. Promote redevelopment and offer business incentives.

H. Historic Preservation Element Goals & Objectives

1. Encourage development patterns adjacent to existing historic structures that complement the character of the historic structures.
2. Encourage parking design for historic districts that is unobtrusive, minimizing the effect on the historic character of the setting. Parking should be limited to that necessary to serve the proposed use.

There are several additional Master Plan goals and objectives which are not directly implemented by the *Redevelopment Plan*. However, it is not because the *Redevelopment Plan* is in conflict with them. Instead, they are either not directly advanced by the *Redevelopment Plan* and/or are not pertinent to the Plan.

7.2 Relationship to Local Zoning

The *Redevelopment Plan*, which includes implementing ordinances, will supersede the underlying zoning ordinances and development regulations for the portion of the City subject to the *Redevelopment Plan*.

7.3 Relationship to Plans of Contiguous Municipalities, County & State

The northern boundary of the Woodbury Redevelopment area directly abuts neighboring Deptford Township to the east and West Deptford Township to the west. A statement of consistency with the plans of these neighboring municipalities is provided below. This is followed by a statement of consistency with plans of the County (Gloucester), regional entities (Delaware Valley Regional Planning Commission) and State (New Jersey State Planning Commission).

7.3.1 Deptford Township

The *Redevelopment Plan* reinforces adjacent neighborhoods in Deptford Township by providing for goods and services and enhanced park systems of regional benefit. The focus of redevelopment along the northern portion of Broad Street terminates several blocks south of the Deptford Township municipal border, before transitioning to neighborhood oriented commercial uses in Deptford, by Park Avenue. This area, zoned Neighborhood Commercial is intended to provide commercial services to nearby residential areas. Redevelopment in Woodbury will complement these neighborhood commercial uses in Deptford with similar uses. In addition, it will provide regional employment, goods and services to nearby residents.

The *2007 Master Plan* designates a large area adjacent to Woodbury as Open Space on its Land Use Plan Map. Open Space as defined in the *2007 Master Plan* includes land to be acquired or restricted for conservation and recreation and lands already preserved by the Township. The *Redevelopment Plan* supports open space and recreation improvements within a regional greenway system that can ultimately connect to Deptford Township Open Space areas. The *Redevelopment Plan* calls for improvements to Lake Park and establishment of a multi-purpose path that will connect Lake Park to parks and open spaces in locations to the east and west. This regional greenway system connects Lake Park to Stewart Lake Park,

which straddles the border of Deptford Township. The *Deptford Township Master Plan* shows a connection from Stewart Lake Park to Whispering Lakes on its *Recreation & Open Space Plan Map*.

7.3.2 *West Deptford Township*

The area of the *Redevelopment Plan* coterminous with West Deptford Township is at its northern reaches, above Progress Street. Here, West Deptford has zoned the Broad Street Corridor for commercial uses (Commercial Zoning District) and adjacent to that is a residentially zoned neighborhood. The Commercial District permits a variety of retail, office, restaurant and services, as well as residential uses and mixed single family residential and business uses. These Districts will be reinforced by downtown redevelopment in Woodbury, by making additional commercial facilities easily accessible to nearby residential neighborhoods and providing for bulk standards that appropriately transition to existing conditions in the adjacent part of West Deptford Township. Furthermore, redevelopment will support the Township Master Plan objective that seeks "To protect and enhance the visual environment by requiring new uses to comply with design, location and environmental standards which will ensure a pattern of quality development, and which will protect areas and buildings of historical and cultural significance."

7.3.3 *Gloucester County Development Management Plan (1982)*

The *Redevelopment Plan* is consistent with the goals and vision of the *Gloucester County Development Management Plan*. The County Plan seeks to encourage continued growth of a viable, diversified economic base; maintain and enhance existing urban areas and encourage a growth pattern that concentrates rather than disperses development; and provide adequate housing opportunities for all portions of the county's population.

Woodbury falls within an area designated as existing development according to the *County Development Management Plan Map*. The County Plan states that existing development areas should be able to accommodate future growth in the form of rehabilitation and infill development.

7.3.4 *Gloucester County Northeast Region Strategic Plan (2005)*

The *Gloucester County Northeast Region Strategic Plan* offers a regional growth strategy addressing towns, highway corridors, subdivisions and open space for the 14 municipalities in the northeastern portion of the county, including Woodbury City. Woodbury City is classified as a Town, consistent with the Delaware Valley Regional Planning Commission designation in its long range plan (see *Connections* below). Goals in the County's *Strategic Plan* which are applicable to "Towns" include supporting vibrant, fully productive, compact and walkable, mixed-use town centers, and stabilizing, maintaining, and expanding the neighborhoods of older towns and boroughs. There are numerous strategies for implementing town planning articulated in the *Strategic Plan* that the *Redevelopment Plan* seeks to achieve. These include:

- A. Create land use and building requirements specific to Redevelopment Areas that are sensitive to the adjoining neighborhoods and ensure that new and renovated residential and institutional development is compatible with the existing neighborhood character.
- B. Assemble parcels of land of sufficient size and dimension – which can be small or large, depending on the need or the type of town – to enable an orderly arrangement of new land uses that will complement efforts to revitalize core business districts.
- C. Identify and promote infill development opportunities. Evaluate land development ordinances to ensure that the traditional town scale and character can be replicated on infill sites.
- D. Zone for a mix of retail uses that will complement the uses targeted for the surrounding corridor nodes and centers.
- E. Zone for densities that will support mass transit alternatives and concentrations of cultural, entertainment and shopping opportunities.
- F. Incorporate high quality design standards for buildings, parking areas, and landscapes into land development ordinances.

- G. Provide a mixture of land uses in the downtown areas.
- H. Provide a broad range of housing opportunities, in the neighborhoods and as upper floor uses in the downtowns.
- I. Provide a pedestrian oriented environment in both the downtown and neighborhood districts.
- J. Provide open space and civic spaces in the core and in the neighborhoods.
- K. Provide for improved pedestrian and bicycle circulation in and around the core areas. Make downtown more walkable between shops and from adjoining neighborhoods. Complete any missing links in the pedestrian circulation system.
- L. Promote transit oriented development districts near transportation nodes such as train stations and bus depots by encouraging a mixture of retail, service, entertainment and residential uses, and discouraging surface parking lots, auto related service and repair uses and big-box retail.
- M. Institute traffic calming measures in downtown commercial districts.

7.3.5 *Connections 2035 – The Regional Plan for a Sustainable Future*

Woodbury City is located within the nine-county Greater Philadelphia region of the Delaware Valley Regional Planning Commission (DVRPC). DVRPC is a federally designated Metropolitan Planning Organization that is required to prepare a long range transportation plan. *Connections*, the long range plan of the Delaware Valley Regional Planning Commission, addresses transportation issues but goes beyond as well to cover land use, the environment and the economy.

Connections is founded upon a number of land use and transportation principles, goals and policies. Among them is the goal to invest in “Centers.” Centers are compact settlements that provide the basis for organizing new development, revitalization, infill and adaptive reuse. The Plan establishes a hierarchy of Centers. Woodbury is identified as a Town Center. A Town Center has a high residential density and integrated

mix of uses. It has a unique sense of place, including a distinct downtown or main street, is pedestrian friendly and transit oriented.

Center policies contained in *Connections* that are supported by the *Redevelopment Plan* include the following:

- A. Attract new residents and jobs to the region's cities and centers.
- B. Restore and maintain the existing infrastructure in identified centers.
- C. Target infrastructure expansions to manage growth, curtail sprawl, and encourage a more sustainable, center-based regional development pattern.
- D. Redevelop abandoned and underutilized brownfield and greyfield sites into thriving mixed-use areas.
- E. Revitalize neighborhoods through economic development activities (such as Main Street programs), housing rehabilitation and maintenance programs, and activities to improve the pedestrian environment (including streetscape and lighting improvements.)

Associated community design policies that are supported by the *Redevelopment Plan* are as follows:

- A. Enhance the design quality of new development and redevelopment, such that it is more sensitive to its surroundings, community character, and thematic landscapes, through the implementation of municipal smart growth tools.
- B. New development should incorporate a mix of uses, new urbanism principles, pedestrian and bicycle friendliness, and, where appropriate, transit-oriented design.
- C. Promote and market the amenities and unique qualities that make the region's developed municipalities and neighborhoods great places to live, work, and play.

7.3.6 *New Jersey State Development and Redevelopment Plan (2001 State Plan)*

The entirety of Woodbury falls within Planning Area 1 (Metropolitan Planning Area) as delineated on the State Plan Policy Map. Local planning within Planning Area 1 is guided by a series of State Plan Policy Objectives. Those restated below are advanced by the *Redevelopment Plan*.

- A. Land Use.** Promote redevelopment and development in Cores and neighborhoods of Centers and in Nodes that have been identified through cooperative regional planning efforts. Promote diversification of land uses, including housing where appropriate, in single-use developments and enhance their linkages to the rest of the community. Ensure efficient and beneficial utilization of scarce land resources throughout the Planning Area to strengthen its existing diversified and compact nature.
- B. Housing.** Provide a full range of housing choices through redevelopment, new construction, rehabilitation, adaptive reuse of nonresidential buildings, and the introduction of new housing into appropriate nonresidential settings. Preserve the existing housing stock through maintenance, rehabilitation and flexible regulation.
- C. Economic Development.** Promote economic development by encouraging strategic land assembly, site preparation and infill development, public/private partnerships and infrastructure improvements that support an identified role for the community within the regional marketplace...Encourage private sector investment through supportive government regulations, policies and programs, including tax politics and expedited review of proposals that support appropriate redevelopment.
- D. Transportation.** Maintain and enhance a transportation system that capitalizes on high-density settlement patterns by encouraging the use of public transit systems, walking and alternative modes of transportation to reduce automobile dependency, link Centers and Nodes, and create opportunities for transit oriented redevelopment. Facilitate efficient goods movement through strategic investments and intermodal linkages

- E. **Redevelopment.** Encourage redevelopment at intensities sufficient to support transit, a broad range of uses and efficient use of infrastructure. Promote design that enhances public safety, encourages pedestrian activity and reduces dependency on the automobile.
- F. **Recreation.** Provide maximum active and passive recreational opportunities and facilities at the neighborhood, local and regional levels by concentrating on the maintenance and rehabilitation of existing parks and open space while expanding and linking the system through redevelopment and reclamation projects.

APPENDIX A | REDEVELOPMENT AREA TAX BLOCKS AND LOTS

The Redevelopment Area is comprised of all of the parcels determined to be “in need of redevelopment” per the Redevelopment Study, and subsequent revisions. This District consists of 934 parcels. The parcels were originally chosen for their proximity to and ability to contribute to the revitalization of the Broad Street Corridor. The block and lot numbers included are as follows:

| <u>Block</u> | <u>Lot</u> |
|--------------|---|
| 58: | 1, 3, 4, 5, 6, 6.01, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17 |
| 59: | 1, 2, 3, 4, 4.01, 5, 6, 6.01, 8 |
| 61: | 1, 2, 3, 4, 5, 6 |
| 64: | 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 10.01, 12, 13, 14, 14.01, 15, 16, 17, 18, 19, 19.01, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 29.01, 30 |
| 68: | 1, 1.01, 2, 2.01, 2.02, 2.03, 2.04, 3, 3.01, 4, 5, 5.01, 5.02, 6, 7, 8, 9, 10, 10.01, 12 |
| 74: | 3, 4, 6, 7, 8, 9, 10, 11, 12, 13, 16, 17, 18, 19, 20, 21, 22, 23, 24, 24.01, 25, 25.01, 26, 26.01, 27 |
| 75: | 1, 1.01, 2, 2.01, 3, 3.01, 3.02, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 29.01, 31, 32, 33, 35, 36, 37, 37.01, 38, 38.01, 38.02 |
| 76: | 1, 2, 2.01, 2.02, 2.03, 3, 3.01, 4, 4.01, 5, 6, 6.01, 7, 8, 9 |
| 76.01: | 1, 1.01, 2, 3, 4, 5, 6, 7, 8, 9 |
| 77: | 1, 2, 3, 4, 7, 8, 9, 9.01, 10, 11, 12, 12.01, 14, 16, 16.01, 16.02, 16.03, 17, 18, 19, 20, 5 |
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| 79: | 1 |

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- 89: 1, 2, 3, 3.01, 4, 5, 6, 7, 8, 9, 11, 12, 13, 14, 15, 15.01, 16, 16.01, 17, 18, 21, 22, 23, 24, 25, 26, 26.01, 27
- 90: 15, 16, 17, 17.01, 18, 19, 20
- 90.01: 13, 14, 14.01, 15, 16, 17, 18, 19
- 91: 1, 2, 3, 4, 5, 6, 39
- 91.01: 16, 17, 18, 19, 19.01, 20, 21, 22, 23, 24, 24.01, 24.02, 25, 26, 46
- 91.02: 1, 2, 3, 4, 4.01, 5, 6, 18, 36, 37
- 106: 1, 2, 3, 4, 4.01, 5, 6, 7, 8, 9, 10, 11, 12, 12.01, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24
- 107: 1, 2, 3, 4, 5, 6, 6.01, 7, 8, 9, 9.01, 10, 11, 13, 13.01, 14, 15, 16, 17
- 108: 1, 1.01, 1.02, 1.03, 2, 3, 3.01, 4, 5, 5.01, 6, 7, 7.01, 8, 8.01, 8.02, 8.03, 9, 10, 11, 12, 12.01, 13, 13.01, 14.01, 14.05, 14.06, 14.07, 14.08, 14.09, 14.10, 14.11, 14.12, 14.13, 14.14, 15, 16
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- 110: 1, 2, 3, 4, 5, 6, 7, 8, 9, 9.01, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22
- 111: 1, 2, 3, 4, 5, 5.01, 6, 7, 8.01
- 112: 1
- 113: 1, 2, 2.01, 3, 3.01, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 16.01, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 27.01, 27.02, 28, 31, 32
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- 115: 1, 1.01, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 13, 14, 15, 16, 17, 18, 19, 20, 21, 21.01, 22, 22.01, 22.02, 23, 23.01, 24, 25, 26, 27, 28, 30, 31, 32, 33, 33.01, 34, 35
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179: 1, 1.01, 1.02, 1.03, 1.04, 1.05, 1.06, 1.07, 2, 3

APPENDIX B | ACQUISITION LISTS

The following list pertains to Section 5.9, Property Acquisition:

TO BE ACQUIRED

| <u>Block</u> | <u>Lot</u> |
|--------------|--|
| 75: | 3.01, 4, 5, 7, 15, 27, 28, 33, 36, 37, 37.01, 38, 38.01, 38.02 |
| 77: | 3, 4, 9, 9.01, 10, 12, 12.01 |
| 113: | 3.01, 17, 18 |
| 114: | 2, 4 |
| 115: | 1.01, 2, 3, 16, 35 |
| 118: | 16.01, 17, 18, 31, 32, 33 |
| 119: | 2, 3, 3.01, 4, 5, 5.01, 5.02, 7, 7.01, 7.02, 8, 8.01, 9, 10, 11, 12, 13, 13.01, 14, 25, 26, 27 |
| 122: | 3, 3.01, 3.02, 7 |

MAY BE ACQUIRED

| <u>Block</u> | <u>Lot</u> |
|--------------|---|
| 58: | 1, 3, 4, 5, 6, 6.01, 7, 10, 11, 13, 14, 15, |
| 59: | 1, 2, 3, 4, 4.01, 5, 6, 6.01, 8 |
| 61: | 1, 2, 3, 4, 5, 6 |
| 64: | 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 10.01, 12, 13, 14, 14.01, 15, 16, 17, 18, 19, 19.01, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 29.01, 30 |
| 68: | 1, 1.01, 2, 2.01, 2.02, 2.03, 2.04, 3, 3.01, 4, 5, 5.01, 5.02, 6, 7, 8, 9, 10, 10.01, 12 |
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117: 1, 2, 3, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 20, 22, 23, 23.01, 24, 25, 26, 26.01, 26.02

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128: 1, 2, 2.01, 2.02, 2.03, 5

129: 1, 2, 3, 3.01, 3.02

142: 9

178: 14, 15, 16, 17, 17.01, 17.02, 18, 19

179: 1, 1.01, 1.02, 1.03, 1.04, 1.05, 1.06, 1.07, 2, 3

NOT TO BE ACQUIRED

| <u>Block</u> | <u>Lot</u> |
|--------------|--|
| 58: | 8, 9, 12, 16, 17 |
| 76.01: | 9 |
| 78: | 5.01 |
| 83: | 8, 9, 10, 11, 12, 13, 13.01, 14.01, 14.02, 14.03 |
| 108: | 8.01, 8.03 |
| 109: | 1 |
| 118: | 1, 2, 2.01, 3, 4, 4.01, 5.01, 13, 14, 15 |
| 120: | 21 |
| 126: | 10 |
| 127: | 5 |

APPENDIX C | COAH PROPERTIES

The properties listed in the table on the following page are COAH units located in the Redevelopment Area. For more information on obligations and relocations pertaining to the plan, see Section 5.10.

| Block | Lot | Address | Type | Credits |
|-------|----------|---------------------|------------------|---------|
| 58 | 3 | 41 Penn St. | Rehab | 1 |
| 58 | 5 | 416 Glover St. | Rehab | 1 |
| 64 | 6 | 41 High St. | Rehab | 1 |
| 64 | 26 | 14 West St. | Rehab | 1 |
| 68 | 7 | 332 Lincoln | Rehab | 1 |
| 75 | 1.01 | 142 Lupton St. | Rehab | 1 |
| 75 | 19 | 128 Lupton Ave. | Rehab | 1 |
| 76.01 | 5 | 36 Lupton Ave. | Rehab | 1 |
| 77 | 1 | 26 W. Centre St. | Group Home | 3 |
| 77 | 9 | 59 S. Broad St. | Group Home | 2 |
| 78 | 2 | 22 Harrison St. | Rehab | 1 |
| 83 | 26/26.01 | 151-153 N Broad St. | Group Home | 10 |
| 106 | 18 | 16 Carpenter St. | Rehab | 1 |
| 108 | 9 | 245 S. Barber Ave. | Rehab | 1 |
| 109 | 8 | 75 E. Barber Ave. | Rehab | 1 |
| 110 | 2 | 166 Franklin | Rehab | 1 |
| 113 | 6 | 45 E. Centre St. | Rehab | 1 |
| 114 | 5 | 25 Aberdeen Place | Rehab | 1 |
| 114 | 12 | 53 Aberdeen Place | Rehab | 1 |
| 114 | 22 | 28 Hopkins St. | Rehab | 1 |
| 114 | 29 | 46 Hopkins St. | Rehab | 1 |
| 114 | 30 | 50 Hopkins Ave. | Rehab | 1 |
| 114 | 31 | 54 Hopkins St. | Rehab | 1 |
| 115 | 10 | 37 Hopkins St. | New Construction | 1 |
| 115 | 22 | 26 E. Barber Ave. | New Construction | 1 |
| 115 | 22.02 | 28 E. Barber Ave. | New Construction | 1 |
| 115 | 23 | 30 E. Barber Ave. | New Construction | 1 |
| 115 | 24 | 145 Franklin St. | Rehab | 1 |
| 115 | 27 | 153 Franklin St. | Rehab | 1 |
| 115 | 33 | 44 E. Barber Ave. | Rehab | 1 |
| 116 | 4 | 63 Hopkins St. | Rehab | 1 |
| 116 | 5 | 65 Hopkins | Rehab | 1 |
| 116 | 15 | 146 Franklin St. | Rehab | 1 |
| 118 | 36 | 60 Curtis Ave. | Rehab | 1 |
| 119 | 16 | 47 Curtis Ave. | Rehab | 1 |
| 119 | 24.01 | 71 Curtis St. | Rehab | 1 |
| 125 | 24 | 682 N Broad St. | Group Home | 10 |

APPENDIX D | COMMUNITY COMMENTS & SUGGESTIONS

Issues

- A. Real and perceived pedestrian safety issues. These included concerns with Broad Street pedestrian crossings at unsignalized intersections and general conditions of loitering and lack of adequate lighting.
- B. Businesses are barely surviving.
- C. There is a lack of housing for younger professionals, families and artists.
- D. Many downtown buildings are characterized as dilapidated.
- E. Tax parcels are laid out in long narrow strips and difficult to lease, use, and redevelop.
- F. Broad Street is perceived as unsafe for pedestrians due to vehicular speeding.
- G. North Woodbury is lacking open space/parks.
- H. Access to surface parking lots and structured parking is difficult to find and the parking areas, themselves, often feel unsafe.

Ideas

- A. Hospital Area
 - 1. There is a need to improve circulation in the area,

with particular attention to pedestrian safety crossing Red Bank and Broad Street.

2. In an effort to take advantage of proximity to Underwood Hospital, there is an opportunity to bring medical-related businesses into the area and increase the amount of ratable properties.

B. Lakefront and Public Spaces

1. The lakefront is an asset that lends itself to active and passive recreation.
2. Establish the lakefront as a unique identity to Woodbury. It is both a front door for downtown and a back yard to the town.
3. Public access and security for lakefront open space needs to be improved.
4. The City should consider relocating the Police Station and using the existing station for lakefront amenity (e.g. restaurant, retail).
5. Pedestrian paths that link the new train station, new development, Broad Street and the Hospital area should be provided.
6. Undesirable elements such as transformers and underused parking lots should be moved or converted to provide more public open space.
7. An interconnected public park system with easy public access, linking downtown and people to the park system should be provided.
8. Make Broad Street an integral element of the public realm with improvements such as use of empty lot on Allens Lane for a park, improvements to the gateway at Broad Street and Edith Avenue and creation of a public park and amphitheater around Centre Street between Railroad Avenue and Broad Street.

C. Circulation, Parking and Wayfinding

1. Define and maintain current municipal parking

lots and add parking on the east side of Broad Street.

2. Improve lighting and repaint faded parking space lines on Broad Street.
3. Improve signage to direct users to parking lots and structured parking located on Cooper Street.
4. Consider a single travel lane in each direction on Broad Street for easier parking.
5. Implement traffic calming and improve dangerous conditions at non-signalized crosswalks on Broad Street while allowing for convenient automobile access to businesses.
6. Improve pedestrian connections and amenities along Broad Street.

D. Central Gathering Point, Public Plaza or Piazza

1. The downtown core needs a recognizable hub, store, square or public space.
2. Gateways should be treated with benches and landscaping.
3. There is a recognized need for more restaurants, businesses, civic spaces and housing.
4. Create upscale and higher density housing including residential above retail.
5. Encourage new buildings on the west side of Broad Street between Barber Avenue and Delaware Street.
6. Accommodate new and old buildings as long as there is a present consistent/cohesive appearance that contributes to maintaining the downtown character.
7. Encourage the development of businesses that provide night life.
8. Increase the ratable base.

E. Enhanced Activity, Day and Night

1. Create a safe pedestrian environment day and night to create a 24/7 downtown.
2. Increase shopping opportunities, the arts and activity on Broad Street.
3. There is a need for new, more diverse businesses.
4. Institute beautification projects along Broad Street.
5. Provide for redevelopment around the proposed Cooper Street Train Station.
6. Redevelop underused parcels, creating a pedestrian connection to Broad Street.
7. Make Centre Street two ways and create a public space/pedestrian only connection to Broad Street.
8. Institute a shuttle service from the train station to the Courthouse and Hospital areas.

F. Redevelopment Around the Proposed Red Bank Train Station Location

1. Develop a new station and transit hub north of Red Bank Avenue in the middle of the block off of Green Street and redevelop/beautify the immediate surroundings.
2. Maintain the proposed rail station south of Red Bank Avenue behind the big box parking lot. Move the CVS to create a parking lot for the new station.
3. Redevelop the CVS area with new buildings and businesses.
4. Create a pedestrian connection from the new station to Broad Street.

APPENDIX E | RESOLUTIONS & APPROVALS

The following pages contain the relevant resolutions and approvals pertaining to the Redevelopment Plan process.

**RESOLUTION
OF THE
WOODBURY PLANNING/ZONING BOARD**

Resolution recommending to the City Council of the City of Woodbury to adopt the Zone identified in the WOODBURY REDEVELOPMENT PLAN: DOWNTOWN BUSINESS DISTRICT, NEEDS STUDY FOR AREA IN NEED OF REDEVELOPMENT/REHABILITATION, REPORT OF FEBRUARY 11, 2009 SUBMITTED BY GROUPELMVINDESIGN ATTACHED HERETO.

WHEREAS, the City of Woodbury, City Council, through Resolution #09-61, authorized a study to determine the need for redevelopment along the Broad Street Corridor, pursuant to N.J.S.A 40 A: 12A-6.

WHEREAS, GROUPELMVINDESIGN was contracted by the City to conduct a preliminary investigation and determine whether or not the designated Study Area was “in need of redevelopment and/or rehabilitation”

WHEREAS, Three Steering Committee meetings, open to the public were conducted and two additional meetings of the Woodbury Planning/Zoning Board were held on April 6, 2009 and May 4, 2009 all being duly and properly noticed.

WHEREAS, throughout these meetings and also by additional submissions, participants expressed their concerns and understandings of the need for Redevelopment as a tool to revitalize the commercial district.

NOW THEREFORE BE IT RESOLVED by the Woodbury Planning/Zoning Board as follows:

1. The Study Area presented in the report/study as referenced in this Resolution has been found to be in need of redevelopment, meeting a number of the statutory criteria as set forth in the report.
2. A parcel and survey analysis as provided in the report reveals prominent vacant properties, obsolete block and property configurations, significantly underutilized properties that include publicly owned land, and an opportunity to utilize Smart Growth planning principles to accommodate new development for the growth of the City of Woodbury.

3. While New Jersey's Redevelopment Statute does not require that each individual parcel within a Redevelopment Area fit the criteria, in fact there are properties throughout the Study Area that are in good condition and contribute to Woodbury's position as a community of choice in Gloucester County.

4. Creating a Redevelopment Plan will help to reinforce strong residential neighborhoods and improve their value through the creation of a strong, walkable, lively, and viable commercial district.

BE IT FURTHER RESOLVED that the recommendation of the STUDY AREA parcels and zone as contained in the GROUPELMVINDESIGN report and as presented to the public and Board be designated by the City Council of the City of Woodbury as an area of redevelopment and a copy of this Resolution and the Report be forwarded to the Clerk/Administrator of the City of Woodbury, the President of Council and City Council for their attention and action. That a copy of this resolution be published and otherwise distributed as may be required by Statute.

Secretary, Woodbury Planning/Zoning Board

*The foregoing resolution memorializes
the action taken at a regular meeting
of the City of Woodbury Planning and
Zoning Board held May 4, 2009*

MEMBERS IN FAVOR:

THOSE OPPOSED:

ABSTAINED:



RESOLUTION #09-114
RESOLUTION OF THE CITY OF WOODBURY AUTHORIZING EXECUTION OF A
CONTRACT WITH GROUP MELVIN DESIGN FOR PREPARATION OF A
REDEVELOPMENT PLAN

WHEREAS, the City of Woodbury solicited proposals for the preparation of a need study and redevelopment plan; and

WHEREAS, the City of Woodbury selected Group Melvin Design; and

WHEREAS, Group Melvin Design was authorized to and did prepare a need study which has been evaluated and recommended by the City of Woodbury Combined Planning/Zoning Board; and

WHEREAS, the City of Woodbury has adopted a resolution designating an area in need of redevelopment; and

WHEREAS, the City of Woodbury wishes to proceed with Phase II and have Group Melvin Design prepare a redevelopment plan; and

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Woodbury hereby authorize the Mayor to execute a contract with Group Melvin Design for the preparation of a redevelopment plan in accordance with the attached proposal in the amount of \$137,000.00; and

ADOPTED at a regular meeting of the Mayor and City Council of the City of Woodbury on May 11, 2009.

CITY OF WOODBURY

By:

C. BARRY E. SLOANE, President of Council

ATTEST:

THOMAS B. BOWE, Clerk/Administrator

RESOLUTION #09-113

**RESOLUTION OF THE CITY OF WOODBURY ADOPTING THE
RECOMMENDATION OF THE COMBINED PLANNING/ZONING BOARD OF THE
CITY OF WOODBURY OF AN AREA IN NEED OF REDEVELOPMENT AND
DESIGNATING AN AREA IN NEED OF REDEVELOPMENT**

WHEREAS, the City Council of the City of Woodbury authorized the City of Woodbury Combined Planning/Zoning Board to undertake a preliminary investigation to determine whether a proposed area of the City should be designated as a redevelopment area according to the criteria set forth in N.J.S.A. 40:12A-1, et seq.; and

WHEREAS, the City of Woodbury Combined Planning/Zoning Board reviewed a map of the delineated area, prepared by Group Melvin Design showing the boundaries of the proposed redevelopment area and the location of the various parcels of property included therein; and

WHEREAS, the Combined Planning/Zoning Board provided public notice for and held public hearings on April 6, 2009 and May 4, 2009, for the purpose of hearing persons who were interested in or would be affected by a determination that the delineated area is an area in need of redevelopment; and

WHEREAS, the Combined Planning/Zoning Board has reviewed the Needs Study Investigation for Determination of a Potential Redevelopment Area setting forth the basis for the investigation and determination dated February 11, 2009 prepared by Group Melvin Design; and

WHEREAS, the Combined Planning/Zoning Board heard testimony regarding the basis for the determination pursuant to the criteria set forth in section 5 of P.L. 1992 c. 79 (C 40A:12A-5); and

WHEREAS, the Combined Planning/Zoning Board heard the statements of the public and interested persons who would be affected by the determination; and

WHEREAS, the City of Woodbury Combined Planning/Zoning Board adopted a resolution on May 4, 2009 that the Study Area parcels and zone as contained in the Group Melvin Design report met the criteria specified in N.J.S.A. 40A:12A-1, et seq., for determination as a redevelopment area and accordingly recommended to the City Council that such area be determined to be a redevelopment area; and

NOW, THEREFORE, BE IT RESOLVED by the Woodbury City Council, County of Gloucester and State of New Jersey on this 11th day of May, 2009, that the recommendation that the Study Area parcels and zone as contained in the Group Melvin Design report be designated a redevelopment area as set forth in the City of Woodbury Combined Planning/Zoning Board resolution dated May 4, 2009, is hereby adopted; and

BE IT FURTHER RESOLVED, that the Woodbury City Council finds that the designated area as set forth by block and lot on attached Exhibit “A” is in an area in need of redevelopment and meets the criteria specified in N.J.S.A. 40A:12A-1, et seq., for determination as a redevelopment area, for the reasons including, but not limited to, the reasons set forth in the above-referenced Group Melvin Design Needs Study, and for the reasons set forth by the Combined Planning/Zoning Board and their professionals; and

BE IT FURTHER RESOLVED, that the Woodbury City Council hereby determines and designates those block and lots set forth on attached Exhibit “A” as a Redevelopment Area pursuant to N.J.S.A. 40A:12A-1 et seq.

ADOPTED at a regular meeting of the Mayor and City Council of the City of Woodbury
on May 11, 2009.

CITY OF WOODBURY

By: C. BARRY E. SLOANE, President of Council

ATTEST:

THOMAS B. BOWE, Clerk/Administrator

2010-08

**A RESOLUTION OF THE CITY OF WOODBURY
PLANNING/ZONING BOARD RECOMMENDING THE
REDEVELOPMENT PLAN TO MAYOR AND COUNCIL**

WHEREAS, Mayor and Council of the City of Woodbury has previously designated certain areas within the City of Woodbury as in need of redevelopment; and

WHEREAS, the Mayor and Council of the City of Woodbury had directed the City Planner to prepare a Redevelopment Plan; and

WHEREAS, Group Melvin Design has prepared a Redevelopment Plan dated March 4, 2010; and

WHEREAS, by Resolution #10-65 as adopted on March 8, 2010, Mayor and Council of the City of Woodbury has referred this Redevelopment Plan as prepared by Group Melvin Design to the Planning/Zoning Board of the City of Woodbury for their review and report pursuant to N.J.S.A. 40A:12A-7; and

WHEREAS, the Planning/Zoning Board has reviewed this Redevelopment Plan, including by a presentation made to the Board by Group Melvin Design at its March 15, 2010 meeting, with input from the public in attendance.

NOW, THEREFORE, BE IT RESOLVED, by the Planning/Zoning Board of the City of Woodbury that the March 14, 2010 Redevelopment Plan, including the Form-Based Code, as prepared by Group Melvin Design, is

hereby recommended to the Mayor and Council of the City of Woodbury pursuant to N.J.S.A. 40A:12-7; this recommendation includes the findings that: (1) no provision contained within this Redevelopment Plan has been found to be inconsistent with the City's Master Plan; (2) no recommendations are needed from the Planning/Zoning Board regarding any possible inconsistencies with the City's Master Plan since no such inconsistencies were found; and (3) the Planning/Zoning Board does not hereby make any other recommendation to Mayor and Council regarding any matter that the Board deemed appropriate.

BE IT FURTHER RESOLVED that the appropriate Board officials are hereby authorized and directed to take all appropriate actions to effectuate the intent and purpose of this Resolution.

ROLL CALL VOTE

THOSE IN FAVOR: 9 Pafumi, Pottillo, Bosworth, Leary,
Haase, Meagher, Rose, Curtis and
Duncan

THOSE OPPOSED: 0

THOSE ABSTAINING: 0

Dated: April 19, 2010

CITY OF WOODBURY
PLANNING/ZONING BOARD

By: _____
STEVEN DUNCAN, CHAIRMAN

ATTEST:

JOHN LEECH, SECRETARY
PLANNING/ZONING BOARD

CERTIFICATION

The foregoing resolution was duly adopted by the Planning/Zoning Board of the City of Woodbury at the regular meeting of said Board on April 19, 2010.

JOHN LEECH, SECRETARY
PLANNING/ZONING BOARD

CERTIFICATION OF CLERK

I, **THOMAS B. BOWE**, Clerk/Administrator, of the City of Woodbury do hereby certify that the attached ordinance:

ORDINANCE #2118-10 – ORDINANCE OF THE CITY OF WOODBURY ADOPTING REDEVELOPMENT PLAN AND FORM-BASED CODE

is a true and exact copy of the original ordinance adopted by Mayor and City Council at the Regularly Scheduled Council Meeting of May 24, 2010.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed the seal of the City of Woodbury this day of June 8, 2010.


THOMAS B. BOWE
Clerk/Administrator

ORDINANCE #2118-10

ORDINANCE OF THE CITY OF WOODBURY ADOPTING REDEVELOPMENT PLAN AND FORM-BASED CODE

WHEREAS, the Mayor and City Council of the City of Woodbury desire to undertake a redevelopment pursuant to N.J.S.A. 40A:12-7; and

WHEREAS, the Mayor and City Council of the City of Woodbury by Resolution #09-113 established an area in need of redevelopment; and

WHEREAS, Mayor and City Council of the City of Woodbury directed Group Melvin Design to prepare a redevelopment plan; and

WHEREAS, by Resolution 10-65, adopted on March 8, 2010, Mayor and Council of the City of Woodbury referred a Redevelopment Plan and Form-Based Code prepared by Group Melvin Design dated March 4, 2010 to the Planning/Zoning Board of the City of Woodbury for their review and report pursuant to N.J.S.A. 40A:12A-7; and

WHEREAS, the Planning/Zoning Board has reviewed the Redevelopment Plan and Form-Based Code and by resolution of the Woodbury Planning/Zoning Board 2010-08 has recommended to the Mayor and Council of the City of Woodbury pursuant to N.J.S.A. 40A:12-7 the adoption of the Redevelopment Plan and Form-Based Code; and

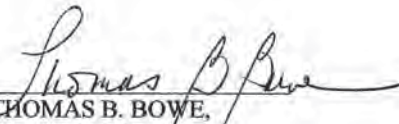
WHEREAS, the Woodbury Planning/Zoning Board found that no provision contained within the Redevelopment Plan and Form-Based Code to be inconsistent with the City's Master Plan, that no recommendations are needed from the Planning/Zoning Board regarding any possible inconsistencies and Mayor and Council wish to adopt such Plan and Form-Based Code with the introductory paragraph attached hereto.

NOW, THEREFORE BE IT ORDAINED, by the Mayor and City Council of the City of Woodbury that the Redevelopment Plan meets the requirements and criteria of N.J.S.A. 40A:12A-5 and N.J.S.A. 40A:12A-14 and is not inconsistent with the City of Woodbury's Master Plan; and

BE IT FURTHER ORDAINED by the Mayor and City Council of the City of Woodbury that the Redevelopment Plan and Form-Based Code prepared by Group Melvin Design dated March 4, 2010 with the introductory paragraph attached hereto is hereby adopted pursuant to N.J.S.A. 40A:12A-7 as the Redevelopment Plan for the City of Woodbury for the portion of the Redevelopment Area described in such plan, a copy of which is on file with the Clerk of the City of Woodbury.

THE FOREGOING was introduced by the City Council of the City of Woodbury at a regular meeting held on May 10, 2010. This Ordinance will be considered for adoption at final reading and public hearing to be held on May 24, 2010 at 7:30 p.m. or at such time as may be determined and announced, in City Hall, Council Chambers, 33 Delaware Street, Woodbury, New Jersey, at which time and place all persons interested will be given the opportunity to be heard concerning said Ordinance. During the time prior to and up to and including the date of such meeting, copies of said Ordinance will be made available at the City Clerk's Office to the members of the general public who may request same.


C. BARRY SLOANE,
President of Council

Attest: 
THOMAS B. BOWE,
Clerk/Administrator

Approved: 
ROBERT CURTIS, Mayor

